



TOWN OF JACKSON

PLANNING & BUILDING DEPARTMENT

TRANSMITTAL MEMO

Town of Jackson

- Public Works/Engineering
- Building
- Environmental Stewardship
- Town Attorney
- Police

Joint Town/County

- Parks and Recreation
- Pathways
- Housing Department

Teton County

- Planning Division
- Engineer
- Surveyor
- Assessor
- Clerk and Recorder
- Road and Levee

State of Wyoming

- Teton Conservation
- WYDOT
- TC School District #1

- Game and Fish

- DEQ

Federal Agencies

- Army Corp of Engineers

Utility Providers

- Qwest

- Lower Valley Energy

- Bresnan Communications

Special Districts

- START

- Jackson Hole Fire/EMS

- Regional Transportation

<p>Date: March 7, 2025</p> <p>Item #: P25-019</p> <p>Planner: Tyler Valentine</p> <p>Phone: 733-0440 ext. 1305</p> <p>Email: tvalentine@jacksonwy.gov</p> <p>Owner: Teton County General Services PO Box 3594 Jackson, WY 83001</p> <p>Applicant: Berning Project Management Wember, Owner Representative PO Box 485 Victor, ID 83455</p>	<p>REQUESTS:</p> <p>The applicant is submitting a request for a Sketch Plan for the new Teton County Justice Center at 180 S King Street, legally known as PT. LOT 2, BLK. 5, CACHE-3 (COUNTY COURTHOUSE, SOCIAL SERVICES, JAIL & PARKING LOT).</p> <p>PIDN: 22-41-16-34-2-07-003</p> <p>For questions, please call Tyler Valentine at 307-733-0440 x 1305 or by email to tvalentine@jacksonwy.gov. Thank you.</p>
<p>Please respond by: March 28, 2025 (with Comments)</p>	

RESPONSE: For Departments not using SmartGov, please send responses via email to planning@jacksonwy.gov



PLANNING PERMIT APPLICATION
Planning & Building Department

150 E Pearl Ave. | ph: (307) 733-0440
P.O. Box 1687 | www.townofjackson.com
Jackson, WY 83001

For Office Use Only

Fees Paid _____

Date & Time Received _____

Application #s _____

Please note: Applications received after 3 PM will be processed the next business day.

PROJECT.

Name/Description: _____

Physical Address: _____

Lot, Subdivision: _____ PIDN: _____

PROPERTY OWNER.

Name: _____ Phone: _____

Mailing Address: _____ ZIP: _____

E-mail: _____

APPLICANT/AGENT.

Name: _____ Phone: _____

Mailing Address: _____ ZIP: _____

E-mail: _____

DESIGNATED PRIMARY CONTACT.

_____ Property Owner _____ Applicant/Agent

TYPE OF APPLICATION. Please check all that apply; review the type of application at www.townofjackson.com/200/Planning

Use Permit

Basic Use

Conditional Use

Special Use

Relief from the LDRs

Administrative Adjustment

Variance

Beneficial Use Determination

Appeal of an Admin. Decision

Physical Development

Sketch Plan

Development Plan

Design Review

Subdivision/Development Option

Subdivision Plat

Boundary Adjustment (replat)

Boundary Adjustment (no plat)

Development Option Plan

Interpretations

Formal Interpretation

Zoning Compliance Verification

Amendments to the LDRs

LDR Text Amendment

Map Amendment

Miscellaneous

Other: _____

Environmental Analysis

PRE-SUBMITTAL STEPS. To see if pre-submittal steps apply to you, go to www.townofjackson.com/200/Planning and select the relevant application type for requirements. Please submit all required pre-submittal steps with application.

Pre-application Conference #: _____ Environmental Analysis #: _____
Original Permit #: _____ Date of Neighborhood Meeting: _____

SUBMITTAL REQUIREMENTS. Please ensure all submittal requirements are included. The Planning Department will not hold or process incomplete applications. Partial or incomplete applications will be returned to the applicant. Go to www.townofjackson.com/200/Planning and select the relevant application type for submittal requirements.

Have you attached the following?

Application Fee. Fees are cumulative. Go to www.townofjackson.com/200/Planning and select the relevant application type for the fees.

Notarized Letter of Authorization. A notarized letter of consent from the landowner is required if the applicant is not the owner, or if an agent is applying on behalf of the landowner. Please see the Letter of Authorization template at <http://www.townofjackson.com/DocumentCenter/View/845/LetterOfAuthorization-PDF>.

Response to Submittal Requirements. The submittal requirements can be found on the TOJ website for the specific application. If a pre-application conference is required, the submittal requirements will be provided to applicant at the conference. The submittal requirements are at www.townofjackson.com/200/Planning under the relevant application type.

Note: Information provided by the applicant or other review agencies during the planning process may identify other requirements that were not evident at the time of application submittal or a Pre-Application Conference, if held. Staff may request additional materials during review as needed to determine compliance with the LDRs.

Under penalty of perjury, I hereby certify that I have read this application and associated checklists and state that, to the best of my knowledge, all information submitted in this request is true and correct. I agree to comply with all county and state laws relating to the subject matter of this application, and hereby authorize representatives of Teton County to enter upon the above-mentioned property during normal business hours, after making a reasonable effort to contact the owner/applicant prior to entering.

Signature of Property Owner or Authorized Applicant/Agent
Jason Berning

Name Printed

Date

Title



**Town of Jackson
150 E Pearl Avenue
PO Box 1687, Jackson, WY 83001
P: (307)733-3932 F: (307)739-0919
www.jacksonwy.gov**

Date:

LETTER OF AUTHORIZATION

NAMING APPLICANT AS OWNER'S AGENT

PRINT full name of property owner as listed on the deed when it is an individual OR print full name and title of President or Principal Officer when the owner listed on the deed is a corporation or an entity other than an individual

Being duly sworn, deposes and says that **Teton County** is the owner in fee of the premises located at:

Name of property owner as listed on deed

Address of Premises: 180 S King Street / Jackson, WY 83001 & 460 E. Pearl Ave, Jackson, WY 83001

Legal Description: PT. LOT 2, BLK 5, CACHE-3 / LOT 1, SUNRISE ADDITION

Please attach additional sheet for additional addresses and legal descriptions

And, that the person named as follows: Name of Applicant/agent: Jason Berning / Berning Project Management

Mailing address of Applicant/agent: PO Box 485 / Victor, ID 83455

Email address of Applicant/agent: berningpm@gmail.com

Phone Number of Applicant/agent: 307-699-3733

Is authorized to act as property owner's agent and be the applicant for the application(s) checked below for a permit to perform the work specified in this(these) application(s) at the premises listed above:

Development/Subdivision Plat Permit Application Building Permit Application

Public Right of Way Permit Grading and Erosion Control Permit Demolition Permit

Other (describe) _____

Under penalty of perjury

Under penalty of perjury, the undersigned swears that the partnership, limited liability company or other entity, the appropriate approval of such entity, if required.

Title if signed by officer, partner or member of corporation, LLC (secretary or corporate owner) partnership or other non-individual Owner

STATE OF Wyoming)

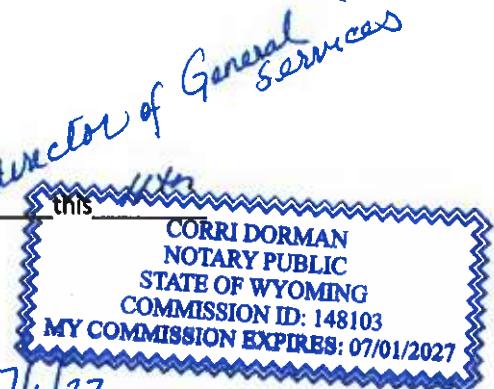
COUNTY OF Celon) 33.

—
—
—

The foregoing instrument was acknowledged before me by Sarah M.
day of January, 2024. WITNESS my hand and official seal.

Costs

My commission expires: 11 27



AndersonMasonDale

Town of Jackson Planning
150 E Pearl Avenue / PO Box 1687
Jackson, WY 83001

March 4, 2025

RE: Teton County Justice Center – Sketch Plan Submittal

Dear Paul Anthony,

Thank you, and the Town of Jackson, for your thorough review and comments on the Teton County Justice Center Pre-Application Submittal.

Please find attached the Sketch Plan Submittal, which addresses the comments and requested design updates, from the pre-design application review. Additionally, we have included a response log that specifically addresses each TOJ pre-application comment.

The project has wrapped up Schematic Design in coordination with this Sketch Plan Submittal and the project is currently undergoing cost estimation. The building size and program remain largely consistent with the Pre-Application Submittal: a three-story facility housing the county courts, associated court offices, sheriff's office, county jail, and a new 911 emergency response dispatch center.

One noteworthy revision enhances the pedestrian experience along King Street, creating a more urban, storefront environment. The updated design positions the primary entry at the prominent corner of King and Simpson. It also introduces a covered porch, a familiar building typology to Jackson, along King Street. The 'porch' is elevated above street level with a large multi-purpose room and a glass storefront along that façade.

Similarly, the design team has introduced a contextually sensitive building massing – stepping back level 3 to create the perception of a 2 story building from the street – respecting the immediate urban character and surrounding building heights.

Please note, several TOJ Pre-Application comments require further evaluation while others may be addressed based on budget availability. As cost estimation progresses and the project enters the Design Development Phase, it will become clearer whether some requested elements can be incorporated within budget constraints.

We would also like to highlight two high-priority items that may require further discussion or additional data collection:

- Traffic Study:

The submitted traffic study is based on data collected on December 19, 2024, representing a low to moderately busy day at the courthouse. This study extrapolates and anticipates busy days and future conditions based on that baseline data.

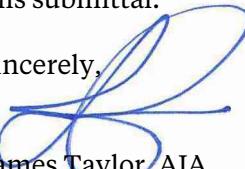
- Additional data collection on a “busy day” – i.e. when a jury trial is in session - can be included in a future submittal to the TOJ upon request
- It should be noted that there are no new programs or uses coming to this site; this new facility is a modernization and revitalization of the existing uses already in place. This is notable because we do not expect an increase in traffic.

- Vehicle Access on to Willow Street:

- This issue is not a simple yes-or-no decision. Multiple factors must be considered.
- The County and Justice Center leadership fully supports efforts to protect pedestrians, especially children and families using the Willow Street Safe Routes to School bike and pedestrian pathways.
- However, vehicle access on Willow Street remains essential for the Sheriff's Office to perform its public safety duties. A secure yard for the Sheriff's Office requires two access points, and site constraints prevent access from Simpson Street. Therefore, access must be provided from King Street and Willow Street.
- The current site condition has an access points on to Willow Street. The future growth of the building's use is expected to have a minimal impact on vehicle traffic, mid block.
- A potential win-win scenario is to design the proposed access points on Willow Street with best practices in traffic calming, pedestrian safety, and visibility. The intersections will be configured to maximize pedestrian visibility while ensuring vehicle speeds remain below 5 mph.

We look forward to the continued conversation and to your review of the Sketch Plan Drawings and responses. We truly appreciate your time and consideration in reviewing this submittal.

Sincerely,



James Taylor, AIA
Principal

Teton County Justice Center - Narrative Description

Date: 4 March 2025
Project #: 24-031
Project: Teton County Courthouse – 180 S. King Street

The is a Narrative Description of the project including response to Finding of Approval of Sketch Plan (LDR Sec. 8.3.2.C). The narrative describes the entire project including: its square footage breakdown totals and by level, describes uses by level and who uses the building, provides a full master parking analysis that justifies the proposed parking meeting the needs of the New Justice Center / Town Hall / Teton County General Services Building (in accordance with the condition of approval for TCGSB CUP), discusses how the site access and circulation work, describes changes to the alley and states which sections are one way or two way, describes preliminarily how snow storage/management will be handled, lists all applicable LDR standards and whether the project complies (i.e., FAR, LSR, height, stories, parking requirements, parking dimensions, setbacks, bike parking, EVSE parking, etc.), provides a statement that housing requirements are not provided as this project is exempt, describes proposed materials and architecture inspiration, describes the proposed pedestrian frontage changes, describes how loading/trash will work and where it is located, etc. The narrative goes through the LDRs and lists all applicable standards and a written response to whether the project complies or not. The applicable sections are listed in the Pre-application Conference Checklist attached (P24-125). Even though this project is in the P/SP zone, which is exempt from many standards, the narrative demonstrates what is proposed. For example, even though there are no street or alley setbacks, it is provided in writing the setbacks proposed on each street, setbacks from the alley, and separation from the Federal courthouse. In general, the narrative assumes that the Town knows nothing about the use of a courthouse/jail or its demands and needs for employees, parking, peak busy times during the week, hours of operation, etc.

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A General Project Description and Building Use

The project consists of a new three-story Justic Center building on the existing County Owned Property at 180 S. King Street.

The ground level program consists of:

Sheriff's office functions, including secure elected official / staff parking, a secure sally port, public sheriff's lobby, sheriff's office manager and public transaction windows, multi-purpose room, interview rooms, booking / holding, investigations, Sheriff's IT and servers, evidence processing, evidence storage, restrooms, break areas, print/copy/work areas, general storage, gear storage and armory.

Level 1.5 (partial level) program consists of:

The primary public entry from the west along King Street. Program at this level consists of public courts entry, lobby and security screening, a separate employee entrance on the Northwest corner of the building, a large multi-purpose meeting room, chair and table storage, kitchenette, restrooms and wellness rooms / mothers room. All program along King Street are either meeting rooms of public entry and thus has over 75% ground level storefront glazing with a covered and raised porch taking precedent from Historic Downtown Jackson building forms.

The second level program consists of:

The county attorney offices, including waiting areas, large conference room, open work stations, private attorney offices, storage, high-density storage solutions.

Sheriff's dispatch / 911 call center including: (6) control workstations, private offices, breakroom, storage, file storage, restrooms, lockers, and a quiet room w/ operable Juliet balcony.

Detention center including: a secure man-lock / vestibule, secure elevators with access to courtroom holding, contact visit / attorney room, video arraignment / visit room, control center, 7 holding pods / day rooms, indoor / outdoor fitness space

Shared storage and employee showers, restrooms and employee support areas.

The third level program consists of:

(3) Courtrooms that maintain 3 distinct paths of travel that do not cross – judges access from the rear directly to Dais, public access from the public gallery from the south and detainee's access from the middle, rising from secure / in-custody elevators and holding cells. Meeting rooms and courtroom sound-locks are accessed from public gallery side.

(2) District court judges chambers / suite with offices, open office space, copy/print area, storage, break room, restrooms. Courts Security office suite sits between both judges chambers.

District Court Clerk office suite with a waiting area, project room, public file viewing room, private offices, open office space, breakroom, active file storage, vault and restroom

Circuit Court judge's chambers with direct access to circuit court clerk suite consisting of: waiting area, private office, open office area, copy/ print a, active file storage and a break room

B Project Design Drivers, Mission Statement & Design Concept

Fundamental Design Concepts

- Functional
- Safe
- Secure
- Appropriately Scaled to Context
- Civic Presence
- A Dynamic Entry Experience
- Timeless and Durable
- Sustainability on Display

Mission Statement

The Justice Center will serve as a symbol of safety, justice, and community, uniting all branches of Teton County's Judicial System under one roof. The building draws its unique character from Jackson's alpine and mountain-west heritage – fostering a sense of belonging and a part of Jackson's unique identity. Thoughtfully chosen materials embody strength, warmth and civic character. Built sustainably, this center will support Teton County's public servants and citizens for generations to come.

Massing and Contextual Sensitivity Public Façades

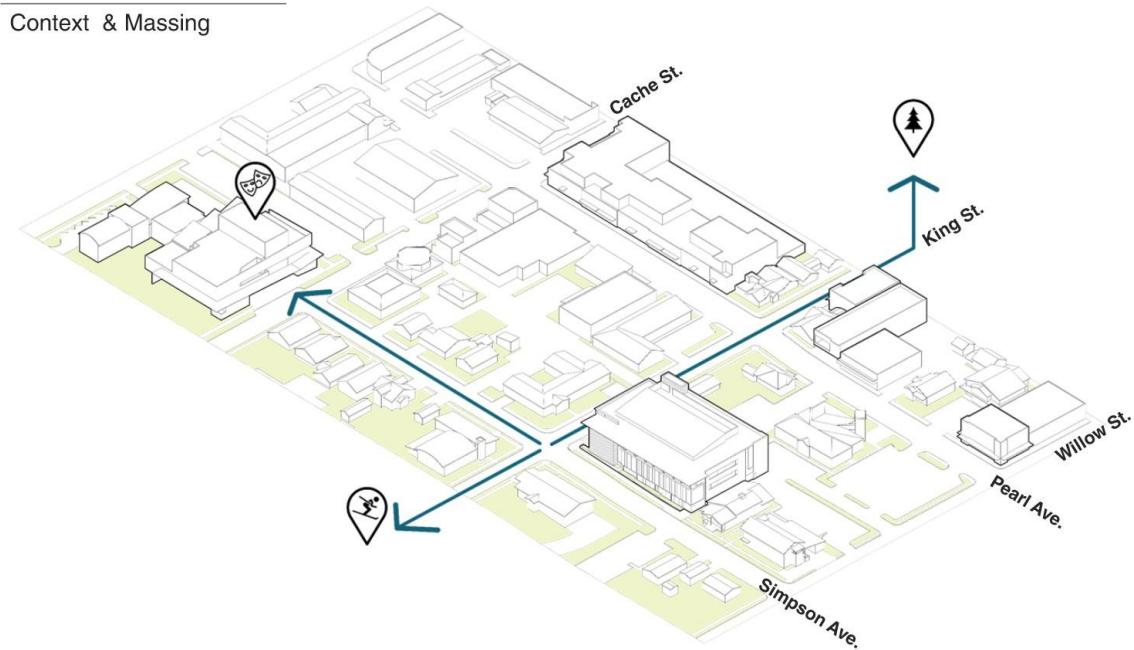
(see diagrams below description)

Pedestrian Experience, Safety and Building Security are the primary considerations in siting the building. King Street is the primary pedestrian oriented façade and along the most public route from the center of Jackson. To support the spirit and intent of LDR's, the King Street elevation - on level 1.5 / at ground floor at King - is setback 28' from the street curb giving ample room for a tree lined sidewalk in grates and an elevated and covered porch. This façade has over 75% ground level storefront glazing aligning with the more community focuses program behind the façade. The raised and covered porch takes precedent from Historic Downtown Jackson building forms. The main public entry is at the corner of King and Simpson, at the terminus of this pedestrian porch, inviting the public into the public gallery and directly up an interconnected stair to the 3rd level Courts Gallery. The building is experienced as

2 stories from King Street as the grade climbs almost a full story from ground level mid-block on the east up to level 1.5 on the west. Street parking and loading areas will remain as exists today.

Simpson Avenue is the secondary street but still an important public facing façade. As mentioned prior, the street slope drops from west to east nearly 8'-0" from King Street down to the mid-block, sheriff's office entry. The ground floor's primary massing is setback from 19'-8" from the curb, with the Southwest (primary entry) corner setback 13'-8" from the street curb. This provides a generous buffer with a tree lawn between the sidewalk and curb – on-street parking will remain as exists today. The ground floor along Simpson Ave. has generous amounts of glass, 40% to 50%, but due to the secure / private nature of the Sheriff's Offices along this façade, a planting bed of Aspen with planted ground cover sits in front of the building. The planting beds terrace down the slope of the street. Level 3 along Simpson Ave. is setback from the street edge allowing the building to be read as a 2-story building along this edge as well. A timber framed canopy projects over the level 3 – protecting the courts gallery from solar gain while also breaking down the massing and providing shadow and warmth to the building experience.

Context Diagrams (showing other 3 story buildings in the immediate vicinity)

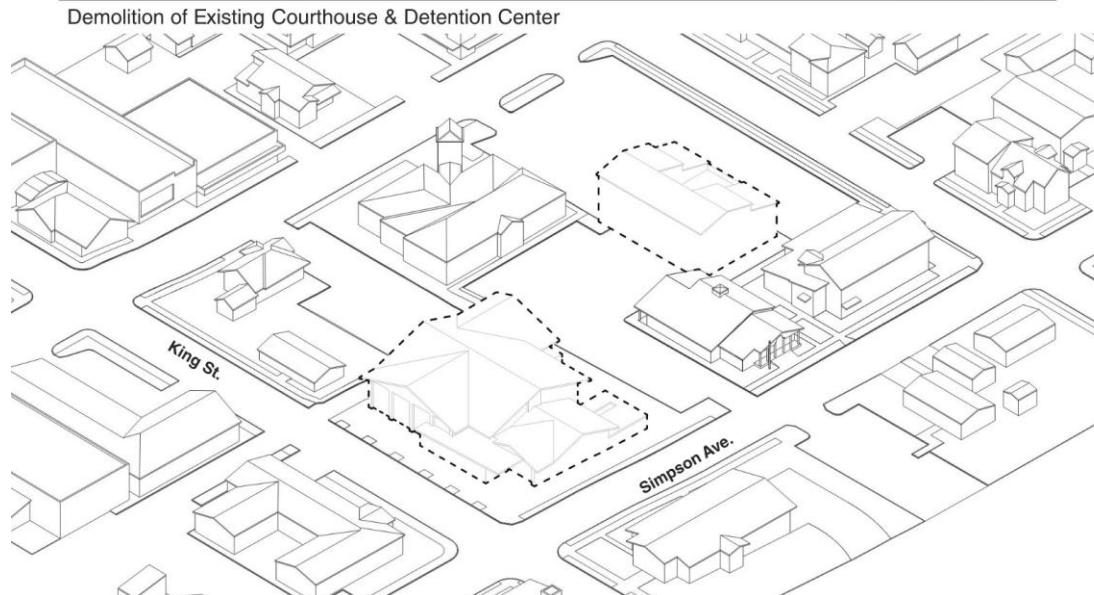
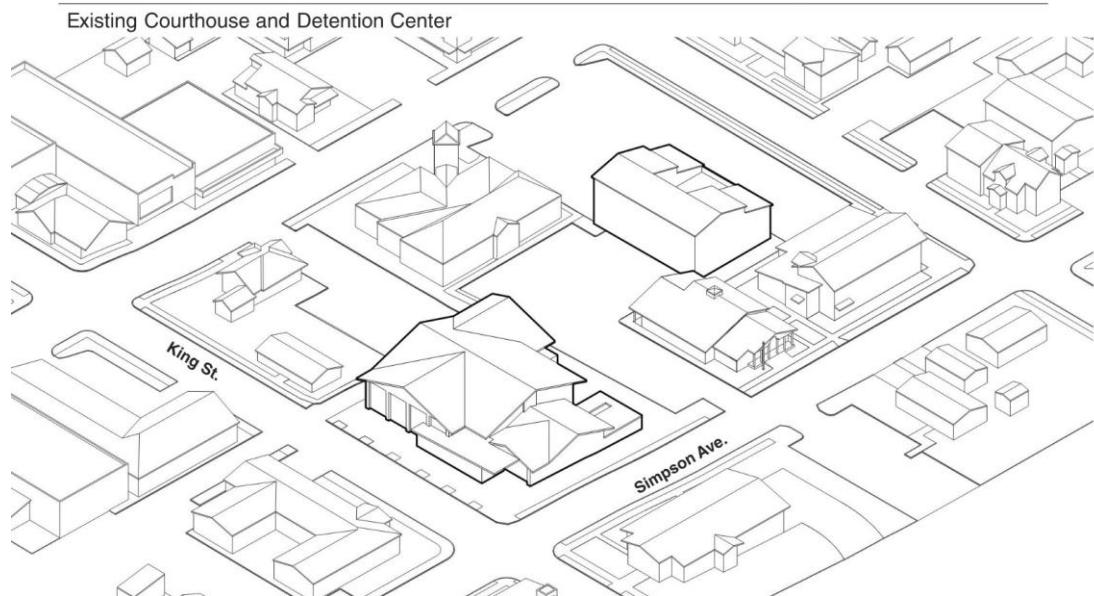


Teton County Justice Center – 180 S. King St. - Narrative Description

4 March 2025

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Concept Massing Diagrams

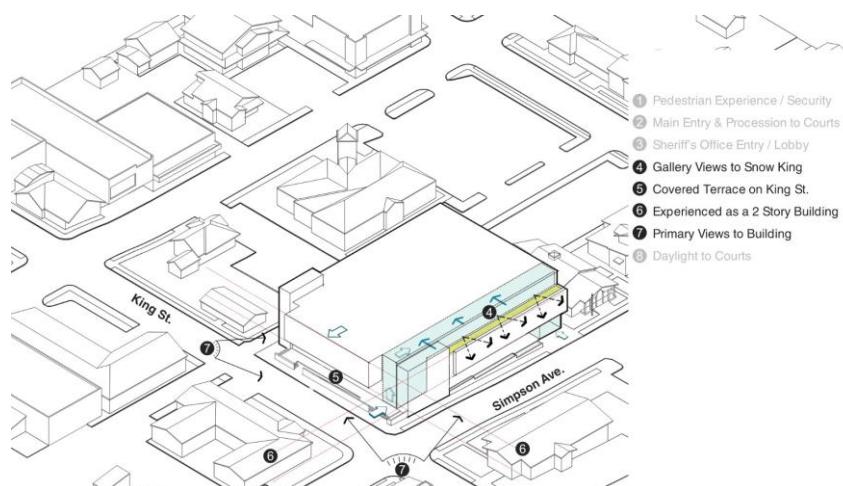
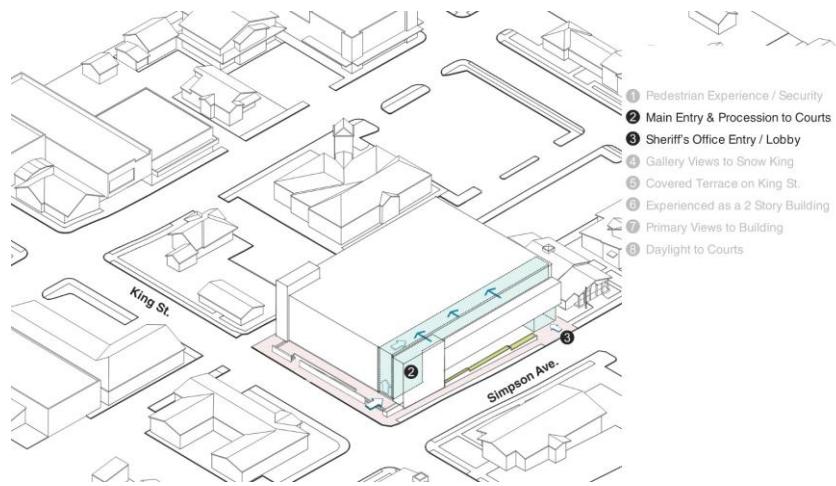
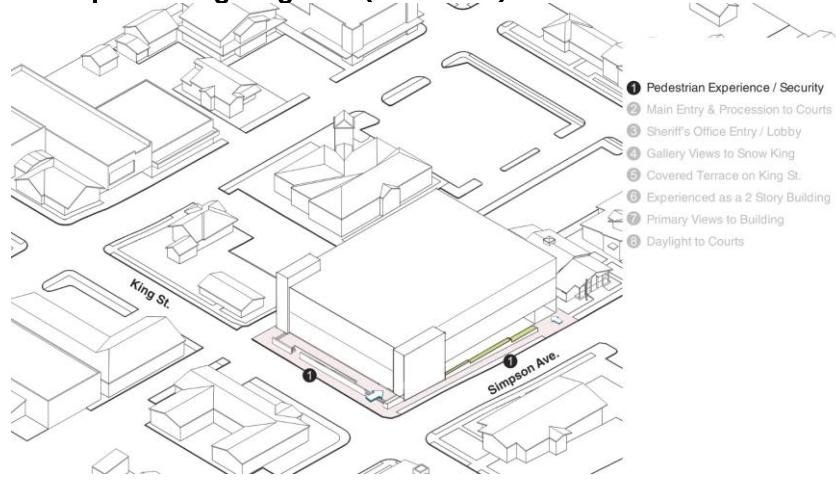


Teton County Justice Center – 180 S. King St. - Narrative Description

4 March 2025

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Concept Massing Diagrams (continued)

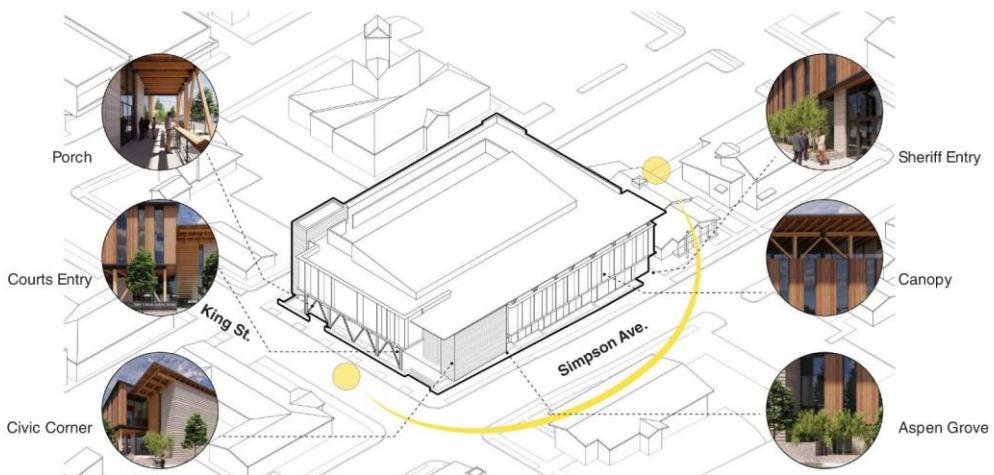
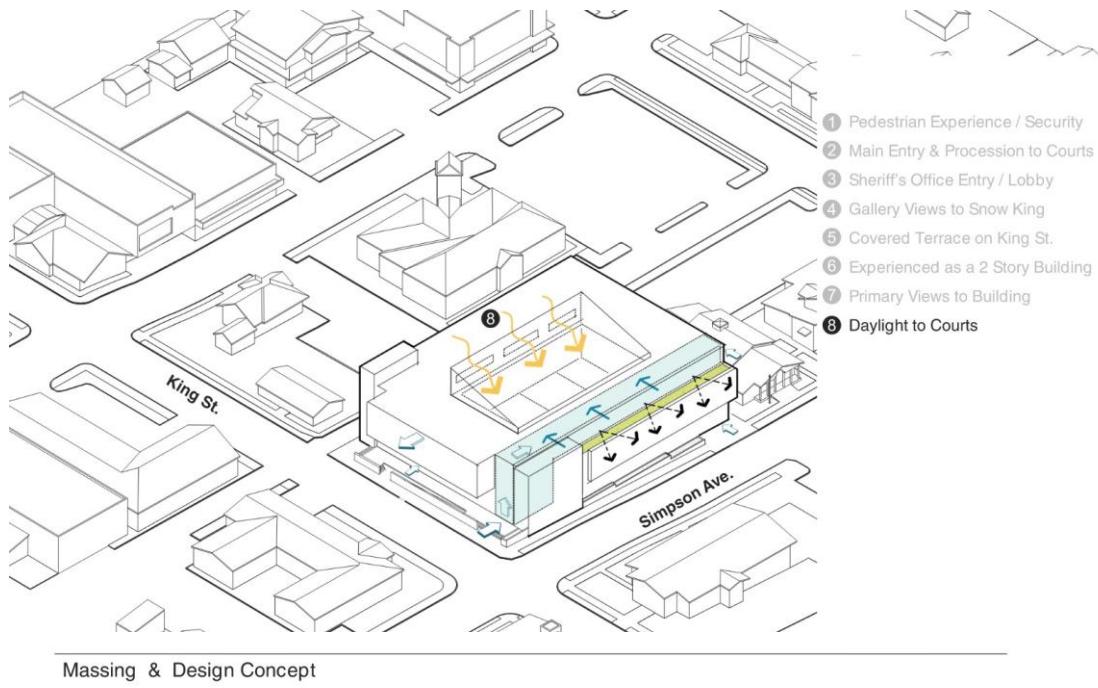


Teton County Justice Center – 180 S. King St. - Narrative Description

4 March 2025

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Concept Massing Diagrams (continued)



Proposed Exterior Materials (RE: A-201 through A-215 in Sketch Plan Drawings)

- The primary exterior Materials (on the King St. and Simpson Ave. Façade)
 - Wood (or Wood Look) Panels (dependent on fire resistance code requirements)
 - Terracotta or Stone or Manufactured Stone (depending on budget)
- The Secondary Exterior Materials (At Alley and Interior Lot Adj. Hanson Courthouse)
 - Masonry Veneer

- Metal Panel (Dark Color)

Architecture Inspiration

Inspiration is Derived from Jackson Identity and Local Character. Particularly the use of wood, natural materials and building forms that create covered boardwalks using canopies and elevated sidewalks. The following projects provided inspiration for their warmth and use of natural materials, especially wood – the perceived depth of building façades, with the use of deep recesses and canopies that accentuate light and shadow play. Exhibit Below includes: The SE Wyoming Welcome Center, Jackson Home Ranch Visitors Center, JH Airport, Cloud Veil JH.

Precedents and Materiality



C Building Data and Occupancy Data

Total Site Area:

1.9 Acres (84,400 SF)

Justice Center Building

79,000 Total GSF

Breakdown of Totals by Level

Ground Level – 26,200

Level 1.5 – 5,000

Level 2 – 23,250

Level 3 – 25,500

Building Occupants Employee (FTE) Count & Hours of Operation

New Justice Center

Current Teton County Courthouse:

Attorney's Office - 12

Clerk of Court - 6

Sheriff (Communications) - 14

Sheriff (Detention) - 17

Sheriff (Sheriff's Office) - 47

District Court – 4

Circuit Court – 4

Total Employees: 104

New Justice Center (Includes 25 yr Future Growth):

Attorney's Office - 19

Clerk of Court - 7

Sheriff (Communications) – 14*

Sheriff (Detention) – 20*

Sheriff (Sheriff's Office) – 50*

District Court – 8

Circuit Court – 8

Total Projected Employees: 126

*Note: approximately 1/3rd of Sheriff Office Employees in building at peak hours

21% Growth projected over the next 30 years

Justice Center Hours of Operation

The New Justice Center hours of operation will match the existing Teton County Courthouse Hours of Operation

Courts and Associated Office Spaces: 9am-5pm Business Hours

Sheriff Office: 24/7 (limited Staff / Personnel after hours)

911 Call Center: 24/7 (2 to 4 staff after hours)

Existing General Service Building:

Total Employees: 20 employees

Hours of Operation 9am-5pm

Existing Town Hall Building

Total Employees: 65 (plus 6 part time / occasional employees)

Mayor and Council – 5 occasionally

Admin – 2

Building and Planning – 8

IT – 7

Finance – 5

Muni Court – 1 plus 1 occasionally

Attorney – 2

External Affairs – 2

Police – 38 (see specific breakdown of PD Employees Below)

Jackson Police (expanded occupancy information)

7 days a week (4) patrol officers working in the city (+1 at the airport). Our dayshift starts at 0700 and our night shift starts at 1900. Each shift has 5 officers, with 1 dedicated to the airport from 0500-2130. Any given day the maximum number of patrol officers is 10, if no one is on vacation or training (spread out over 24 hours). Four parking spaces during business hours for patrol alone (We have three marked spaces on the south side for emergency vehicles now). We rarely have all of the officers in the office at the same time.

2 admin officers (Chief and Lieutenant) that park 0800-1700 M-F.

1.5 Information Coordinators that park 0730-1730 M-F with one not working on Wednesdays.

5 Detectives that work mostly day shift (10 hour shifts four days/week) with at least one working on Saturday (they rotate).

1 Police Social Worker (M-F)

1 Code Enforcement Officer (T-F) (I'm not sure if Roxanne counted this person in building/planning or not)

1 CSO Sergeant (M-Th)

1 CSO Corporal (T-F)

1 CSO (M-Th)

1 CSO (Sat-Tue)

2 IT (M-Th) (Roxanne's IT numbers may include these two)

Building to be Demolished

Existing Teton County Courthouse 32,000 GSF

Existing Teton County Jail – 16,000 GSF

Existing Buildings to Remain

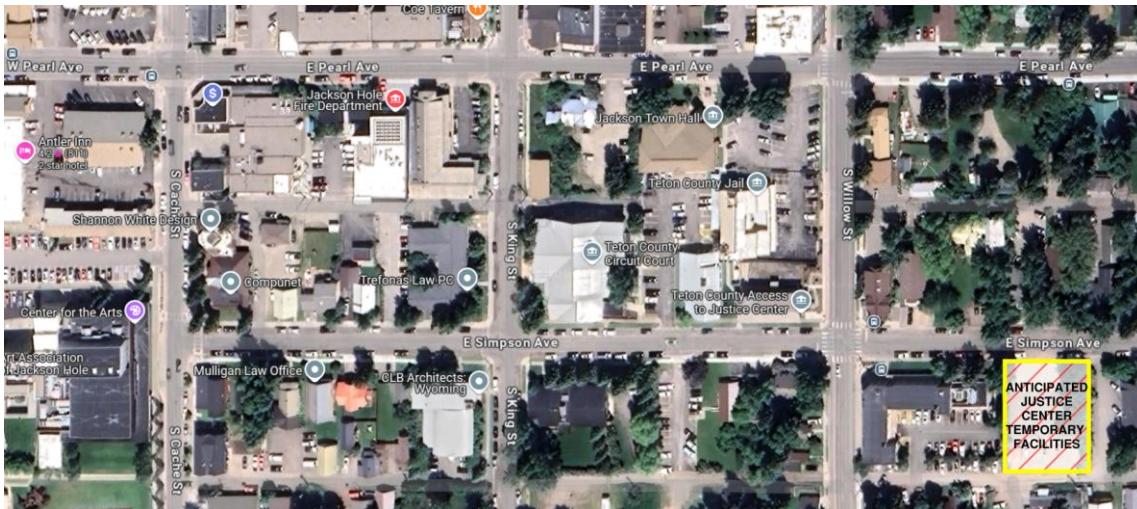
Jackson Town Hall

Hanson Courthouse (owned and operated by Teton County) – 3,800 GSF

Teton County General Services Building – 11,312 GSF

Temporary Facilities for County Functions

- The County functions that currently take place in the existing Teton County Courthouse - Courts, Sheriff's office, County attorney's office, District and Circuit Clerks - will be moved to temporary facilities, that is yet to be determined by the County. Potential Scenarios include
 - Use of the existing Hanson Courthouse
 - Renting office space in the County
 - Using modular buildings on county owned land
- All three temporary facility scenarios described above may be required to maintain County operations efficiently and economically.
- As more information comes to light the County will keep the town updated.
- The County is currently in negotiations to rent the building at 207 E. Simpson, directly adjacent to the county building and property. This rental will include the parking lot and detached garage building. It is anticipated that this building will be used by the Sheriff Department and County Attorney's office.
- The exhibit below identifies a county owned property at 207 E. Simpson Ave. This site may be used for temporary modular space for county functions displaced during construction activity.
- If this scenario is exercised, the County will work with the Town to provide a temporary use agreement for modulars - very similar to the process to approve construction trailers - as these modulars are a function of construction activity and are by their nature as temporary as a construction trailer. Expediting any anticipated modulars is critical to keep the project schedule moving and on budget.
- If modulars are to be used on the Simpson site, they will be installed meeting LDR requirements for temporary construction facilities. These modulars will be connected to the Town water and sewer system.

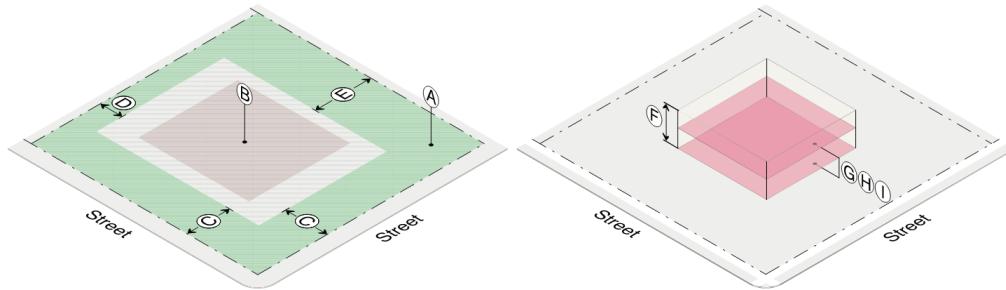


E LDR Standards / Compliance-4.2.1 Public / Semi-Public – Town (P/SP-ToJ)

A. Intent

The purpose of the Public/Semi Public - Town (P/SP-ToJ) zone is to provide locations for new and existing uses and facilities of a public or semi-public nature. In particular, the P/SP-ToJ zone is intended to allow flexibility for public and semi-public uses and facilities that often have unique functional needs, such as for height, floor area, setbacks, and impervious surface, that cannot be accommodated in other zoning districts. Land in the P/SP-ToJ zone and/or facilities operated therein may be under the control of federal, state, or local governments, or other governmental entities such as a school district or hospital district. It is not the intent of these LDRs that property in the P/SP-ToJ zone retain that designation after the property is divested by the public entity. At the time P/SP-ToJ designated land is transferred, or is proposed to be transferred, into private ownership, the property shall be reclassified to an appropriate zoning district to allow private use pursuant to the Jackson/Teton County Comprehensive Plan.

Although many requirements in the P/SP-ToJ zoning are not applicable. The Justice Center aspires to comply or come close to complying with adjacent property LDR standards from Downtown Design Area 2. The intent is for the proposed building to fit the quality and character of the surrounding neighborhood as much as possible.



1. Structure Location and Massing

- A. LSR (MIN): N/A LSR Proposed: 0.06 (5,000/84,400)
- B. Lot Coverage (MAX): N/A Lot Coverage Proposed: 0.42 (35,400/84,400)
- C. Street Setback (MIN): N/A King Street: 20' Setback (Building Face at Grade)
King Street: 14' Setback (To Building Overhang)
Simpson Ave: 8' Setback (To Main Building Mass)
Simpson Ave: 1' Setback (Limited to Building Corner
@ King & Simpson)
- D. Side Setback (MIN): N/A Distance from Hanson Courthouse: 6'-2"
- E. Rear Setback (MIN): N/A Distance from Alley Property Line: 1'
- F. Height (MAX): N/A Level 3 Low Roof (Public ROW facing): 51'-6"
High Roof (Courtroom Pop-Up) 65'-6"

Teton County Justic Center – 180 S. King St. - Narrative Description

4 March 2025

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G. Stories (MAX): N/A	3 Stories
H. Stories (LO): N/A	N/A
G. FAR (MAX): N/A	1.52 (includes all building on Teton County Property)

2. Maximum Scale of Development: N/A

3. Building Design

Non-residential building design guidelines (Div. 5.8 LDR)

-Note North Half of Lot 2 in "Downtown Design Area 2"

4. Site Development: N/A

5. Landscaping (Div. 5.5): N/A

6. Fencing: N/A

7. Environmental Standards

- Natural Resources Buffers
 - Cache Creek 20': N/A
 - Flat Creek North of Hansen Ave. 25': N/A
 - Flat Creek South of Hansen Ave. 50': N/A
 - Wetland 30': N/A
- Irrigation Ditch Setback (MIN) (7.7.4.D): N/A
- Natural Resource Overlay (NRO) (SEC 5.2.1) N/A

8. Scenic Standards

- Exterior Lighting
 - Light Trespass Prohibited - Complies
 - Lights of 600 Initial Lumens Fully Shielded - Complies
 - Lumens per SF of Dev. (Max): 3 Lumens - Complies
 - Lumens Per Site (Max)
 - All Fixtures: 100,000 Lumens - Complies
 - Unshielded Fixtures: 5,500 Lumens - Complies
 - Light Color: ≤ 3000 KELVIN

9. Natural Hazards to Avoid (Project is NOT located within):

- Steep Slopes (Lot Average Cross-Slope $\geq 10\%$)
- Areas of Unstable Soils
- Fault Areas
- Floodplain
- Wildland Urban Interface

10. Signs (Div. 5.6): N/A

Signs Proposed: Building Identity / Address, RE: Exterior Elevations on King Street and Simpson Ave.

11. Grading, Erosion Control, Stormwater

- Grading (SEC. 5.7.2) - Compliant
- Erosion Control (SEC. 5.7.3.) – Compliant
 - Erosion Shall be Controlled at all times - Compliant
- Storm Water Management (SEC. 5.7.4)
 - No Increase in Peak Flow Rate or Velocity Across Property Lines - Compliant

12. Required Physical Development Permits

- **Sketch Plan (SEC. 8.3.2)**
- **Development Plan (SEC 8.3.3)**
- **Building Permit (SEC 8.3.4)**
- **DRC Review (SEC 8.2.6)**

Parking Requirements (SEC. 6.2.2-A)

- Institutional & Assembly Uses
(Courts, Institutional Offices, Detention Center)
- Ratio: Independent Calculation Based on Existing Use & Percent Growth of Justice Center.
- Existing On-Site Parking Count (on County Owned Lots) 109 Total Stalls
- Proposed On-Site Parking Count (on County Owned Lots) 121 Total Stalls
- Total Property Parking Increased By 11 Stalls (10%) increase
- 5 Accessible Parking Spaces are required per LDR 6.2.2-C and 5 Accessible parking spaces are to be provided
- **Refer to Master Parking Analysis Section of this Narrative for more Information and Justification (pg. 16 of this narrative)**

Parking Space Dimensions (SEC. 6.2.5_D)

- Requirement 90 deg. parking spaces are required to be 9 feet wide by 20 feet Long
- Spaces can be reduced to 18 feet depth, including wheel stop, if an additional space of 2 feet in length is provided in front of the overhang of the car, provided that an overhang shall not reduce the width of an adjacent walkway to less than 4 feet in width.
- Spacing backing into the Alley must be 22 feet depth
- All spaces provided meet the requirements above
 - All Spaces are 9 feet wide x 20 feet depth with exception to two locations as described below
 - The south bank of parking in the sheriff's secure lot is 18 feet depth with 2 feet overhang space in front of the vehicle and wheel stops
 - The bank of parking just south of Town Hall access directly from the alley is 22 feet depth

EVSE Parking Requirements (SEC. 6.2.2 for Other Nonresidential use)

- 5% of total parking spaces will be EVSE installed and 30% will be EVSE capable. Refer to Sheet A-001 for EVSE parking stall information.

Bike Parking Requirements (SEC. 6.2.2-D)

- Required: One bicycle parking space shall be provided for every 10 vehicle space
- Proposed bicycle parking count complies see below for details.
- Total Provided 18 short term spaces and 7 long term spaces.
- Long Term spaces are provided inside the building on Level 1.5 near the employee entrance on King Street (RE: Drawings Sheet A-101.5)
- Short Term Spaces are provided in the ROW along King Street between Tree Grates between the sidewalk / walking path and the curb and along Simpson Street (RE: Drawings Sheet A-001 and L-301)

- LDR Compliant Calculations:
 - Assembly: 6,813 SF
 - Bike Parking Ratio: 1 PER 2,200 SF
 - $6,813 / 2,200 = 3$ Bike Spaces (1.5 Short Term + 1.5 Long Term)
 - Business / Office 35,015 SF
 - Bike Parking Ratio: 1 PER 1,650 SF
 - $35,015 / 1,650 = 21$ Bike Spaces (15.75 Long Term + 6.75 Short Term)

Exterior Lighting (LDR 5.3.1)

Per LDR Section 5.3.1:

- All exterior fixtures that exceed 600 lumens shall be shielded at 90 degrees.
- The maximum lumens for shielded fixtures is 3 lumen per 1 square foot of site development not to exceed 50,000 lumens per acre of site development.
- Fixtures less than 600 lumens may be unshielded, but the total lumens allowed cannot exceed 2,000 lumens per acre of site development.

Housing Requirements (LDR 6.3.2 C)

The project is exempt from providing housing per LDR Section 6.3.2(C)-13
“Any use in the P/SP zone is exempt from standards of this division”

F Site Access | Site Circulation | Trash | Delivery | Snow Storage

Site Access & Vehicle Circulation

Vehicular Access to and through the campus will be similar as it exists today

- Alley
 - The Alley will not connect to Willow Street
 - The Alley will be Two-way from the Secure Sheriff's yard northern gate to the east, accessing the N/E parking lot adjacent to Town Hall and continuing with access to Pearl St.
 - One-way Alley (outbound) east from the Secure Sheriff's yard out onto King Street, along the north side of the Justice Center.
 - Consistent with today's condition there will be no entry into the Alley from King St.
- Two driveways will remain as exists today along Pearl Avenue, which currently serve the N/E parking lot adjacent to Town Hall
- The N/E Parking Lot adjacent to Town Hall will function similar as it does today, except the drive aisles and re-striping of parking stalls at 90-degrees will accommodate two-way traffic. The Eastern Pearl Ave. access is to be signed to prohibit left-in movements.
- One access point is proposed onto Willow Street – a secure / gated driveway providing access from Secure Sheriff Yard onto Willow St.
 - To ensure pedestrian and bike safety on Willow St. This drive will have limited use by Sheriff's personnel only. There will be no public access on this driveway Willow St.

- The public will not have vehicle access onto Willow St.
- The fence, gate and paving will have pedestrian bike crossing indicators and marked signs, making those operating vehicles aware and diligent looking for potential pedestrians and bikes
- The fence and gate will be located to maintain clear site lines for drivers allowing visibility up and down Willow St. for oncoming pedestrians and bikes
- The existing driveway from Simpson Avenue will no longer exist. However, in future if the Hanson Courthouse were ever to be demolished the primary access point to and from the secure yard could be provided to Simpson Ave.
- Refer to Traffic Study for additional data and information

Site Trash Pick Up & Deliveries

Trash and recycling dumpster are planned to be located on north east side of the secure sheriff yard, surrounded by fencing and a gate accessed from the Alley. Trash trucks will access the dumpster from Pearl St. traveling through the NE parking lot. RE: A-001 drawing sheet for dumpster locations.

Scheduled, large deliveries (infrequent occurrence), and food truck deliveries (serving the detention center kitchen) will occur in secure yard and sally port. Delivery truck will access the secure yard from the Willow Street entry point avoiding maneuvering through the NE parking lot.

Daily business deliveries (USPS, Fed X, UPS, etc.) will occur at the front door along King Street and at the front door to the Sheriff's office along Simpson Ave.

Snow Storage (LDR Section 6.2.5.C)

Required: A minimum site area representing 2.5% of the total required off-street parking and loading area, inclusive of drive aisle and access drives, shall be provided as the snow storage area.

Sheriff's Secure Parking Lot and associated driveway:

- Paved Area: 11,200 SF
- 2.5% Snow Storage; 280 SF (equivalent to 1.5 Parking Spaces)
- Snow Storage will occur inside the secure yard on the north end of the site taking up two parking spaces in the winter months.

NE Parking Lot and associated driveways

- Paved Area: 20,200 SF
- 2.5% Snow Storage; 505 SF (equivalent to 2.75 Parking Space)
- Snow Storage will occur in NE parking lot taking up to three adjacent parking spaces in winter months.

The sidewalks and roadways will be clear and snow storage removed from site by Town and County Operations.

G Master Plan Parking Analysis

This narrative includes a summary of data demonstrating parking demand using independent parking calculations derived from current and projected growth building employee and visitor count. The following summarizes the justification of the parking counts provided, showing that the proposed parking meets the needs of the New Justice Center and Teton County General Services Building. See notes and separate calculations below regarding Town Hall.

For additional data and information please refer to the Traffic Impact Study submitted with Sketch Plan Collateral. The traffic impact study includes, but is not limited to, the following data:

- Turning movement data and vehicle trips (collected in December 2024)
- Existing parking lot utilization data (collected January 2025),
- Future Trip Generation Projections based on estimated future conditions

Parking Demand Summary

Justice Center Parking Demand

(employee counts + infrequent jury/court visitors. Data includes Future Growth)

- General Services, 20 people, assume 15% use non-auto modes.....**17 spaces of demand**
- Attorney, Office Clerk, and Courts, 42 people, assume 15% use non-auto modes....**36 spaces of demand**
- Sheriff, 84 total people, assumes approximately 33% present during work day. The rest majority of sheriff patrol will be in the field during the work day Sheriff patrol staff takes cars home, thus personal vehicles are not left on site all day, thus not further burdening the parking demand.....**28 spaces of demand**
- Sheriff patrol vehicles, assume 6 would be parked on-site at peak times.....**6 spaces of demand**
- Visitors, consider providing 4 spaces for visitors/deliveries.....**4 spaces of demand**
- Jury pools called for voir dire, totals 62 people, since these are not a regular occurrence, and currently no dedicated parking is provided – those called for Jury Duty can and do currently park on-street or are encouraged in the Town Parking garage 2 blocks West.....**0 spaces of demand**

Total for County Operations (Justice Center + General Service), 91 spaces.

Town Hall Parking Demand

(Employee Counts provided by Tyler Valentine February 25, 2025)

Employees - 22 staff people plus 5 (Mayor and Council (occasional count as 1/5th of the time parked vehicle), assume 15% use non-auto modes.....**20 spaces of demand**

Police, 38 total people. Assume 13 people, approximately 33%, are present during the work day while others are on patrol.....**13 spaces of demand**

Police Patrol vehicles assumed 4 would be parked on site at peak times (per Town's input).....**4 spaces of demand**

Visitors, Consider 4 spaces for visitors/deliveries.....**4 spaces of demand**

Total Demand for Town Hall Operations, 41 spaces.*

Town and County Total Parking Demand 132 Spaces

On-Site Parking Provided – 121 Spaces (Existing Spaces 109)

- See “Parking Utilization Data Table” and “Parking Analysis Conclusions” below justifying this provided parking count
- **51 Spaces** N/E Parking Lot (Including 9 Spaces Dedicated to TOJ Easement)*
- **9 Spaces** Town of Jackson Immediately South of Town Hall**
- **61 Spaces** – all other Teton County owned Lots on Campus

**The Town Hall Parking Demand Data is provided for reference as requested by the Town to be part of the Sketch Plan narrative. The Town Hall is not within the boundary or property line of the county owned property in question. The County has provided a parking easement to the Town, in the existing northeast parking lot allowing the Town to utilize the parking spaces up against the East side of the Town Hall (20 feet). Otherwise, all parking in both parking lots is solely parking for county vehicles and guests. At present this parking lot does not have signage nor is regularly monitored. The New Justice Center proposed conditions will not impact the parking spaces held by the Town up against the Town Hall on the East side, per the existing easement agreement.*

***Parking Spaces on the Town owned property immediately south of Town Hall are not on the county owned property in question. However, to maximize parking for the block the sketch plan submittal is proposing re-paving / restriping to increase the parking count at this particular location from 6 existing to 9 proposed.*

Parking Utilization Data Table

The parking demand calculations are estimated based on real world employee and visitor numbers. The actual building occupants will vary, depending on TOJ police operations and County Sheriffs operations and Court / Jury Trail Activity on any given day and so the other piece of data (see full Traffic Impact Study for more info) that is critical in demonstrating that current parking demand is met and exceeded by current supply is the parking utilization study and data collection table below. Parking lots around the site were 68% full on this representative data collection. This number can vary throughout the year, but the point remains that real world conditions recorded below must also be taken into account.

Table 3. Campus Parking Data (January 14th, 2025)

Parking Area	Counted Parked Vehicles	Approx. Space Capacity	Percentage Utilized
Town Hall	29	36	81%
Detention Facility	7	14	50%
Central Area	18	33	55%
South of Simpson Ave.	17	26	65%
Along Pearl Ave ¹	23	24	96%
Along Willow St. ^{1,2}	16	24	67%
Along Simpson Ave. ¹	20	25	80%
Along King St. ¹	9	20	45%
Totals	138	202	68%

1. Includes both sides of the roadway.

2. Parallel parking allowed in winter. Parking lanes are used as bicycle lanes during summer.

The parking on and around the site was 68 percent utilized on January 14th, 1:45 PM (plus or minus 15 minutes). The on-street parking along Pearl Avenue was the most highly utilized with essentially all of its parking being used. The businesses along the north side also generate parking along this roadway. The second highest utilizing was the Town Hall lot and the parallel parking along Simpson Avenue which were each 80 percent utilized.

While on-street parking along the adjacent roads serves the uses, it is not all being generated by campus uses. For purposes of this study, 50 percent was assumed to be associated with either the courthouse or Town Hall. Given that assumption and the parking counts in the lots, the courthouse and Town Hall collectively generated an estimated 104 parked vehicles.

Master Parking Analysis Conclusion

The estimated Parking Demand (131 Spaces) is not met by the off-street parking spaces provided at 121 Spaces. The two numbers are very close and it is important to keep in mind there is a margin of error in the parking demand estimations. However, when paired with the real-world data collection of parking utilization rates, the overall conclusion is that parking demand is met, on most average days, by parking to be provided by the project.

AndersonMasonDale

Tyler Valentine
PO Box 1687
Jackson, WY 83001

March 4, 2025

Dear Tyler Valentine,

In response to the Determination of Insufficiency for Sketch Plan at 180 S King Street, please see the written response to all items identified in the Pre-Application Conference below. Responses noted in red.

1. PARKING REQUIREMENT: As part of the CUP approval for the Teton County General Service building (P24-065), the following condition was approved requiring the General Services building to be included in the parking calculation as part of the Courthouse project:

“Determination of, and provision for, any newly generated off-street parking for the General Services Building shall be deferred until master planning of the municipal complex and construction of the proposed Teton County Courthouse project is complete.”

Parking in the P/SP zone requires an independent calculation which puts the burden on the application to propose the appropriate amount of parking to accommodate all uses, including Town Hall, the Hansen Courthouse, and the General Services Building. There appears to be a total of 117 proposed on-site parking spaces. A detailed parking analysis will be required which provides an inventory of the parking demand/needs for all existing/proposed uses. At the pre-application meeting, there should be a discussion over whether street parking spaces should be credited toward the development similar to other downtown properties on the DC, CR and TS zones.

Please refer to the Teton County Justice Center Narrative Description for the independent parking calculation and detailed parking analysis. Since the pre-application submission the total proposed on-site parking spaces has been updated to 120 spaces.

2. FLOOR PLANS / MAIN ENTRANCE – it appears the main entrance is moving from King St to Simpson Ave. The floor plans do not seem to address the King Street frontage well, in fact the first floor shows storage/Mechanical spaces up against King Street which is not ideal. These types of conditioned spaces are better served off the alley and would create a better street frontage along King Street.

Since the pre-application submission, the main entrance has been moved from Simpson Ave to King Street. The Design Team agreed that the entrance on Simpson Ave did not address the King Street frontage. Rearranging the main entrance to King Street will provide generous public frontage as well as an enhanced civic arrival experience. Additionally, this new layout keeps the public entrance approximately where the current courthouse entrance is located to maintain the public familiarity with building access. Please refer to Sheets A101 – A103.

3. PEDESTRIAN FRONTAGE TYPE - The pedestrian frontage type on South King and East Simpson will likely be Trees in Grates. There is a potential for Trees in Lawn along East Simpson.

The design team has proposed Trees in Grates along King Street to match the current frontage type. We have proposed Trees in Lawn along Simpson Ave as this Frontage type is more adaptive to the grade change along Simpson Ave. Please refer to Sheets L-301 and A-001.

4. BUILDING HEIGHT/BULK/SCALE – The P/SP zone is a flexible zone when it comes to setbacks, FAR, building height, etc. because it serves a wide variety of public-serving and institutional uses. Although flexibility is provided, the design, architecture, building height, and massing should be done in consideration of the surrounding built environment, and in consideration of the Town Council/community's recent sensitivity to large buildings. Staff is aware that the project is very conceptual in nature, however, staff has concerns over a proposed 60' building with another 18' mechanical screen wall for a total height of 78'. If the applicant plans to propose a building this tall, significant design efforts will need to be made to break up the massing through additive/subtractive architectural moves, third-floor step-backs, appropriate materials changes that align with changes in plane, etc. The applicant should also explain why the first and second floors are each over 18' tall and why the third floor is over 23' tall.

Since the pre-application, the Design Team has developed the exterior massing. Additive and subtractive architectural moves have been introduced. Along King Along Simpson Ave, Level 3 has been pushed back to reduce the mass. We have placed the courtrooms at the center of the building, to keep the high-volume spaces stepped back from the primary façade. The material palette will be carefully selected to align with the stepbacks. We will focus on materials that create texture and visual separation as the building transitions between floors.

The first to second floor is over 18' tall to provide 14'-0" minimum clear ceiling height at the Level 1 Sally Port to accommodate an ambulance. The second to third floor, and the third floor to low roof heights have been reduced to 16'-0". To reduce the mass of the buildings on Simpson Ave and King Street, we have limited the extents of the high roof to above the courtrooms only. The high roof will also serve to screen mechanical units from Simpson Ave.

Please refer to Sheets A-201 through A-215. Please refer to the Teton County Justice Center Narrative Description for additional information on the massing and contextual response.

5. CONSTRUCTION STAGING / TEMPORARY OFFICES – What is the plan for construction staging and displacement of office space?
Please see the Conceptual Construction Staging plan. A temporary site gate to be installed at the North Alley entrance at King Street. Dumpsters to be placed to the north of Hansen Courthouse. Construction trailer and lay down space to be located in the parking lot to the south of Simpson Ave. Pedestrian access will be re-routed to the south side of Simpson Ave.
6. PARKING EASEMENT – The applicant will need to provide a response on whether they are proposing any changes that would impact or require re-negotiating the parking easement between the Town and County.
The applicant is not proposing any changes that would require re-negotiating the parking easement between the Town and County. We are proposing that the NE parking lot be re-striped. However, the parking stall count within the existing parking easement will remain unchanged.
7. ALLEY EXTENSION – At this time staff is not prepared to provide a recommendation of support or opposition to the proposal to extend the alley east to Willow. As part of the Sketch Plan, this topic will be a key issue, and one of the main considerations to support or not support this extension will be the South Willow bike lane which is part of the Safe Routes to School system. Please keep in mind that the Town Council recently denied a proposed access off Willow as part of the General Services Building CUP because they were concerned the new access would conflict with the bike route. Since the proposed alley extension would significantly increase the amount of traffic entering/exiting the property from Willow, concerns over the bike path will continue to be a leading key issue. If the Town Council allows the alley to be extended east to Willow, the alley should be aligned to match the existing alley. As shown on the proposed site plan, the alley drive aisle shifts slightly north due to a landscape island just north of the secured sheriff parking. This island should be shifted south so that the alley is continuously straight.
The County and Justice Center leadership fully supports efforts to protect pedestrians, especially children and families using the Willow Street Safe Routes to School bike and pedestrian pathways.

Since the pre-application submission, the proposed site plan has been revised to remove the alley extension to Willow Street. The Alley will be Two-way from the Secure Sheriff's yard northern gate to the east, accessing the N/E parking lot adjacent to Town Hall and continuing with access to Pearl St. One-way Alley (outbound) east from the Secure Sheriff's yard out onto King Street, along the

north side of the Justice Center. Consistent with today's condition there will be no entry into the Alley from King St.

8. EXISTING WILLOW ACCESS – Similar to the comments above, staff is not prepared to provide a recommendation of support or opposition to keeping this access. Removal of this access would result in 3-4 additional parking spaces and would still allow the secured parking lot to be accessed from the alley. The applicant will be tasked with justifying why this curb cut is essential to the function of the Secured Sheriff's parking lot. Ultimately the Town Council will decide on his access.

Vehicle access on Willow Street remains essential for the Sheriff's Office to perform its public safety duties. A secure yard for the Sheriff's Office requires two access points onto 2 different rights-of-way, and site constraints prevent access from Simpson Street. Therefore, access must be provided from King Street and Willow Street. If an event were to occur in the alley, and vehicle access was blocked then Sheriff vehicles could still exit the secure lot via Willow.

To ensure pedestrian and bike safety on Willow St. This drive will have limited use by Sheriff's personnel only. There will be no public access on this driveway to Willow St. The fence, gate and paving will have pedestrian bike crossing indicators and marked signs, making those operating vehicles aware and diligent looking for potential pedestrians and bikes. The fence and gate will be located to maintain clear site lines for drivers allowing visibility up and down Willow St. for oncoming pedestrians and bikes.

9. BIKE PARKING STANDARDS – LDR Section 6.2.2.D addresses the Town's new bike parking standards that will apply to this development. The new standards require a percentage of required bike parking to be short-term and long-term. Office and Assembly uses have different standards and thus the applicant should work early on with Town staff to determine the correct requirement.

Please refer to Sheet L-301 for the bike parking calculation table in accordance with LDR Section 6.2.2.D. 18 short-term bike parking spaces and 7 long-term bike parking spaces are to be provided. Long-term bike parking spaces are provided at Level 1.5, refer to Sheet A101.5. Within the bike parking total, the Design Team has accommodated 25% for the larger and alternative bicycles.

10. PARKING DIMENSIONS – All uncovered surface parking and drive aisle dimensions shall comply with LDR Section 5.2.5.D. For covered parking spaces, such as those underneath the new building, reduced parking and drive aisle dimensions are allowed per the attached Resolution 09-25.

Parking and drive aisle dimensions have been indicated on Sheet A-001 to comply with LDR Section 6.2.5.D. The standard parking space width is 9 feet. The standard surface parking space length is 20 feet. Parking spaces accessed

perpendicular to the alley are 22 feet in length. The parking spaces at the south side of the secure lot are 18 feet in length, as an additional 2 feet in length is provided for the front overhang. The standard drive aisle is 24 feet.

11. TRASH – A Trash enclosure will be required for the new Justice Center that complies with LDR Section 6.4.2. In addition, the Town’s trash enclosure south of the building is not shown on the site plan. Is this an omission or on purpose? **A trash enclosure is proposed in the northeast corner of the Secure Parking lot, with truck access from the North Alley. Up to (4) Trash and recycling dumpsters may be housed within the enclosure. Smaller recycling bins may be housed in the Level 1 Secure Parking area. Proposed trash and recycling dumpster sizes, and enclosure based on current waste volume of Courthouse, Detention Center and Hansen Courthouse. The existing Town trash enclosure to the south of Town Hall to remain, the fence line has been indicated on the Sheet A-001 and C3.0. The county would propose that the trash enclosure in the northeast corner of the Secure Parking lot be a shared enclosure for Town Hall and County.**
12. EVSE STANDARDS – LDR Section 6.2.2.F addresses the Town’s new Electric Vehicle standards that will apply to this development. Essentially, whatever parking amount is required for the Justice Center, 30% of spaces shall be EVSE capable (i.e., conduit running to/from the spaces to the power source) and 5% shall be EVSE installed. These spaces shall be identified on the site plans for the Development Plan.
Per LDR Section 6.2.2 for Other Nonresidential use, 5% of total parking spaces will be EVSE installed and 30% will be EVSE capable. 6 stalls to be EVSE installed and 36 stalls to be EVSE capable. Refer to Sheet A-001 for EVSE parking stall information.
13. SNOW STORAGE – LDR Section 6.2.5.C addresses snow storage. The applicant shall demonstrate the locations of snow storage that comply with the 2.5% rule. Essentially, 2.5% of all uncovered paved surface areas (i.e., pedestrian walkways, drive aisles, parking, etc.) shall be dedicated to snow storage.
A minimum site area representing 2.5% of the total required off-street parking and loading area, inclusive of drive aisles and access drives, shall be provided as the snow storage area.
Sheriff’s Secure Parking Lot and associated driveway:
 - Paved Area: 11,200 SF
 - 2.5% Snow Storage; 280 SF (equivalent to 1.5 Parking Spaces)
 - Snow Storage will occur inside the secure yard on the north end of the site taking up two parking spaces in the winter months.NE Parking Lot and associated driveways
 - Paved Area: 20,200 SF
 - 2.5% Snow Storage; 505 SF (equivalent to 2.75 Parking Space)
 - Snow Storage will occur in NE parking lot taking up three adjacent parking spaces in winter months.

14. EXTERIOR LIGHTING – This zone is not exempt from exterior lighting standards. All exterior light fixtures shall comply with LDR Section 5.3.1. Per the LDRs, all exterior fixtures that exceed 600 lumens shall be shielded at 90 degrees. The maximum lumens for shielded fixtures is 3 lumen per 1 square foot of site development not to exceed 50,000 lumens per acre of site development. Fixtures less than 600 lumens may be unshielded, but the total lumens allowed cannot exceed 2,000 lumens per acre of site development.
Street lighting to be coordinated with Electrical Engineer and TOJ Public works for spacing and location. Standard TOJ street lighting specifications and details will be followed. Site photometrics will be provided to review the exterior lighting.
15. PARKING LOT SOUTH OF SIMPSON: What is the intended use for the parking lot south of East Simpson? Is that mostly for staff parking or will it be for public use?
The parking lot south of Simpson Ave will be intended for the Justice Center visitor parking. Elected officials and Sheriff fleet vehicles will be parked in the east secure parking lot and the Level 1 building parking lot.
16. SIGNAGE - All signage is approved separately through a sign permit. However, a preliminary signage plan shall be provided with the Development Plan. The plan shall identify locations and types of directional and wayfinding signage. The general public views this site as one and often public members come into Town Hall looking for the existing courthouse. The applicant should consider how pedestrian access through the site can be maintained or provided.
Acknowledged, a preliminary signage plan will be provided with the Development Plan. Exterior elevation drawings in the sketch plan show conceptual location of building identification & wayfinding signage at the primary entry of the building along King St.

Teton County Justice Center

180 S King Street
02.05.2025

Permit Number	Begin Date	Step Type	Town of Jackson Comment	Created By	Design Team Response By	Design Team Response
P24-125			BUILDING DEPARTMENT COMMENTS Structure must meet the most current adopted Building Codes of the Town of Jackson.		KLA	Acknowledged. Structure will meet current Town of Jackson adoption of IBC 2021 and associated design criteria.
P24-125			ENGINEERING DEPARTMENT COMMENTS Pre-Ap Comments - INSUFFICIENT P24-125 ADDRESS: 180 S King St. OWNER: Teton County General Services APPLICANT: Berning Project Management 9/18/2024 Jeff Silliman, 733-3079 DATE OF SUBMITTAL: 9/18/2024 DATE OF MATERIALS: June 26, 2024 The engineering division has reviewed your application for a [PERMIT TYPE] submitted on and with application materials as dated above. *The following comments are being provided for use in preparation of future applications and are required for sufficiency. PROJECT SPECIFIC COMMENTS: 1. Replace LVE street light on Simpson/King.		-	-
			1. Replace LVE street light on Simpson/King.		Jorgensen	Street lighting to be coordinated with Electrical Engineer and TOJ Public works for spacing and location. Standard TOJ street lighting specifications and details will be followed.
			2. Provide EV car charging spaces.		AMD	Per Town of Jackson Land Development Requirements, Division 6.2.2 for <i>Other Nonresidential</i> use, 5% of total parking spaces will be EVSE installed and 30% will be EVSE capable. Refer to Sheet A-001 for EVSE parking stall information.
			3. Show snow storage and stormwater management. May consider removing the security gate on Willow and gain more parking spaces and snow storage.		Jorgensen	Snow storage and stormwater management will follow TOJ regulations. Schematic Design has not progressed enough to include these two items. The difference between the pre and post development runoff will be stored on site (likely with stormtech chambers).
			4. The sheriffs parking access off Willow may not be necessary and may get flagged the way GSB access did.		Jorgensen	A Traffic Impact letter is included in the Sketch Plan submission to help facilitate the analysis of the Willow St access. More information and conversations with the TOJ to come. The County and Justice Center leadership fully supports efforts to protect pedestrians, especially children and families using the Willow Street Safe Routes to School bike and pedestrian pathways. However, vehicle access on Willow Street remains essential for the Sheriff's Office to perform its public safety duties. A secure yard for the Sheriff's Office requires two access points, and site constraints prevent access from Simpson Street. Therefore, access must be provided from King Street and Willow Street.

			5. Water to this development is off Simpson Ave.		Jorgensen	Correct. Civil currently shows a new service connecting to the water main in Simpson Street. It is expected the existing service will not have the demand for the proposed building and fire suppression system
			6. Show pedestrian improvements. a. Remove steps on Simpson and match sloped sidewalk across the street.		Jorgensen	Civil design is matching the slope of Simpson street similar to the sidewalk across the street. Existing steps will be removed
			7. Town has easement on the sewer running north/south. Town cannot give up the easement as shown until there is a plan to reroute the sewer.		Jorgensen	Three sewer options (plan view only) are provided in this submission, refer to Civil drawings and Sheet A-001. Further analysis and coordination to happen with TOJ Public Works for reroute of the sewer and vacate of the easement. Additionally the proposed options for relocating the sewer lines will be reviewed with the selected Construction Manager/General Contractor (CMGC) to establish costs associated with the new sewer line to finalize the design of this work.
			8. Show water entry on Simpson, adjacent to the tap on the main, not on King. TOJ CODE A construction-staging narrative shall be submitted for review and approval with the Development Plan application.		Jorgensen	Water entry for the proposed Courthouse will come from the Simpson Ave main.
			A construction-staging narrative shall be submitted for review and approval with the Development Plan application.		Wember/BPM/County	A CMGC has been brought on board and will provide a construction staging narrative for TOJ review.
			Plans provided shall show all work to be completed within the Towns right-of-way.		Jorgensen	Civil plans will show the limits of disturbance which will include all work within the public ROW
			Show all encroachments into the Towns right-of-way or easements. Encroachment agreements are required for encroachments of buildings, retaining walls, foundations, canopies, balconies, roofs, shoring, etc.		AMD/KLA	No buildings, foundations, canopies, balconies, roofs or shoring will encroach upon the Town's right-of-way or easements. There are proposed site walls along Simpson Ave, adjacent to the Sheriff Entry, that are within the Town right-of-way. The proposed site walls serve as vehicle protection. Please refer to Sheet A-001 for the proposed site walls.
			A demolition permit is required for each existing structure to be removed from the site. Water and sewer services to be abandoned for the project shall be abandoned at the main during the demolition phase of the project.		AMD/Jorgensen	Civil will provide a demo plan and information for the abandonment of water and sewer services at the main per TOJ regulations. Architecture will provide a demo plan for existing structures to be removed.
			LAND DEVELOPMENT REGULATIONS Development shall comply with the physical development standards for its zoning.		-	-
			5.7 GRADING, EROSION CONTROL, AND STORMWATER 5.7.1 Grading Permit Required: Provide information on how the application will comply with this section, including the Geotechnical Report.		Jorgensen	TOJ Grading Plan and application will be provided.
			5.7.2 Grading Standards: Provide a grading plan that shows compliance with this section. Plan shall include existing and proposed site contours with elevation labels, spot elevations, high and low points, grade breaks. Provide easements for grading proposed on adjacent properties.		Jorgensen	Civil plan to include the necessary grading elements to meet TOJ standards.
			5.7.3 Erosion Control Standards: Provide an erosion control plan that shows compliance with this section.		Jorgensen	Civil plan to include the necessary erosion control elements to meet TOJ standards.
			5.7.4 Stormwater Management Standards: Provide an erosion control plan that shows compliance with this section.		Jorgensen	Civil plan to include the necessary stormwater management elements to meet TOJ standards.
			6.2 PARKING AND LOADING STANDARDS 6.2.2 Required Parking and Loading: Provide a plan that shows the proposed parking in compliance with this section. Include location and dimension of parking spaces, accessible spaces, including van accessible spaces, and bicycle parking. For uses that require deliveries, show the proposed loading facilities.		AMD	Please refer to Sheet A-001 for parking and loading plan and calculations, including dimensions, accessible spaces, and bicycle parking.
			6.2.5 Off-Street Parking and Loading Design Standards: Provide a parking plan in compliance with this section. Include surface materials and drainage plans, access and circulation, and snow storage.		AMD/Jorgensen	Civil to provide surface material, cross section details, and snow storage for parking.
			6.4 OPERATIONAL STANDARDS Provide a Construction Management Plan: https://www.jacksonwy.gov/DocumentCenter/View/7098/Checklist-CONST-MGMT-PLANv4		Wember/BPM/County	A CMGC has been brought on board and will provide a construction management plan for TOJ review.
			7.2 SUBDIVISION STANDARDS 7.2.2 Standards Applicable to all Subdivisions 7.2.2.A Subdivision Improvements:		-	-

			1. Provide plans and specifications for roads, streets, alleys, sidewalks, and pathways including street light, signage, and markings.		Jorgensen	Civil specifications will include these elements.
			2. Provide a sanitary sewer wastewater plan.		Jorgensen	Sewer plan and sewer TOJ details will be provided.
			3. Provide a water supply plan.		Jorgensen	Water supply plan and standard TOJ details will be provided.
			4. Provide a storm drainage plan, basins, calculations, treatment method. Stormwater discharged to a public storm sewer or right of way requires treatment. Wyoming DEQ 5D2 permits are required for subsurface discharges.		Jorgensen	Stormwater management plan, calculations, etc. will be provided.
			5. Provide an irrigation plan or statement.		Limegreen	All plantings will be irrigated with an automatic irrigation system.
			6. Provide a plan for all other utilities, such as telephone, cable TV, electric, fiber, gas.		Jorgensen	Utility plan will be provided.
			7. Provide a plan for permanent reference monuments, property corners, etc.		Jorgensen	Property corners, etc. will not change. If a monument is disturbed during construction, it will be reinstalled or replaced by survey department.
			8. Permits are required prior to construction of development improvements.		AMD/Jorgensen	Acknowledged. All required permits will be acquired prior to construction of development improvements.
			9. All improvement shall be designed by a professional engineer licensed to do such work in the State of Wyoming.		Jorgensen	Acknowledged, all improvements shall be designed by a professional engineer licenses to work in the State of Wyoming.
			10. The Town Council may require installation and construction of utilities, pavement and other land improvements in excess of subdivision design needs, to assure adequate service to future development areas.		Wember/BPM/County	Acknowledged.
			7.7 REQUIRED UTILITIES 7.7.2 Potable Water Supply:		-	-
			1. Provide a water supply plan and estimated average day, maximum day, and maximum hour, required fire flows, and per capita maximum daily demands. Demands shall be determined by one of the following: Wyoming DEQ Chapter 12 Section 8 (equivalent per capita water use shall be at least 125 gpd and 340 gpd for average and maximum day respectively); Wyoming DEQ Chapter 25 Tables 1 and 2 with consumption and irrigation factored in, metered water supply data from another development where similar water demands have been demonstrated, AWWA M22 method, or other Town Engineer approved source.		Jorgensen	Further information to be provided with the water supply plan after schematic design.
			2. Provide right-of-way or easements as required, 30 feet minimum width with 10 feet minimum to either side.		Jorgensen	Acknowledged. Potential easements to be determined
			3. Provide a water system analysis indicating the required domestic and fire flow demands. Identify impacts to or upgrade requirements for the existing distribution, supply, or treatment system.		Jorgensen	Further information to be provided with the water supply plan after schematic design.
			4. Show compliance with state regulations, construction standards, connections for lots of record, provisions for system expansion, and fire protection. Provide information on planned metering and backflow prevention locations.		Jorgensen	TOJ construction standards will be adhered to. Design Team will provide information on planned metering and backflow prevention locations.
			5. State whether the water system will be privately or publicly owned. Water services over 2-inch in diameter require engineering design. Water mains larger than 8-inch and or longer than 250 feet require a Wyoming DEQ permit.		Jorgensen	Only a private service from the public Simpson Ave main will be installed.
			7.7.3 Sanitary Sewer System:		-	-
			1. Provide a sanitary sewer wastewater plan and estimated average day, maximum day, and peak hour for the design of the project, per capita design flows, extraneous flows, and industrial and/or commercial waste volumes. Volumes shall be determined by one of the following: Wyoming DEQ Chapter 25 Tables 1 and 2; metered water supply data from another development where similar water demands have been demonstrated; or other Town Engineer approved source.		Jorgensen	Further information to be provided with the sewer plan after schematic design.
			2. Provide right-of-way or easements as required, 30 feet minimum width with 10 feet minimum to either side.		Jorgensen	Acknowledged. Potential easements to be determined.
			3. Provide a sanitary sewer analysis. Identify downstream impacts on existing sewers, lift stations, and treatment facilities.		Jorgensen	Further information to be provided with the sewer plan after schematic design.

			4. Show compliance with state regulations, construction standards, maximum allowable infiltration, connections for lots of record, provisions for system expansion.		Jorgensen	Further information to be provided with the sewer plan after schematic design.
			5. State whether the sewer system will be privately or publicly owned.		Jorgensen	Sewer will be a private service tying into the public TOJ system.
			7.7.4 Irrigation System:		-	-
			1. Provide an irrigation plan when the system is for the entire development or the system will be owned by the Town. For all systems provide the irrigation water demand, type of backflow preventer, location, and whether irrigation will be metered separately from domestic.		Limegreen	An irrigation system will be designed starting in Design Development. The design will include calculations for water demand, plans showing backflow location, controller location, and water meter.
			2. Show compliance with surface water rights (if applicable), irrigation surface water runoff, setbacks to ditches, and access to ditches.		Jorgensen	Not applicable at this location.
			3. Show compliance to standards for any alteration of an irrigation ditch.		Limegreen	Not applicable to the project. The project scope will not alter any existing irrigation ditches.
			4. A Wyoming Department of Transportation license is required for irrigation systems and landscaping located within the WYDOT right-of-way.		Limegreen	Not applicable to the project. The project irrigation system and landscaping is not located within the WYDOT right-of-way.
			7.7.5 Other Utilities:		-	-
			Provide a plan for all other utilities, such as telephone, cable TV, electric, fiber, gas. All utilities shall be installed underground. Provide right-of-way or easements as required. Show that private utilities can be located on private properties, e.g. transformers.		Jorgensen	Utility plan will be provided.
11/11/2024			POST MEETING ADDITIONAL COMMENTS	Brian Lenz	-	-
			1. Build the building to the public way, don't adjust the public way to the building. Use the existing street and curb grades and design back into the building, maintaining a consistent plain complying with ADA standards. Do not use the buffer area as a grade break, except that along Simpson Ave. along the hillside grades may be adjusted WITH Town Engineering coordination.		Jorgensen	Finish floors and entry to the building is being designed to accommodate the existing ROW grades and ADA standards. The buffer will not be used as a grade break unless there is TOJ coordination.
			2. Provide the street sections of King Street and Simpson Avenue showing the existing street components relative to the right-of-way boundary and the Community Streets plan. Adjusting the curb lines may be warranted to bring the street up to a current section.		Jorgensen	Street section of King Street and Simpson Avenue will be provided.
			3. Review the corner of King and Simpson with Town Engineering, Pathways, and Planning, for a possible bulb out.		Jorgensen	Although this sidewalk and right-of-way is outside the boundary of the project, if funds allow the County will consider removal and replacement as part of the project. This portion of work will be considered at additive alternate. A Traffic Impact letter is included in the Sketch Plan submission to help facilitate the analysis of King and Simpson. More information and conversations with the TOJ to come.
			4. Depending on exterior lighting of the building, a street light at the corner may be warranted at the corner and the alley.		Jorgensen/CRA	Site photometrics will be provided to review the exterior lighting.
			5. The sidewalk east of the existing entrance needs to be removed and replaced with a compliant sidewalk as part of the project.		Jorgensen	Although this sidewalk and right-of-way is outside the boundary of the project, if funds allow the County will consider removal and replacement as part of the project. This portion of work will be considered at additive alternate.
			6. Verify the location of water and sewer services for the Clifford Hansen courthouse and plan for relocation and abandonment if conflicting with the new structure.		Jorgensen	Location of water and sewer services for Hansen courthouse indicated on the property survey. Relocation and/or abandonment to be determined based on new service line locations.
			7. Review whether vehicular access can be provided to the new GSB maintenance shop from the parking lot.		Jorgensen, Wember/BPM/County	There is no current plan to provide vehicular access to the new GSB addition via the Secure parking lot.
			8. How will trash and recycling be handled for the new facility.		Wember/BPM/County	A trash enclosure is proposed in the northeast corner of the Secure Parking lot, with truck access from the North Alley. Up to (4) Trash and recycling dumpsters may be housed within the enclosure. Smaller recycling bins may be housed in the Level 1 Secure Parking area. Proposed trash and recycling dumpster sizes, and enclosure based of current waste volume of Courthouse, Detention Center and Hansen Courthouse.
			9. Provide for a trash and recycling enclosure for town hall. Coordinate with Town Planning and Facilities.		Wember/BPM/County	The county would propose that the trash enclosure in the northeast corner of the Secure Parking lot, be a shared enclosure for Town Hall and County.

			10. Provide elevated sidewalk crossings at all driveways and alleys.		Jorgensen	Jorgensen will provide elevated sidewalk crossings at driveways and alleys.
			11. Provide information and clarification on how the parking lot east of Town Hall is and will be administered and maintained.		Wember/BPM/County	The parking lot east of the Town Hall will continue to be administered as a shared County & Town lot. The 9 parking stalls in the the 20'-0" parking easement along the west side of the lot will remain.
			12. Remove all vehicular access to Pearl preferred, or minimize to one access. (CORRECTION: 12. Remove all vehicular access to Willow preferred, or minimize to one access.)		Jorgensen	A Traffic Impact letter is included in the Sketch Plan submission to help facilitate the analysis of the Willow St access. More information and conversations with the TOJ to come. The County and Justice Center leadership fully supports efforts to protect pedestrians, especially children and families using the Willow Street Safe Routes to School bike and pedestrian pathways. However, vehicle access on Willow Street remains essential for the Sheriff's Office to perform its public safety duties. A secure yard for the Sheriff's Office requires two access points, and site constraints prevent access from Simpson Street. Therefore, access must be provided from King Street and Willow Street.
			13. Review the overhead utilities in the alley. Verify that adequate clearance is provided. If not, plan to bury the utilities, rather than raising them. Burying utilities may provide more room for two way traffic in the alley.		Jorgensen	Overhead utilities in the Alley will be reviewed to ensure adequate clearance is provided. Burying utilities is an option that will be explored in the next phase of design.

P24-125	FIRE DEPARTMENT COMMENTS					
			Project shall meet the requirements of the current International Fire Code (IFC) including but not limited to the following:		-	-
			1. The owner or owner's authorized agent shall be responsible for the development, implementation and maintenance of an approved written site safety plan establishing a fire prevention program at the project site applicable throughout all phases of construction. The plan shall be submitted and approved before a building permit is issued.		Wember/BPM/County	The owner's authorized agent will work with the selected CMGC to develop a site safety plan during the construction phases of the work.
			2. A stamped set of automatic sprinkler plans shall be submitted for review and approval prior to any work being done. A permit shall be required for sprinkler work. Inspections and testing shall be scheduled with this department.		AMD/CLA	Acknowledged. A stamped set of automatic sprinkler plans shall be submitted for review and approval prior to work being done. Inspections and testing shall be scheduled with the Fire Department.
			3. A stamped set of fire alarm plans shall be submitted for review and approval prior to any work being done. A permit shall be required for fire alarm work. Inspections and testing shall be scheduled with this department.		AMD/CLA	Acknowledged. A stamped set of fire alarm plans shall be submitted for review and approval prior to work being done. Inspections and testing shall be scheduled with the Fire Department.
			4. Applicable occupancies shall have an approved fire safety and evacuation plan per the IFC, Section 403, Emergency Preparedness and Section 404, Fire Safety, Evacuation and Lockdown Plans.		Wember/BPM/County	The design team to establish evacuation plans and that will be part of the signage package for the project. The owner's authorized agent will work with the County, and specifically the Sheriffs office, to establish evacuation and lockdown plans.
			5. Exterior doors and openings required by the IFC and the International Building Code (IBC) shall be maintained readily accessible for emergency access by the fire department. Building shall be provided with approved address identification and shall be legible and placed in a position that is visible from the street or road fronting the property. When access to a structure is restricted, a knox box shall be provided in an approved location.		AMD	Exterior doors and hardware will meet current Town of Jackson adoption of IFC and IBC 2021. Please refer to Sheet G-101 for proposed address identification, knox box, FACP and FDC locations.
			6. An approved water supply capable of supplying the required fire flow for fire protection shall be provided. Fire flow requirements shall be determined by an approved method.		CLA	A water supply capable of supplying the required fire protection flow rate will be provided. Fire flow requirements will be determined in design development.
			7. Elevator operation, maintenance and fire service keys shall be in accordance with the IFC, IBC and ASME A17.1 - Safety Code for Elevators/Escalators. Inspections and testing shall be scheduled with this department.		AMD	Acknowledged. Elevators will meet current Town of Jackson adoption of IFC and IBC 2021 and ASME A17.1. Inspections and testing shall be scheduled with the Fire Department.
			8. Commercial cooking equipment and systems in accordance with the IFC, Section 606. Inspections and testing shall be scheduled with this department.		AMD	Acknowledged. Commercial cooking equipment will meet current Town of Jackson adoption of IFC 2021. Inspections and testing shall be scheduled with the Fire Department.
			9. Interior finish, decorative materials and furnishings shall be in accordance with the IFC, Chapter 8 for occupancy type.		AMD	Acknowledged. Interior finishes, decorative materials and furnishings shall be in accordance with the IFC Chapter 8.
			10. Fire extinguishers installed for occupancy type in accordance with the IFC, Section 906.		AMD	Acknowledged. Fire extinguishers shall be installed per occupancy type in accordance with the IFC, Section 906.

			11. Means of egress in accordance with the IFC, Chapter 10 including minimum number of exits, maximum common path of egress travel distance specific to occupancy type, emergency lighting, emergency power, exit signs. Inspections and testing shall be scheduled with this department.		AMD	Please refer to Sheet G-102 for proposed means of egress, including number of exits, common path of travel, and occupancy type. Emergency lighting, power and exit sign locations shall be submitted for review and approval prior to work being done. Inspections and testing shall be scheduled with the Fire Department.
			12. Occupancy load posted in assembly occupancies.		AMD	Occupancy load signs shall be posted in assembly occupancies in accordance with IBC 2021, Section 1004.9. Please refer to notes on Sheet G-102.
			13. Meet requirements of NFPA 101 - Life Safety Code, Chapter 22 - New Detention and Correction Occupancies.		AMD	Acknowledged. The Detention and correction occupancies will meet the requirements of NFPA 101, Chapter 22.
P24-125	JOINT HOUSING DEPARTMENT COMMENTS					
			No comments - P/SP zone exempt from housing mitigation and no housing incentive tools proposed to be used		-	-
P24-125	PATHWAYS DEPARTMENT COMMENTS					
			Driveways/access points shall be constructed with continuous sidewalks (sidewalks shall remain at the top of curb elevation across the driveway).		Jorgensen	Agreed. Jorgensen to design sidewalks per TOJ standard details.
			Explore eliminating the curb cut on Willow and using the alley for access to and from the site interior.		Jorgensen	A Traffic Impact letter is included in the Sketch Plan submission to help facilitate the analysis of the Willow St access. More information and conversations with the TOJ to come. The County and Justice Center leadership fully supports efforts to protect pedestrians, especially children and families using the Willow Street Safe Routes to School bike and pedestrian pathways. However, vehicle access on Willow Street remains essential for the Sheriff's Office to perform its public safety duties. A secure yard for the Sheriff's Office requires two access points, and site constraints prevent access from Simpson Street. Therefore, access must be provided from King Street and Willow Street.
			Bike Parking shall comply with TOJ LDRs.		Limegreen	The design team proposes 18 short term spaces and 7 long term spaces. See Bike Parking Calculation Table on Sheet L-301.
			Look at angling the building face at the Simpson/King corner to provide better sight distance at the intersection.		AMD	Since the pre-app submission the primary building entry has been revised. The primary public entry is now at the corner of Simpson and King. The west setback has been increased to provide space for public circulation and improve the sight distance at the intersection.
			Staff recommends exploring a realignment of the sidewalk on Willow in conjunction with a protected bike lane and on-street parking.		Jorgensen, Wember/BPM/County	Although this sidewalk and right-of-way is outside the boundary of the project, if funds allow the County will consider realignment of the sidewalk on Willow as part of the project. This portion of work will be considered at additive alternate.
P24-125	PLANNING DEPARTMENT COMMENTS					
			Please see the pre-application checklist.		AMD	Acknowledged. The Project Team has been provided the pre-application checklist.
P24-125	PARKS AND RECREATION DEPARTMENT					
	08/15/2024			KSLUDER@JACKSONWY.GOV	-	-

P24-125	POLICE DEPARTMENT COMMENTS						
	08/27/2024			LPOTZ@JACKSON WY.GOV	-	-	-
P24-125	COMPLETENESS REVIEW						
	09/13/2024			BSCHILLING@JACK SONWY.GOV	-	-	-
P24-125	LEGAL DEPARTMENT COMMENTS						
	09/18/2024			KMALONE@JACKS ONWY.GOV	-	-	-
P24-125	START DEPARTMENT COMMENTS						
	09/18/2024			JSILLIMAN@JACKS ONWY.GOV	-	-	-
P24-125	PRE-APPLICATION MEETING						
	10/23/2024			TVALENTINE@JAC KSONWY.GOV	-	-	-



TETON COUNTY JUSTICE CENTER

TOJ: SKETCH PLAN

TETON COUNTY JUSTICE CENTER

MARCH 4TH, 2025

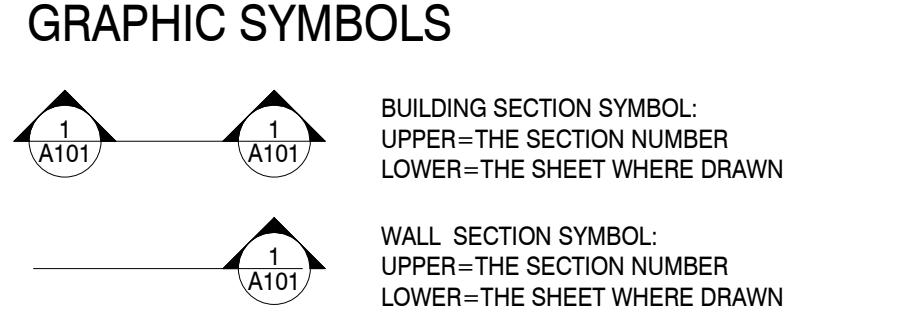
ABBREVIATIONS

NOTE: NOT ALL ABBREVIATIONS AND SYMBOLS ARE USED ON DRAWINGS CONTAINED IN THE SET. SYMBOLS AND ABBREVIATIONS ON THIS SHEET APPLY ONLY TO ARCHITECTURAL DRAWINGS

A/C	AIR CONDITIONING	FBO	FURNISHED BY OWNER	QTY	QUANTITY
ACF	ACCESS FLOORING	FD	FLOOR DRAIN	R	RESISTANCE, THERMAL, RISER
ACR	ACRYLIC	FEC	FIRE EXTINGUISHER CABINET	RAD	RADIUS
ACST	ACOUSTIC	FEX	FIRE EXTINGUISHER	RB	RESILIENT BASE
AD	AREA DRAIN	FIN	FINISH (ED)	RCP	REFLECTED CEILING PLAN
ADD	ADDENDUM	FLR(ING)	FLOOR (ING)	RD	ROOFING
ADH	ADHESIVE	FND	FOUNDATION	RE	REFERENCE, REFER TO
ADJ	ADJACENT	FOS	FACE OF STUD	RCINF	REINFORCING
AJST	ADJUSTABLE	FR	FIRE RESISTIVE	REM	REMOVE
AFF	ABOVE FINISHED FLOOR	FT	FOOT FEET	REQ	REQUIRED (D)
AHU	AIR HANDLING UNIT	FTG	FOOTING	RM	ROOM
AL ALUM	ALUMINUM	FUT	FUTURE	RO	ROUGH OPENING
ALT	ALTERNATE			ROW	RIGHT OF WAY
ANOD	ANODIZED	GA	GAUGE, GAGE	RR	RESTROOM
APC	ACOUSTICAL PANEL CEILING	GALV	GALVANIZED		
APPROX	APPROXIMATE (LY)	GB	GYPSUM BOARD	SBLK	SPLASH BLOCK
ARC	ARCHITECTURAL	GBAR	GRAB BAR	SC	SOLID CORE
ARG	ABUSE RESISTANT GYPSUM BOARD	GC	GENERAL CONTRACT (OR)	SCH	SCHEDULE
ASP	ASPHALT	GL	GLASS, GLAZING	SD	STORM DRAIN
AUTO	AUTOMATIC	GR	GRILLE	SEC	SECTION
AW	ALUMINUM WINDOW			SF	STOREFRONT
BD	BOARD	HB	HOSE BIB	SHT	SHEET
BLDG	BUILDING	HC	HOLLOW CORE	SHTH	SEETHING
BLKG	BLOCK (ING)	HDW	HARDWARE	SIM	SIMILAR
BM	BEAM	HM	HOLLOW METAL	SOG	SLAB ON GRADE
BO	BY OTHERS	HPT	HIGH POINT	SPEC	SPECIFICATION (S)
BOT	BOTTOM	HR	HOUR	SPL	SPECIAL
BR	BACKER ROD	HT	HEIGHT	SQ	SQUARE
BRG	BEARING	HTR	HEATER	SST	STAINLESS STEEL
BRK	BRICK	HVAC	HEATING/VENTILATION/AC	STC	SOUND TRANSMISSION CLASS
BS	BOTH SIDES	HW	HOT WATER	STD	STANDARD
BSMT	BASEMENT	HWH	HOT WATER HEATER	STL	STAIR
BTWN	BETWEEN	ID	INSIDE DIAMETER	STO	STORAGE
BUR	BUILT UP ROOFING	IN	INCH (ES)	STRUC	STRUCTURAL
BZD	BRONZE DOOR	INCAN	INCANDESCENT	SUS	SUSPENDED
BZF	BRONZE FRAME	INCL	INCLUDE (D, ING)	SYM	SYMMETRICAL
CAB	CABINET	INFO	INFORMATION	T	TREAD
CFM	CUBIC FEET/MINUTE	INS	INSULATE (D, IN)	T&B	TOP AND BOTTOM
CG	CORNER GUARD	INT	INTERIOR	T&G	TONGUE AND GROOVE
CIPC	CAST-IN-PLACE CONCRETE	INV	INVERT	TEL	TELEPHONE
CJ	CONTROL JOINT	JOINT		TEMP	TEMPERATURE
CL	CENTERLINE	JT		THK	THICK (NESS)
CLG	CEILING			THR	THRESHOLD
CLO	CLOSET	LAB	LABORATORY	THRU	THROUGH
CLR	CLEAR (ANCE)	LAM	LAMINATE (D)	TO	TOP OF
CMU	CONCRETE MASONRY UNIT	LAV	LAVATORY	TOB	TOP OF BEAM
CNC	CONCRETE COUNTERTOP	LB	POUND (S)	TCC	TOP OF CURB
CO	CLEANOUT	LIB	LIBRARY	TOI	TOP OF INSULATION
COL	COLUMN	LIN	LINEAL	TM	TOP OF MASONRY
COM	COMPRESSED (ED, ION, IBLE, OR)	LKR	LOCKER	TOP	TOP OF PARAPET
CONC	CONCRETE	LT	LIGHT	TOS	TOP OF SLAB
CONST	CONSTRUCTION	MANT	MANTAIN (ENANCE)	TOSTL	TOP OF STEEL
CONT	CONTINUOUS OR CONTINUE	MAS	MAS-BOARDY	TOW	TOP OF WALL
CONTR	CONTRACT (OR)	MAX	MAXIMUM	TPART	TOILET PARTITION
COORD	COORDINATE	MC	MECHANICAL CONTRACTOR	TPD	TOILET PAPER DISPENSER
CORR	CORRIDOR	MECH	MECHANIC (AL)	TR	TRANSOM
CORRUG	CORRUGATED	MET	METAL	TS	PIPE STEEL
CP	CEMENT PLASTER (STUCCO)	MFR	MANUFACTURE (ER)	TV	TELEVISION
CPT	CERAMIC PLASTER	MH	MANHOLE	TYP	TYPICAL
CS	CAST STONE	MIN	MINIMUM	T2	TERRAZZO
CSNK	COUNTERSUNK	MIR	MIRROR	UL	UNDERWRITERS LABORATORY
CT	CERAMIC TILE	MISC	MISCELLANEOUS	UNFIN	UNFINISHED
CTR	CENTERLINE	MO	MASONRY OPENING	UON	UNLESS OTHERWISE NOTED
CW	CURTAIN WALL	MR	MOISTURE RESISTANT	UR	URINAL
DBL	DOUBLE	MT	MOUNT (ED, ING)	VAR	VARIABLE (VARIES)
DEMO	DEMOLISH, DEMOLITION	MTL	MATERIAL (S)	VB	VAPOR BARRIER
DF	DRINKING FOUNTAIN	NA	NOT APPLICABLE	VCT	VINYL COMPOSITION TILE
DG	DECORATIVE GLAZING	NIC	NOT IN CONTRACT	VERT	VERTICAL
DIAM	DIAMETER	NO	NUMBER	VEST	VESTIBULE
DIMEN	DIMENSION	NOM	NOMINAL	VF	VERIFY IN FIELD
DISP	DISPENSER	NR	NONE REQUIRED	W	WIDE OR WIDTH
DM	DECORATIVE METAL	NTS	NOT TO SCALE	W/	WITH
DN	DOWN	OA	OVERALL	W/O	WITHOUT
DP	DEMOUNTABLE PARTITION	OB	OBSCURE	WC	WATER CLOSET
DR	DOOR	OC	ON CENTER (S)	WD	WOOD
DRY	DRYER	OD	OUTSIDE DIAMETER	WP (G)	WATERPROOF (ING)
DS	DOWNSPOUT	OFCI	OWNER FURNISHED CONTRACTOR INSTALLED	WR	WATER RESISTANT
DTL	DETAIL	OFOI	OWNER FURNISHED OWNER INSTALLED	WWF	WELDED WIRE FABRIC
DWG	DRAWING	OH	OVERHEAD		
EA	EACH	OPG	OPENING		
EC	EXPANSION CONTROL	OPH	OPPOSITE HAND		
EF	ENTRANCE FLOORING (GRILLES OR MATT)	OPP	OPPOSITE		
EFC	ENTRANCE FLOOR CARPETING	ORD	OVERFLOW ROOF DRAIN		
EJ	EXPANSION JOINT	ORD	OVERFLOW ROOF DRAIN		
EL	ELEVATOR	OZ	OUNCE		
ELEC	ELECTRIC (AL)	PAR	PARALLEL		
ELEV	ELEVATION	PC	PREFACET CONCRETE		
ENC	ENCLOSE (URE)	PERP	PERPENDICULAR		
ENT	ENTRANCE	PL	PLATE		
EPN	EPoxy PAINT	PLAM	PLATE LAMINATE		
EQ	EQUA	PLBG	PLUMBING		
EQP	EQUIPMENT	PNT	PAINT (ED)		
EWG	ELECTRIC WATER COOLER	PRN	PORCELAIN		
EXH	EVAPORATOR	PSF	POUNDS PER SQUARE FOOT		
EXIST. EX. (E)	EXISTING	PSI	POUNDS PER SQUARE INCH		
EXP	EXPOSED	PTN	PARTITION		
EXPN	EXPANSION	PVC	POLYVINYL CHLORIDE		
EXT	EXTERIOR	PVMT	PAVEMENT		
		PWD	PLYWOOD		

SYMBOLS LEGEND

GRAPHIC SYMBOLS



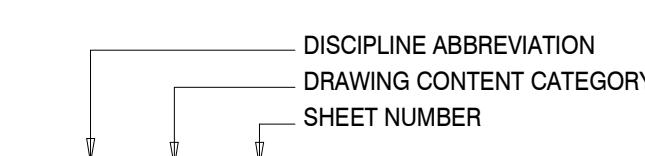
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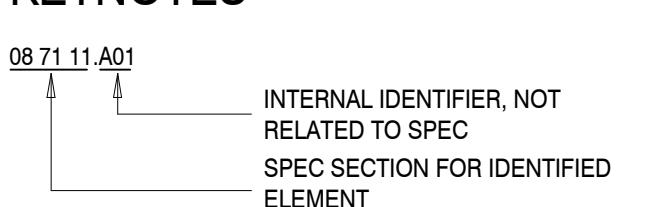
0	GENERAL INFORMATION AND STANDARDS
1	PLANS INCLUDING SITE, FLOOR, CEILING, ETC.
2	EXTERIOR ELEVATION AND BUILDING SECTIONS
4	ENLARGED FLOOR PLANS AND INTERIOR ELEVATIONS
5	SCHEDULES AND ASSOCIATED DETAILS
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7	INTERIOR DETAIL DRAWINGS
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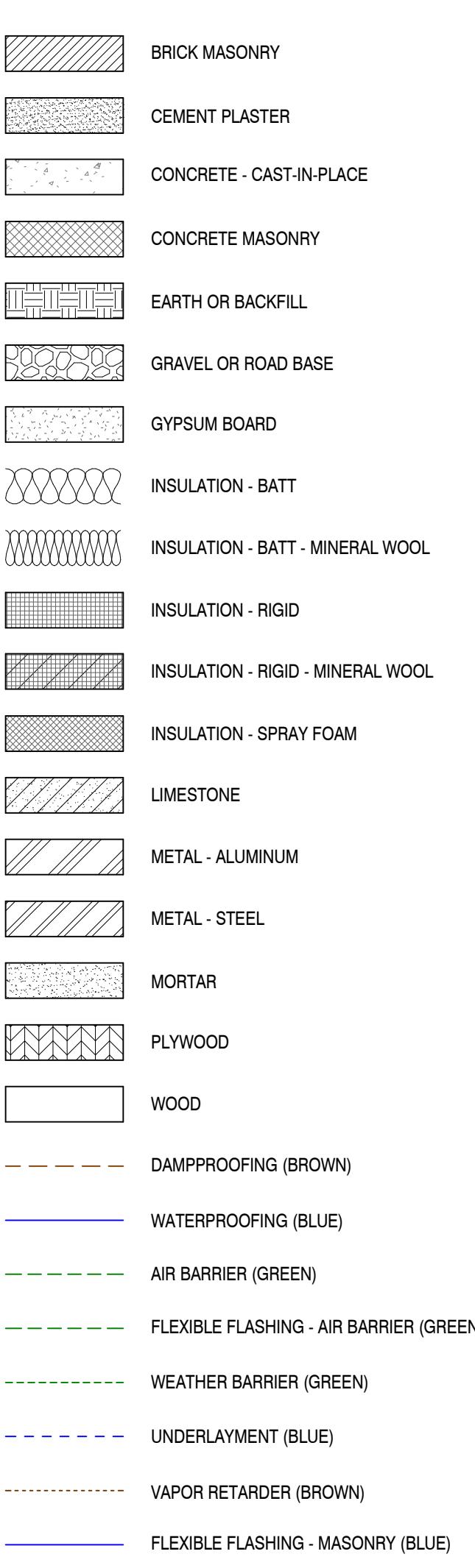


A-401

KEYNOTES



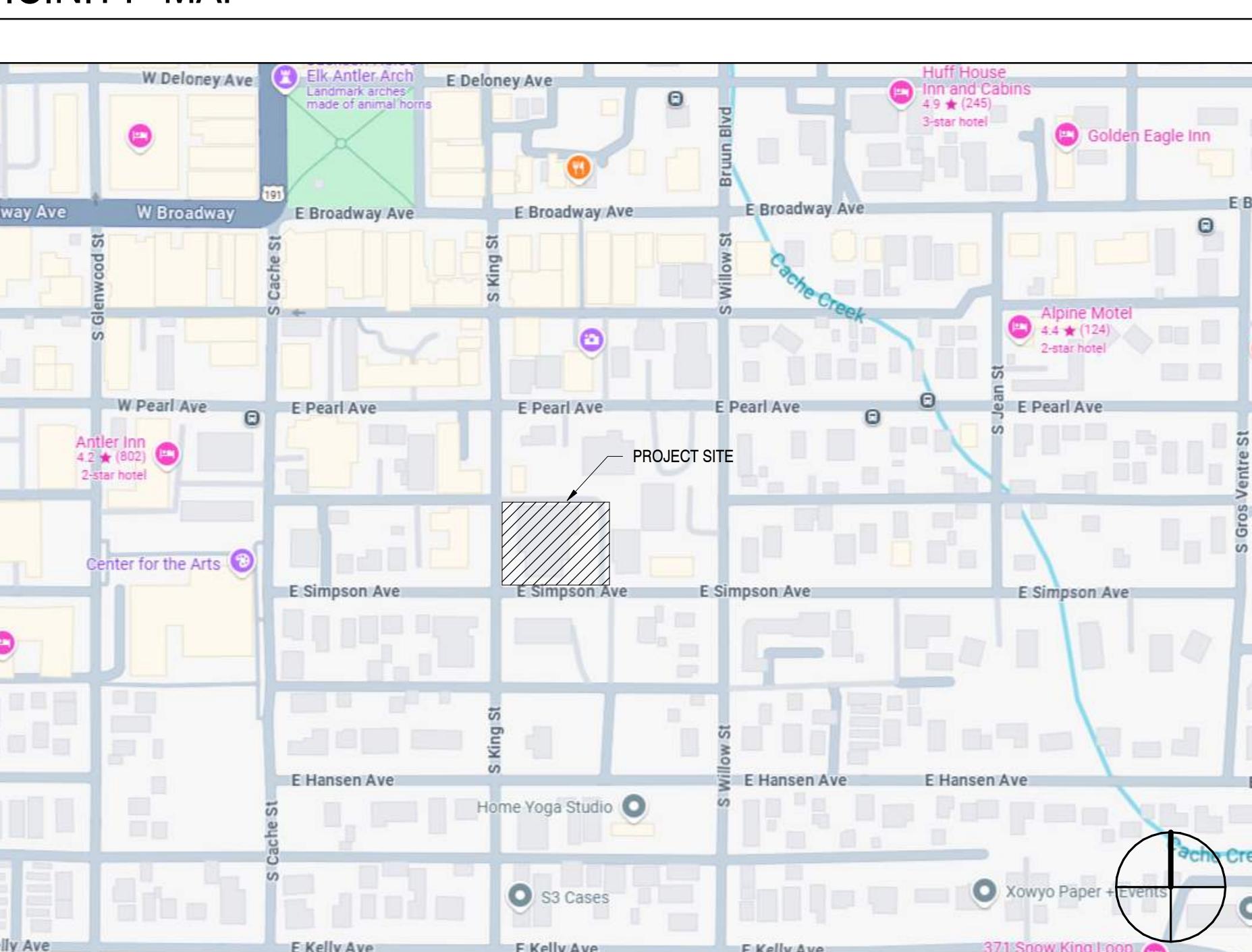
MATERIAL PATTERNS



TEXT SYMBOLS

&	AND
@	AT
X OR x	BY
#	POUND OR NUMBER
/ OR :	PER
X	(X) FEET
X'	(X) INCHES

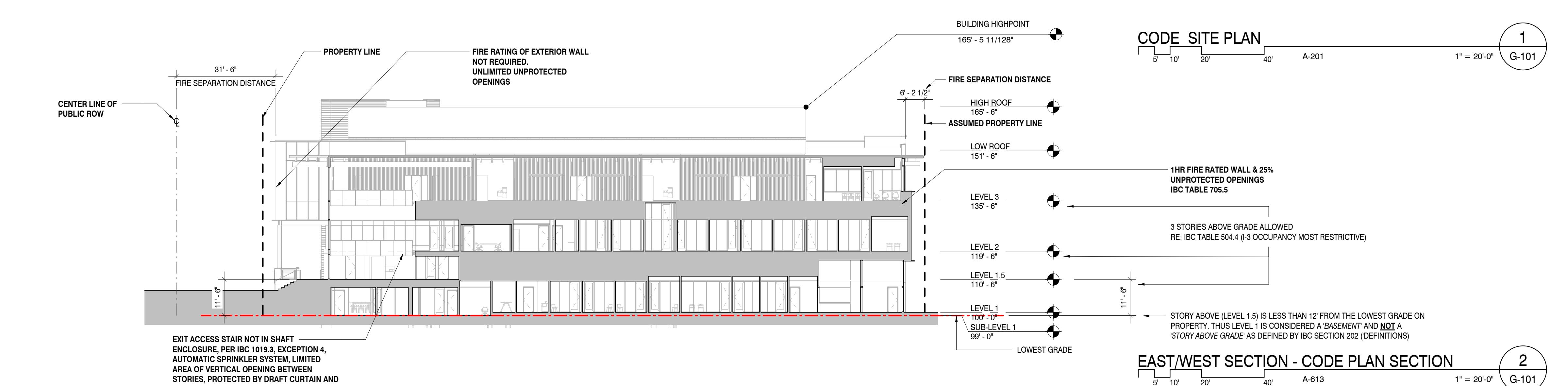
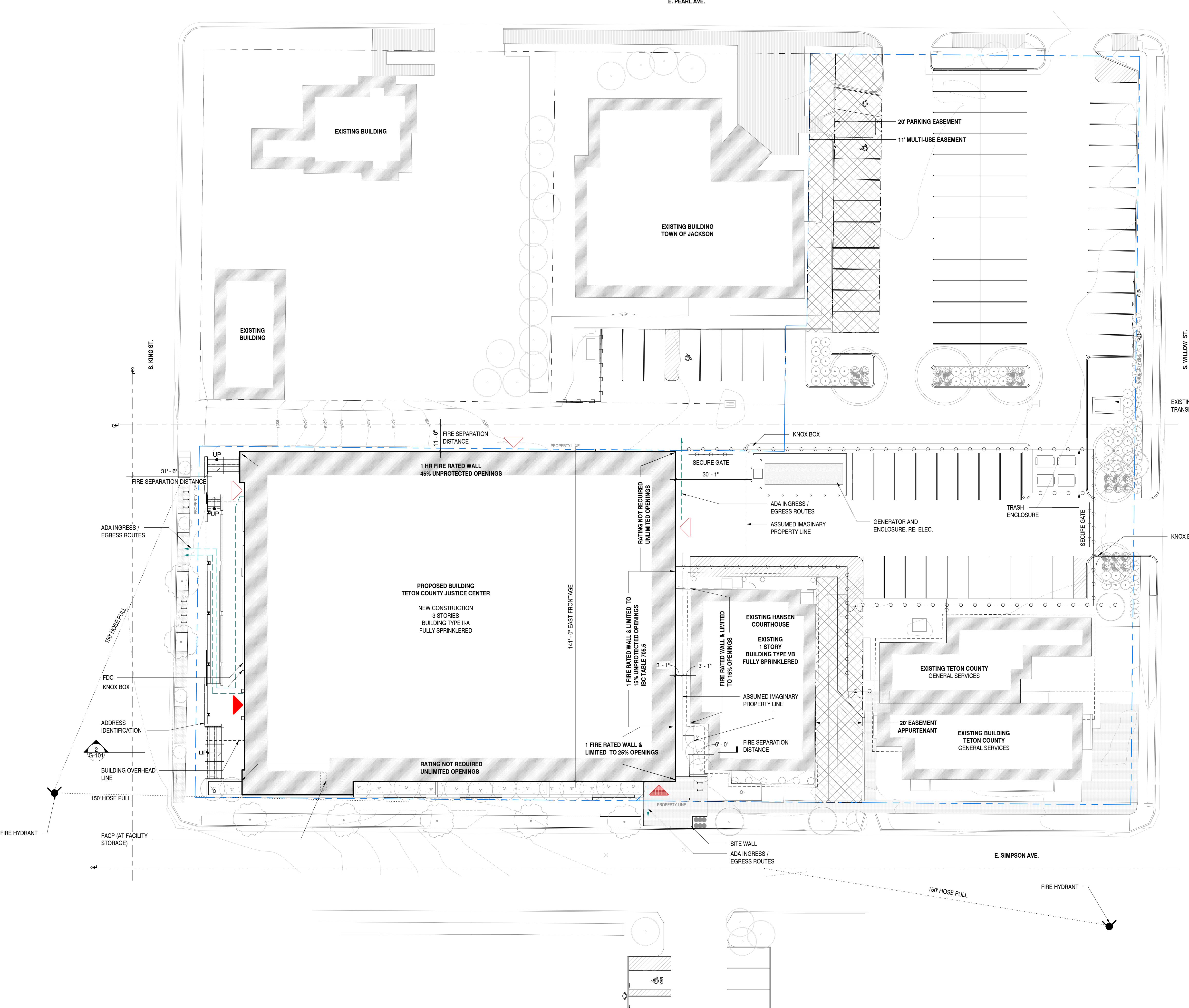
VICINITY MAP



DRAWING INDEX

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L-301	LANDSCAPE PLAN

AndersonMasonDale Architects



TETON COUNTY JUSTICE CENTER 24-031

APPLICABLE CODES

JACKSON MUNICIPAL CODE TITLE 15 BUILDINGS & CONSTRUCTION
 2021 INTERNATIONAL BUILDING CODE
 2021 INTERNATIONAL MECHANICAL CODE
 2021 INTERNATIONAL PLUMBING CODE
 2021 INTERNATIONAL FIRE CODE & INTERNATIONAL WILDLAND-URBAN INTERFACE CODE
 2021 INTERNATIONAL ENERGY CONSERVATION CODE
 2021 INTERNATIONAL FUEL GAS CODE
 2020 NATIONAL ELECTRIC CODE
 2017 ICC/ANSI A117.1

STANDARDS

NFPA STANDARDS: 3 (2021), 4 (2021), 13 (2019), 14 (2019), 20 (2019),
 25 (2020), 70 (2020), 72 (2022), 101 (2012),
 170 (2021), 1225 (2022)

CODE SUMMARY

BUILDING	TETON COUNTY JUSTICE CENTER		
LOCATION	150 E SIMPSON AVE, JACKSON, WY 83001		
GENERAL	3 STORY, STEEL FRAMING		
	LEVEL 1-3 NON-SEPARATED MIXED-USE OCCUPANCY		
	FULLY SPRINKLERED		
	79,965	GSF	

USE AND OCCUPANCY

OCCUPANCY CLASSIFICATION	GROUP A-3, B, I-3, S-1	IBC 303, 304, 309, 310, 311	
MIXED USE OCCUPANCY	NON-SEPARATED OCCUPANCY	IBC 508.3	BUILDING AREA, HEIGHT AND STORIES BASED ON MOST RESTRICTIVE (I-3)
INCIDENTAL USE AREAS	LAUNDRY ROOMS (OVER 100 SF, L2)	IBC TABLE 509.1	1 HOUR OR PROVIDE AUTOMATIC SPRINKLER SYSTEM
	ROOMS WITH BOILERS, LARGEST PIECE OF EQUIP IS OVER 15 PSI & 10 HP	IBC TABLE 509.1	1 HOUR OR PROVIDE AUTOMATIC SPRINKLER SYSTEM
	GROUP I-3 CELLS EQUIPPED WITH PADDED SURFACES	IBC TABLE 509.1	1 HOUR (PADDED CELLS LOCATED IN BOOKING / HOLDING AREA)
HAZARDOUS MATERIALS	EVIDENCE STORAGE (L1)	IBC 414.2, 414.3	

SPECIAL REQUIREMENTS FOR I-3 OCCUPANCY

LOCKING ARRANGEMENT	GROUP I-3 CONDITION 3	IBC TABLE 408.1	
SLIDING DOOR IN MEANS OF EGRESS PATH	PERMITTED	IBC 408.3.2	PERMITTED
SALLY PORT IN MEANS OF EGRESS PATH	PERMITTED	IBC 408.3.7	
LOCKED DOORS IN MEANS OF EGRESS PATH	PERMITTED	IBC 408.4	
20 MIN. DOOR NOT REQUIRED IN SECURE PERIMETER		IBC 716.2.2.1	EXCEPTION 4 IBC 408.7 MODIFIES OPENINGS TO ACCOMMODATE DETENTION DOOR AND INDIVIDUAL SECURITY GLAZING PANELS UP TO 1296 SQ INCHES
SMOKE COMPARTMENTS & 1HR SMOKE BARRIER	REQUIRED	IBC 408.6	
SMOKE BARRIER ARE CONTINUOUS FROM OUTSIDE	WALL TO OUTSIDE WALL	IBC 709.4	
ENGINEERED SMOKE CONTROL SYSTEM	REQUIRED	IBC 408.9	AT WINDOWLESS SMOKE COMPARTMENTS

HEIGHT AND AREA- ALLOWABLE

CONSTRUCTION TYPE	IIA, NON-SEPARATED MIXED OCCUPANCY	IBC TABLE 601	
ALLOWABLE HEIGHT	85 FT	IBC TABLE 504.3	A, B, S-1, I-3 OCCUPANCY, AUTO SPRINKLER
ALLOWABLE STORIES ABOVE GRADE PLANE	3 STORIES	IBC TABLE 504.4	I-3 OCCUPANCY, AUTO SPRINKLER

ACTUAL HEIGHT	52 FT		
ROOFTOP STRUCTURES		IBC 1511.2.1	MECHANICAL SCREENING & STAIR PENTHOUSE ARE BELOW HEIGHT RESTRICTIONS (MAX 18')
ACTUAL NUMBER OF STORIES ABOVE GRADE	3 STORIES		
BASEMENT (LEVEL-1)	(DOES NOT CONTRIBUTE TO STORIES ABOVE GRADE)		IBC 2021 DEFINITIONS " A STORY THAT IS NOT ABOVE GRADE PLANE"
ALLOWABLE AREA PER FLOOR	I-3 45,000	IBC TABLE 506.2	NON-SEPARATED MIXED OCCUPANCY, I-3 MOST STRINGENT

GROSS SQUARE FOOTAGE

FLOOR AREA	OVERHANGS		
LEVEL 1 26,206	0		
LEVEL 1.5 5,015	1,506		
LEVEL 2 23,239	0		
LEVEL 3 25,505	0		
TOTAL GSF 79,965	WITHOUT OVERHANGS		
TOTAL 81,471	INCLUDING EXTERIOR SF BELOW OVERHANGS		

FIRE PROTECTION SYSTEMS

SPRINKLER SYSTEM	THROUGHOUT PER NFPA 13	IBC 903	
FIRE ALARM SYSTEM	PROVIDED	IBC 901.2	FA EMERGENCY VOICE COMMUNICATION PROVIDED. LOCATION OF FACP TO BE FIELD APPROVED BY THE FIRE DEPARTMENT.
SHUNT TRIP		IBC 3007.4	
STANDPIPES	CLASS I	IBC 905.3.1 EXCEPTION-1	AUTOMATIC SPRINKLER SYSTEM
PORTABLE FIRE EXTINGUISHERS	YES	IBC 906.1, IFC	DISTANCE NOT TO EXCEED 75 FT, CLASS K REQ. FOR CAFÉ

FIRE RESISTANCE RATED CONSTRUCTION

TYPE IIA			
SHAFTS	1 HR or 2 HR (varies)	IBC 713.4	1 HR WHERE CONNECTING < 4 STORIES; 2 HR CONNECTING 4 STORIES OR MORE
PASSENGER ELEVATOR HOISTWAY	1HR	IBC 3002.1	1 HR RATED ELEVATOR HOISTWAY
FLOOR ASSEMBLIES	1 HR	IBC TABLE 601	
ROOF ASSEMBLIES	1 HR	IBC TABLE 601	HEAVY TIMBER ROOF CONSTRUCTION ALLOWED - EXCEPTION C
STRUCTURAL FRAME	1 HR	IBC TABLE 601	H
EXTERIOR LOAD BEARING WALLS	1 HR	IBC TABLE 601	
INTERIOR LOAD BEARING WALLS	1 HR	IBC TABLE 601	
EXTERIOR NONBEARING WALLS/PARTITIONS	1 HR	IBC TABLE 705.5	FIRE SEPARATION DISTANCE GREATER THAN 10 FT LESS THAN 30 FT
INTERIOR PARTITIONS	0 HR	IBC TABLE 601	
CORRIDORS	1 HR	IBC TABLE 1020.2	WITH SPRINKLER, I-3 MOST STRINGENT, MAY BE REDUCED BY 408.8
EXTERIOR WALLS	1 HR	IBC TABLES 705.5 & 705.8	FIRE SEPARATION DISTANCE VARIES RE: CODE SITE PLAN
EXTERIOR WALL OPENINGS	Varies Per Elevation	IBC TABLE 705.8	NORTH & WEST - UNLIMITED / EAST 15% / NORTH 45%
SMOKE BARRIERS		IBC 408.6 & 709	DETENTION HOUSING TO BE DIVIDED INTO 2 SMOKE COMPARTMENTS
DWELLING UNITS WALL ASSEMBLIES	30 MIN	IBC 408.8.1	SEPARATED BY SMOKE TIGHT PARTITION WHERE DISTANCE OF TRAVEL TO CORRIDOR EXCEEDS 50 FT (NOT SURE THIS CONDITION EXISTS)

MEANS OF EGRESS

OCCUPANT LOAD FACTOR		IBC TABLE 1004.5	
ASSEMBLY UNCONCENTRATED	1:15 NET	IBC TABLE 1004.5	TABLES AND CHAIRS, NOT FIXED
ASSEMBLY - EXHIBIT	1:30 NET	IBC TABLE 1004.5	
ASSEMBLY WITH FIXED SEATING	1 SEAT/18" L	IBC 1004.6	
COURTROOMS - OTHER THAN FIXED SEATING	1:40 NET	IBC TABLE 1004.5	
INSTITUTIONAL - SLEEPING AREAS	1:120 GROSS	IBC TABLE 1004.5	
BUSINESS	1:150 GROSS	IBC TABLE 1004.5	
KITCHEN, COMMERCIAL	1:200 GROSS	IBC TABLE 1004.5	
STORAGE, MECHANICAL, ELECTRICAL, PLUMBING	1:300 GROSS	IBC TABLE 1004.5	
TWO-WAY COMMUNICATION SYSTEM	PROVIDED	IBC 1009.8	PER 1009.8. WHERE FLOORS DO NOT HAVE DIRECT ACCESS TO GRADE, A MONITORED 2-WAY COMMUNICATION SYSTEM IS PROVIDED AT EACH ELEVATOR LANDING.

FIRE-ALARM VOICE COMMUNICATION	PROVIDED	IBC 907.2.3	
EXIT DOORS AT ASSEMBLY OCCUPANCY	PROVIDED WITH PANIC HARDWARE		
OCCUPANTS (CALCULATED FOR EXITING):			
LEVEL 1	SEE CODE PLANS		
LEVEL 1.5	SEE CODE PLANS		
LEVEL 2	SEE CODE PLANS		
LEVEL 3	SEE CODE PLANS		
TOTAL	SEE CODE PLANS		
MEANS OF EGRESS SIZING			
36 INCH WIDE DOORS	33/0.15 = 220	IBC 1005.3.2 EX. #1	
72 INCH WIDE DOORS	69/0.15 = 460	IBC 1005.3.2 EX. #1	
MINIMUM CORRIDOR WIDTH	44 INCHES	IBC 1020.3	
CORRIDORS	WIDTH/0.15	IBC 1005.3.2 EX. #1	
STAIRS	WIDTH/0.20	IBC 1005.3.1 EX. #1	
MINIMUM NUMBER OF EXITS	(3) REQD.	IBC TABLE 1006.3.3	(3) PROVIDED @ L1-6; (1) PROVIDED @ ROOF (UNOCCUPIED)
EXIT SIGNS	PROVIDED IN COMPLIANCE WITH	IBC 1013	
EXIT SIGN ILLUMINATION	PROVIDED IN COMPLIANCE WITH	IBC 1013	
EXIT ACCESS TRAVEL DISTANCE	250 FT AT A, B, & S-1 OCCUPANCY; 200 FT AT I-3 OCCUPANCY	IBC 1017.2	WITH SPRINKLER SYSTEM
EXIT THROUGH INTERVENING SPACES		IBC 1016.2	
NUMBER OF EXITS FROM ROOM	1 REQUIRED IF OCC LOAD < 50 2 REQUIRED IF OCC LOAD > 49	IBC TABLE 1006.2.1	
EXIT SEPARATION	SEPARATION DISTANCE NOT LESS THAN 1/3 THE MAXIMUM OVERALL DIAGONAL	IBC 1007.1.1 EXCEPTION 2	
DEAD END CORRIDOR	20 FT @ A-3 & 50 FT @ B	1020.5 EXCEPTION 1 & 2	AUTOMATIC SPRINKLER
	UNLIMITED WHERE CORRIDOR LENGTH < 2.5 TIMES LEAST WIDTH	IBC 1020.5 EXCEPTION 3	
PANIC HARDWARE	REQUIRED AT ALL EXIT DOORS SERVING ALL OCCUPANCIES AND THOSE WITH OCC LOAD >49	IBC 1010.2.9	
COMMON PATH OF TRAVEL	75 FT A; 100 FT AT B, S and I-3	IBC TABLE 1006.2.1	AUTOMATIC SPRINKLER, A OCCUPANCY MOST STRINGENT
INTERIOR EXIT ACCESS STAIRWAY	AT PUBLIC STAIR	IBC 1019.3	Exception 4 - AUTOMATIC SPRINKLER, OPENING SIZE IS LIMITED TO TWICE THE SIZE OF STAIR FOOTPRINT, DRAFT CURTAIN AT FLOOR OPENINGS
ELEVATORS	1 HR HOISTWAY ENCLOSURE PROTECTION	IBC 3002; IBC 713.4	1-HR FRR HOISTWAY PROVIDED
	HOISTWAY OPENING PROTECTION	IBC 3006.2.1; IBC 3006.3; IBC 710.5.2.2 & 710.5.2.2.1	ENCLOSED E

PLUMBING FIXTURE COUNT								
	OCCUPANTS	1 WATER CLOSETS PER OCCUPANT BELOW	WATER CLOSETS REQUIRED	1 LAVS PER OCCUPANT BELOW	LAVS REQUIRED	1 DRINKING FOUNTAINS PER 100 OCCUPANTS	DRINKING FOUNTAINS REQUIRED	SERVICE SINKS REQUIRED
ASSEMBLY - TYPE A-3								
OCC. LOAD	436					1/500	0.872	1
MALE	218	1/125	1.744	1/200	1.090			
FEMALE	218	1/65	3.354	1/200	1.090			
STORAGE - TYPE S								
OCC. LOAD	65					1/1000	0.065	1
MALE	33	1/100	0.325	1/100	0.325			
FEMALE	33	1/100	0.325	1/100	0.325			
BUSINESS - TYPE B								
OCC. LOAD	441					1/100	4.410	1
MALE	221	1/25 for 50 + 1/50	5.410	1/40 for 40 +1/80	3.756			
FEMALE	221	1/25 for 50 + 1/50	5.410	1/40 for 40 +1/80	3.756			
INSTITUTIONAL - TYPE I-3								
OCC. LOAD	86					1/100	0.860	1
MALE	43	1 per 25	1.720	1 per 35	1.229			
FEMALE	43	1 per 25	1.720	1 per 35	1.229			
TOTAL REQUIRED	1,028		20.008		12.800		6.207	4
TOTAL PROVIDED			68		64		10	4

SKETCH PLAN TETON COUNTY COURTHOUSE

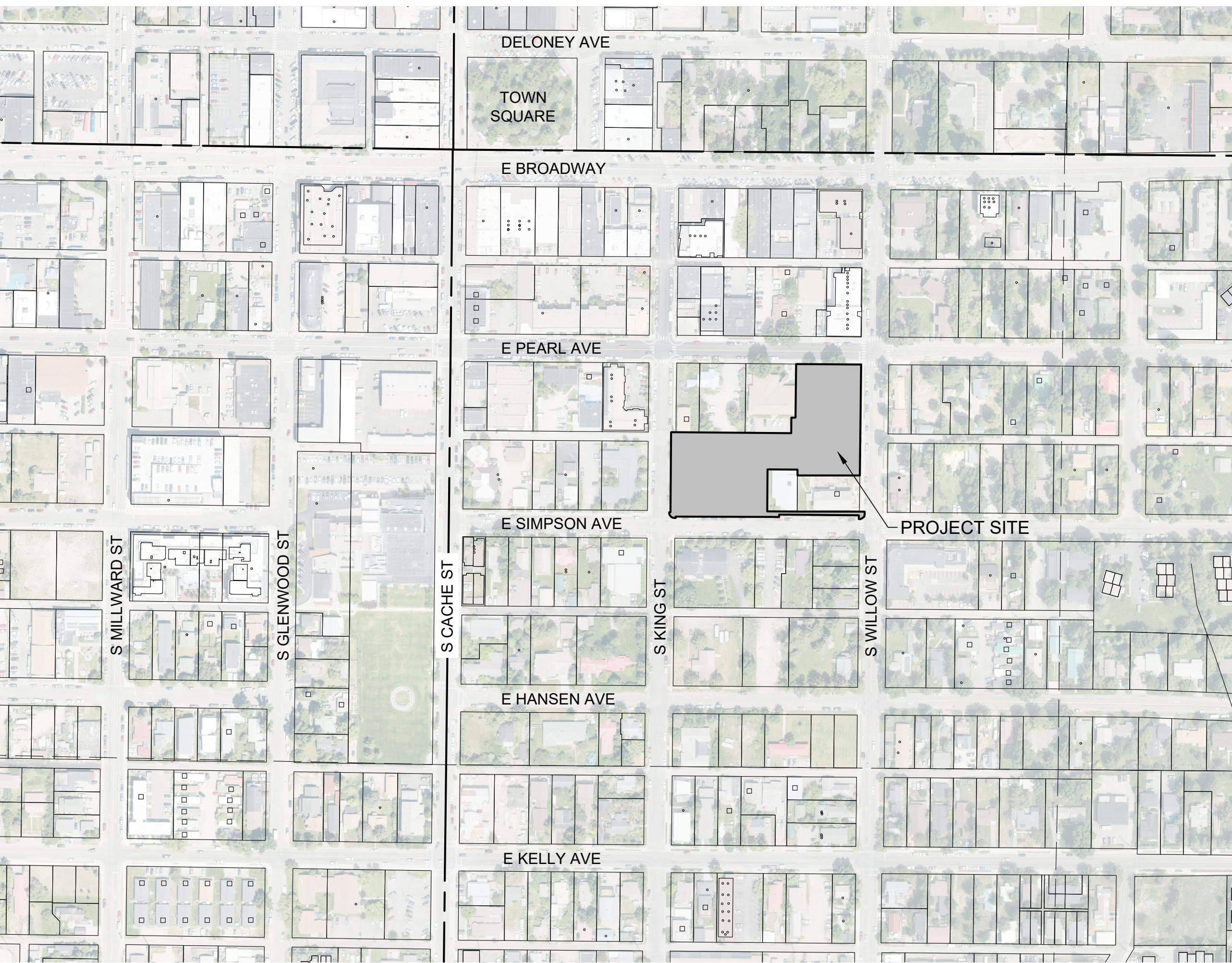
180 SOUTH KING STREET

LOCATED WITHIN
NW $\frac{1}{4}$ NW $\frac{1}{4}$ SECTION 34,
T41N, R116W, 6TH P.M.
TETON COUNTY, WYOMING



Sheet List Table

Sheet Number	Sheet Title
C1.0	GENERAL NOTES
C2.0	EXISTING CONDITIONS
C3.0	SITE OVERVIEW
C3.1	SAN SEWER REALIGNMENT OPTION 1
C3.2	SAN SEWER REALIGNMENT OPTION 2
C3.3	SAN SEWER REALIGNMENT OPTION 3
C4.0	GRADING DETAILS
C4.1	STORM WATER DETAILS
C4.2	WATER DETAILS
C4.3	SEWER DETAILS



VICINITY MAP

1" = 200' for 22x34 Prints
1" = 400' for 11x17 Prints

GENERAL PROJECT NOTES:

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE TOWN OF JACKSON LAND DEVELOPMENT REGULATIONS, THE WYOMING DEPARTMENT OF ENVIRONMENTAL QUALITY RULES AND REGULATIONS, AND THE LATEST EDITION OF THE WYOMING PUBLIC WORKS STANDARD SPECIFICATIONS OR AS MODIFIED BY THE SPECIAL PROVISIONS AND THE SUPPLEMENTARY SPECIFICATIONS.
- PROJECT SCOPE: BUILD SITE AND UTILITY INFRASTRUCTURE AND GRADING TO ACCOMMODATE A NEW COURTHOUSE BUILDING, PARKING LOT, AND THE DEMOLITION OF THE EXISTING DETENTION CENTER AND COURTHOUSE.
- PROPERTY ZONE: PUBLIC/SEMI-PUBLIC - TOWN (P/SP-Tz)
- PROPERTY AREA: 1.8 ACRES
TOTAL AREA OF DISTURBANCE: 1.53 ACRES
- PROJECT SCHEDULE: CONSTRUCTION SCHEDULED TO START IN SPRING 2026 AND BE COMPLETED BY FALL 2028.
- BASE MAPPING PREPARED FROM SURVEYS PERFORMED BY JORGENSEN ASSOCIATES IN MAY 2021 AND SUPPLEMENTED IN MAY AND JULY 2023.
- GEOTECHNICAL REPORT PREPARED BY JORGENSEN GEOTECHNICAL IN JUNE 2023.
- THE PROJECT AREA IS NOT LOCATED IN A FLOODPLAIN.
- NO AVALANCHE PATHS ARE PRESENT ON THIS SITE.
- BUILDING TIES ARE TO FOUNDATION STEM WALLS.

WATER AND SEWER UTILITY NOTES

- WATER AND SEWER INSTALLATIONS SHALL BE IN ACCORDANCE WITH CURRENT WYOMING DEPARTMENT OF ENVIRONMENTAL QUALITY, WATER QUALITY DIVISION RULES AND REGULATIONS. AND CONFORM TO TOWN OF JACKSON WATER AND SEWER REGULATIONS.
- TYPE A TRENCH BACKFILL SHALL APPLY TO ALL PAVED SURFACES AND AROUND ALL APPURTENANCES INCLUDING VALVES, FIRE HYDRANTS AND MANHOLES. TYPE B TRENCH BACKFILL SHALL APPLY ONLY TO NON-TRAVELED AREAS.
- CONTINUOUS RIGID TYPE A PIPELINE INSULATION SHALL BE PROVIDED AS SHOWN ON THE TYPICAL TRENCH DETAIL, AT ALL LOCATIONS WHERE DEPTH OF COVER IS LESS THAN 5 FEET FOR SEWER MAINS AND SERVICES; LESS THAN 7 FEET FOR WATER MAINS AND SERVICES OR AS SHOWN ON THE PLANS. ALL SERVICES UNDER ROADWAYS, DRIVEWAYS, AND SIDEWALKS SHALL BE INSTALLED WITH 2" TYPE A INSULATION.
- Maintain a minimum of 10 FEET horizontal separation between water pipes and sewer pipes/manholes, measured edge to edge. Maintain a minimum of 18-INCHES vertical separation at water/sewer pipe crossings. Where sewer pipe crosses above water pipe, special construction is required. See typical details.
- WATER SERVICE LINE SHALL BE POLYETHYLENE (PE) CONFORMING TO THE REQUIREMENTS OF AWWA C 901. PE PIPE SHALL BE PRESSURE TUBING CONFORMING TO TABLE 7 OF SAID SPECIFICATION. TUBING SHALL BE CLASS 200 WITH A DR OF 9.
- SANITARY SEWER GRAVITY LINES AND FITTINGS SHALL BE SCH 40 PVC AND CONFORM TO ASTM D1785. PRESSURE SEWER SERVICE LINES SHALL BE POLYETHYLENE, SDR11 IPS PE.
- LOCATIONS OF EXISTING FACILITIES ARE FROM DRAWINGS PROVIDED TO JORGENSEN ASSOCIATES. CONTRACTOR SHALL NOT HOLE IN THE PRESENCE OF THE ENGINEER AND ANY ADJUSTMENTS NEEDED SHALL BE REFLECTED ON AS-CONSTRUCTED DRAWINGS.

REVEGETATION SPECIFICATIONS:

(FOLLOW MITIGATION PLAN. FOLLOW THESE SPECIFICATIONS WHERE NOTHING IS SPECIFIED ON MITIGATION PLANS OR BY LANDSCAPE ARCHITECT.)

1. SEED MIXTURE:

COMMON NAME	LBS./ACRE
MOUNTAIN BROME	10 LBS./ACRE
THICKSPIKE WHEATGRASS	12 LBS./ACRE
IDAHO FESCUE	6 LBS./ACRE
WESTERN WHEATGRASS	12 LBS./ACRE
ALPINE TIMOTHY	12 LBS./ACRE
TOTAL PURE LIVE SEED APPLICATION RATE	52 LBS./ACRE

- SEED MIXES CONTAINING NATIVE FLOWERING PLANTS SUCH AS LUPINE, YARROW AND PAINTBRUSH ARE ACCEPTABLE.
- ALL SEED SHALL COMPLY WITH WYOMING SEED LAW. SEED SHALL BE PURCHASED FROM A DEALER LICENSED WITH THE WYOMING DEPARTMENT OF AGRICULTURE. CERTIFICATIONS FOR THE SEED MIX SHALL BE PROVIDED TO THE ENGINEER PRIOR TO SEEDING.
- TOPSOIL SHALL BE UNIFORMLY SPREAD ON PREPARED SURFACES PRIOR TO SEEDING. REMOVE FOREIGN MATERIALS, WEEDS AND UNDESIRABLE PLANTS FROM THE PREPARED SOIL PRIOR TO SEEDING.
- HARD PACKED OR CAKED TOPSOIL SURFACES SHALL BE SCARIFIED OR DISKED PRIOR TO SEEDING.
- SEED SHALL BE UNIFORMLY DISTRIBUTED OVER THE SURFACE BY APPROVED MECHANICAL BROADCASTING DEVICES AND THE GROUND SHALL BE IMMEDIATELY RAKED OR DRAGGED TO COVER THE SEED.
- SEEDING SHALL BE PERFORMED BETWEEN THE TIME THE FROST LEAVES THE GROUND IN THE SPRING AND BEFORE THE FROST ENTERS THE GROUND IN THE FALL.

CONSTRUCTION NOTES & SPECIFICATIONS:

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE PERMITS AND AGREEMENTS INCLUDING: TOWN OF JACKSON GRADING AND EROSION CONTROL PERMIT, WYDEQ PERMIT TO CONSTRUCT, WYDEQ WYDPS PERMIT, CONTACT ENGINEER FOR COPIES OF PERMITS AND PERMIT REQUIREMENTS. CONTRACTOR SHALL REVIEW AND BE RESPONSIBLE FOR PERMIT COMPLIANCE.
- IF NECESSARY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN A WYOMING DEPARTMENT OF ENVIRONMENTAL QUALITY NPDPS STORMWATER PERMIT PRIOR TO COMMENCING ANY LAND DISTURBING ACTIVITIES.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD AND SHALL PROMPTLY NOTIFY THE ENGINEER OF ANY VARIATIONS OR DISCREPANCIES.
- CONTRACTOR SHALL VERIFY LOCATION OF ALL BURIED AND OVERHEAD UTILITIES PRIOR TO ANY EXCAVATION IN THE VICINITY. UTILITY LOCATIONS SHOWN ON THESE DRAWINGS ARE APPROXIMATE AND BASED ON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. ENGINEER DOES NOT WARRANT THE ACCURACY NOR COMPLETENESS OF THE INFORMATION SHOWN FOR EXISTING UTILITIES. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES PRIOR TO INSTALLING IMPROVEMENTS. PRIVATE UNDERGROUND UTILITIES EXIST IN THE PROJECT AREA. CONTACT ENGINEER TO LOCATE EXISTING WATER AND SEWER LINES.
- CONTRACTOR TO CONFIRM STOCKPILE AND STAGING LOCATIONS WITH THE OWNER.
- CONTRACTOR TO LOCATE ALL UTILITIES PRIOR TO CONSTRUCTION.
- FILL MATERIAL SHALL BE SUITABLE ON-SITE OR IMPORTED MATERIAL WITH ROCK NO LARGER THAN 6 INCHES IN DIAMETER. LARGER MATERIAL MAY BE PLACED ONLY WHEN AUTHORIZED BY THE ENGINEER.
- SUBGRADE, PIT RUN SUBBASE, AND SITE FILL MATERIALS SHALL BE MECHANICALLY COMPAKTED TO A MINIMUM OF 95% OF MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D698 (AASHTO T-99 - STANDARD PROCTOR DENSITY) IN LIFTS NOT TO EXCEED 8 INCHES IN LOOSE THICKNESS.
- CRUSHED GRAVEL BASE MATERIAL SHALL BE GRADING H OR GRADING W.
- CRUSHED GRAVEL BASE COURSES SHALL BE MECHANICALLY COMPAKTED TO A MINIMUM OF 95% OF MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D1557 (AASHTO T-180 - MODIFIED PROCTOR DENSITY).
- DESTRUCTION AND DAMAGE TO TREES AND OTHER NATURAL VEGETATION SHALL BE MINIMIZED AND ALL DISTURBED SURFACES SHALL BE RESEEDED AS SOON AS PRACTICABLE IN ACCORDANCE TO THE REVEGETATION SPECIFICATIONS.
- STRIP AND SALVAGE TOPSOIL FROM ALL EXCAVATED AREAS.
- WEEDS SHALL BE CONTROLLED BY SPRAYING, LIMITING DISTURBANCE AREA, OR OTHER MEANS PRIOR TO REVEGETATION AND AFTER REVEGETATION IS COMPLETE.
- REVEGETATION SHALL OCCUR AFTER CONSTRUCTION IS COMPLETE AND WILL BE COMPLETED IN THE SPRING OF 2028.
- FUGITIVE DUST WILL BE CONTROLLED BY WATERING DURING DRY PERIODS OR AS REQUIRED BY OWNER.
- ALL EXCAVATED MATERIALS SHALL BE STOCKPILED AND PROCESSED ON-SITE ONLY AT LOCATIONS AS DESIGNATED ON THE PLANS.
- TOPS OF CUT AND FILL SLOPES SHALL BE ROUNDED TO AVOID Raveling AND Erosion.
- TOPSOIL SHALL BE PLACED ON ALL SLOPES AND AREAS STRIPPED FOR GRADING.
- CUT AND FILL SLOPES SHALL NOT EXCEED 2:1 WITHOUT SPECIAL STABILIZATION AND APPROVAL FROM ENGINEER.
- NO WETLANDS SHALL BE DISTURBED WITHOUT FIRST OBTAINING NECESSARY PERMITS FROM THE U. S. ARMY CORPS OF ENGINEERS.
- AREAS WITH UNSUITABLE SUBGRADE UNDER ROADWAYS OR BUILDING SHALL BE SUB-EXCAVATED AND REPLACED WITH APPROVED STRUCTURAL FILL. COORDINATE WITH ENGINEER FOR REMOVAL DEPTHS AND STRUCTURAL FILL SELECTION.
- ENGINEER SHALL BE CONTACTED PRIOR TO CONSTRUCTION TO PROVIDE CONSTRUCTION OBSERVATION.
- ALL DEVELOPMENT SHALL BE SETBACK A MINIMUM OF THIRTY (30) FEET FROM WETLANDS.
- ALL FENCING MUST COMPLY WITH SECTION 49220, WILDLIFE FRIENDLY FENCING, OF THE TETON COUNTY LAND DEVELOPMENT REGULATIONS. THIS SECTION INCLUDES A PROVISION FOR SPECIAL PURPOSE FENCING. ALL SPECIAL PURPOSE FENCING MUST BE APPROVED BY THE TETON COUNTY PLANNING DIRECTOR.
- BEFORE START OF CONSTRUCTION INSTALL CONSTRUCTION FENCE/SILT FENCE TO DEFINE LIMITS OF LAND DISTURBING ACTIVITIES. FENCE FILTER FABRIC SHALL BE SPUN BOND GEOTEXTILE FABRIC.
- CONTRACTOR IS RESPONSIBLE FOR PERIODIC INSPECTIONS, MAINTENANCE AND REPAIR OF EROSION CONTROL STRUCTURES TO INSURE PROPER SEDIMENT CONTAINMENT AND EROSION CONTROL.
- STORMWATER RUNOFF SHALL BE MANAGED IN ACCORDANCE WITH TOWN OF JACKSON LAND DEVELOPMENT REGULATIONS. RUNOFF FROM THE SITE SHALL NOT EXCEED THE PRE-CONSTRUCTION VALUES, AND VOLUME OF STORAGE SHALL NOT BE REDUCED.
- ALL AVAILABLE TOPSOIL SHALL BE STORED FOR USE IN REVEGETATION OPERATIONS. TOPSOIL SHALL BE UNIFORMLY SPREAD ON PREPARED SURFACES PRIOR TO SEEDING. REMOVE FOREIGN MATERIALS, WEEDS, AND UNDESIRABLE PLANTS FROM THE PREPARED SOIL PRIOR TO SEEDING.
- HARD PACKED OR CAKED TOPSOIL SURFACES SHALL BE SCARIFIED OR DISKED PRIOR TO SEEDING. REVEGETATE ALL DISTURBED AREAS AS SOON AS PRACTICAL AFTER GRADING.
- EXISTING VEGETATION IS TO BE MAINTAINED TO THE EXTENT POSSIBLE.
- DURING CONSTRUCTION CONTRACTOR SHALL OBSERVE THE PROJECT SITE FOR THE GROWTH OF NOXIOUS WEEDS. THE GROWTH OF NOXIOUS WEED SHALL BE REPORTED TO THE TETON COUNTY WEED AND PEST DISTRICT OFFICE. CONTRACTOR AND OWNER SHALL IMPLEMENT A WEED CONTROL PROGRAM TO CONTROL NOXIOUS WEEDS.
- UNLESS OTHERWISE SPECIFIED, GRADING SHALL PROVIDE A MINIMUM 6 FEET OF INCHES OF FALL IN THE FIRST 10 FEET AWAY FROM FOUNDATION WALL ADJACENT TO LANDSCAPED AREAS. HARDSCAPES SHALL BE SLOPED AT A MINIMUM 2% AWAY FROM THE BUILDING FOR THE FIRST 10 FEET.

AndersonMasonDale
Architects

JORGENSEN

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24-031

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Wember
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Issue Date
To: PRE-APPLICATION
CONCEPT PRICING PACKAGE
SCHEMATIC DESIGN
To: SKETCH PLAN
26 JUNE 2024
11 JULY 2024
06 DECEMBER 2024
04 MARCH 2025

Project Number: 21036
Drawn By: KB
Reviewed By: BS
Approved By: BS

GENERAL NOTES

C1.0

TETON COUNTY
JUSTICE CENTER180 S King Street
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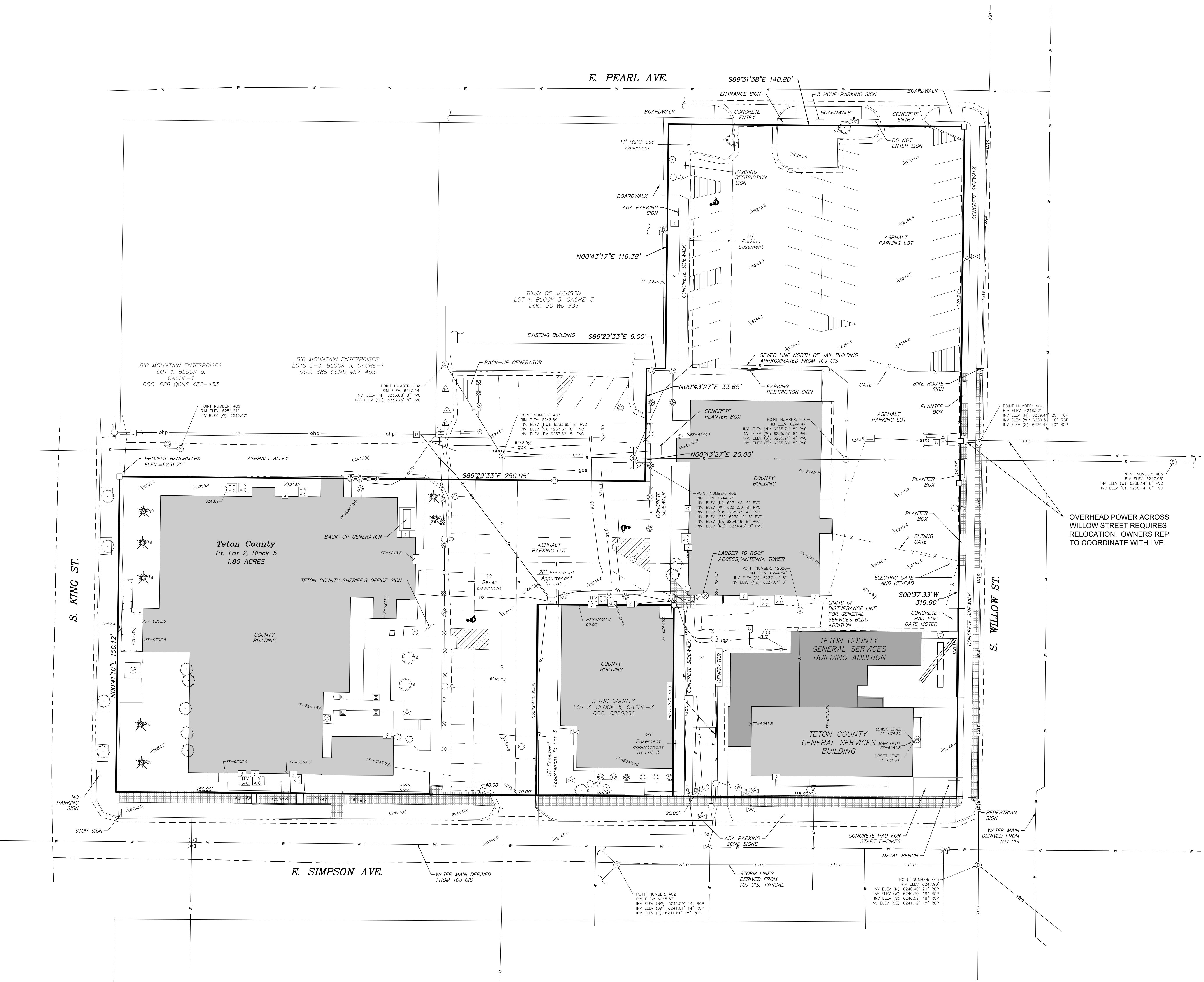
24-031

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ToJ: SKETCH PLAN
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04 MARCH 2025Project Number: 21036
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EXISTING CONDITIONS

- PRELIMINARY -
SUBJECT TO CORRECTION
AND APPROVAL

C2.0



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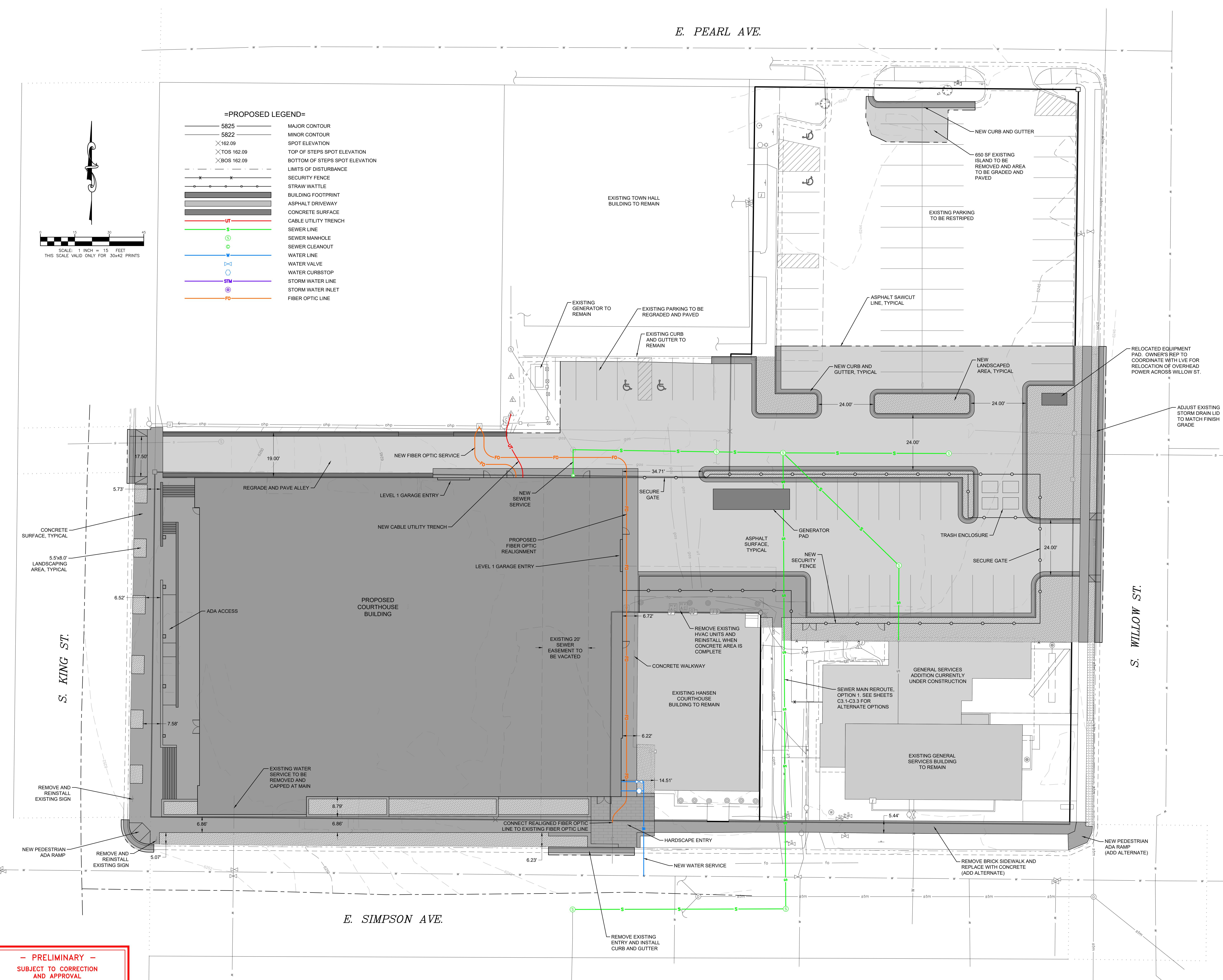
Werner

7391 E. 110th Street, P.O. #100
Greenwood Village, CO 80111
Telephone: 303-378-4100Issue
To: PRE-APPLICATION
CONCEPT PRICING PACKAGE
SCHEMATIC DESIGN
To: SKETCH PLAN
Date
26 JUNE 2024
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06 DECEMBER 2024
04 MARCH 2025Project Number:
21036
Drawn By:
KB
Reviewed By:
BS
Approved By:
BS

SITE OVERVIEW

C3.0

E. PEARL AVE.



TETON COUNTY
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Jackson, WY 83001

24-031

Owner

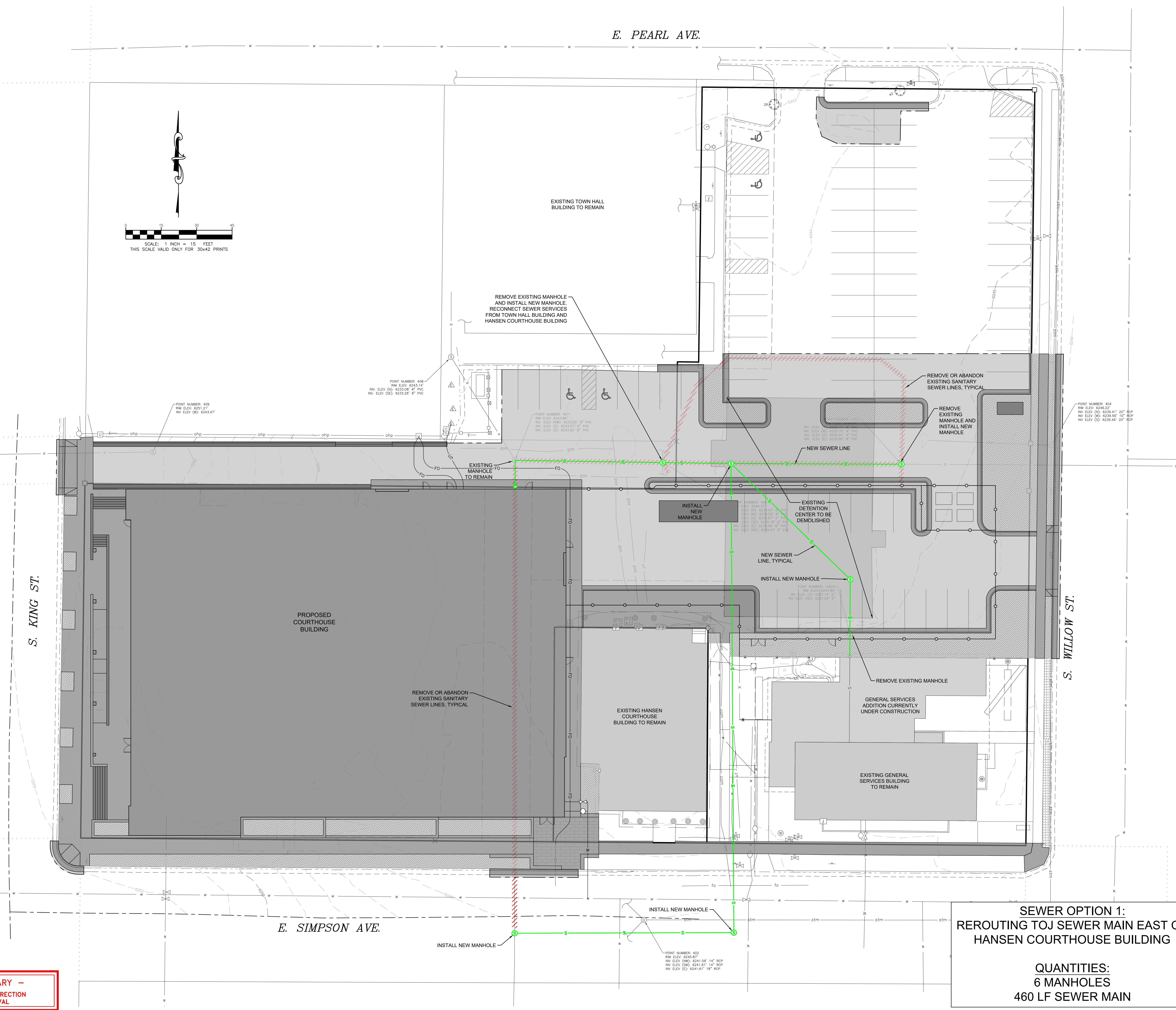
Teton County General Services
P.O. Box 3368
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Telephone: 307-732-8409

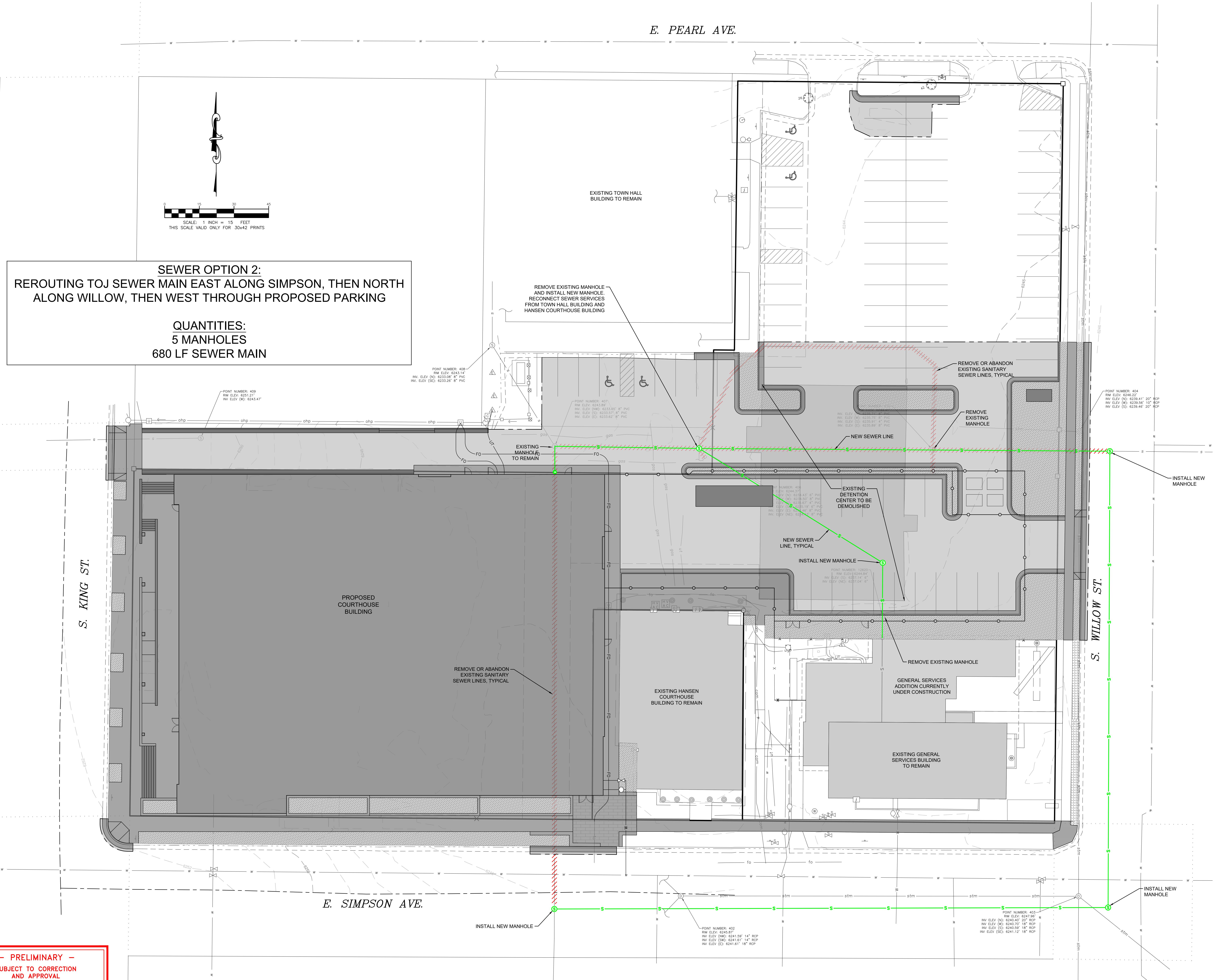
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Drawn By:
Reviewed By:
Approved By:
21036
KB
BS
BSSAN SEWER REALIGNMENT
OPTION 1

C3.1

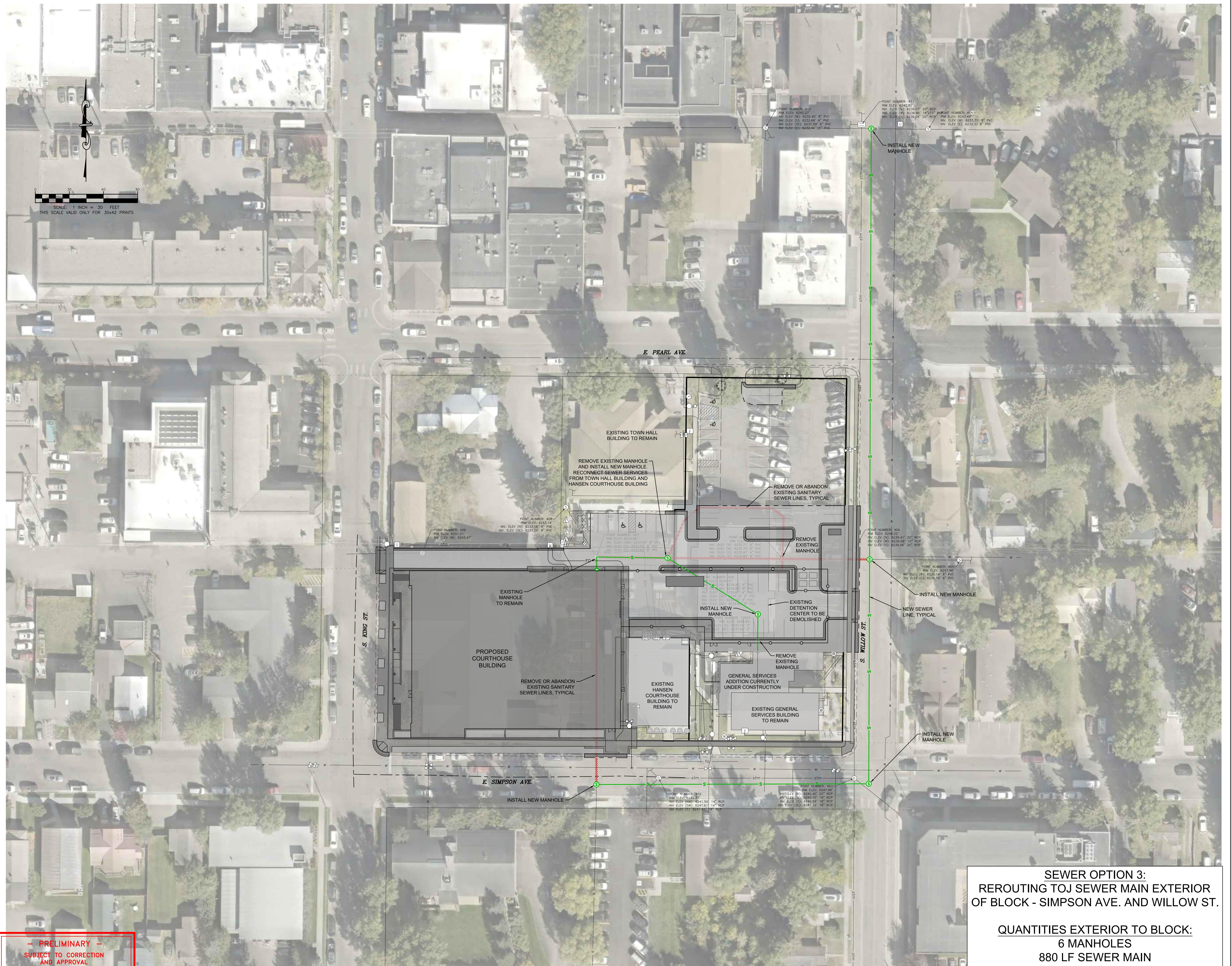
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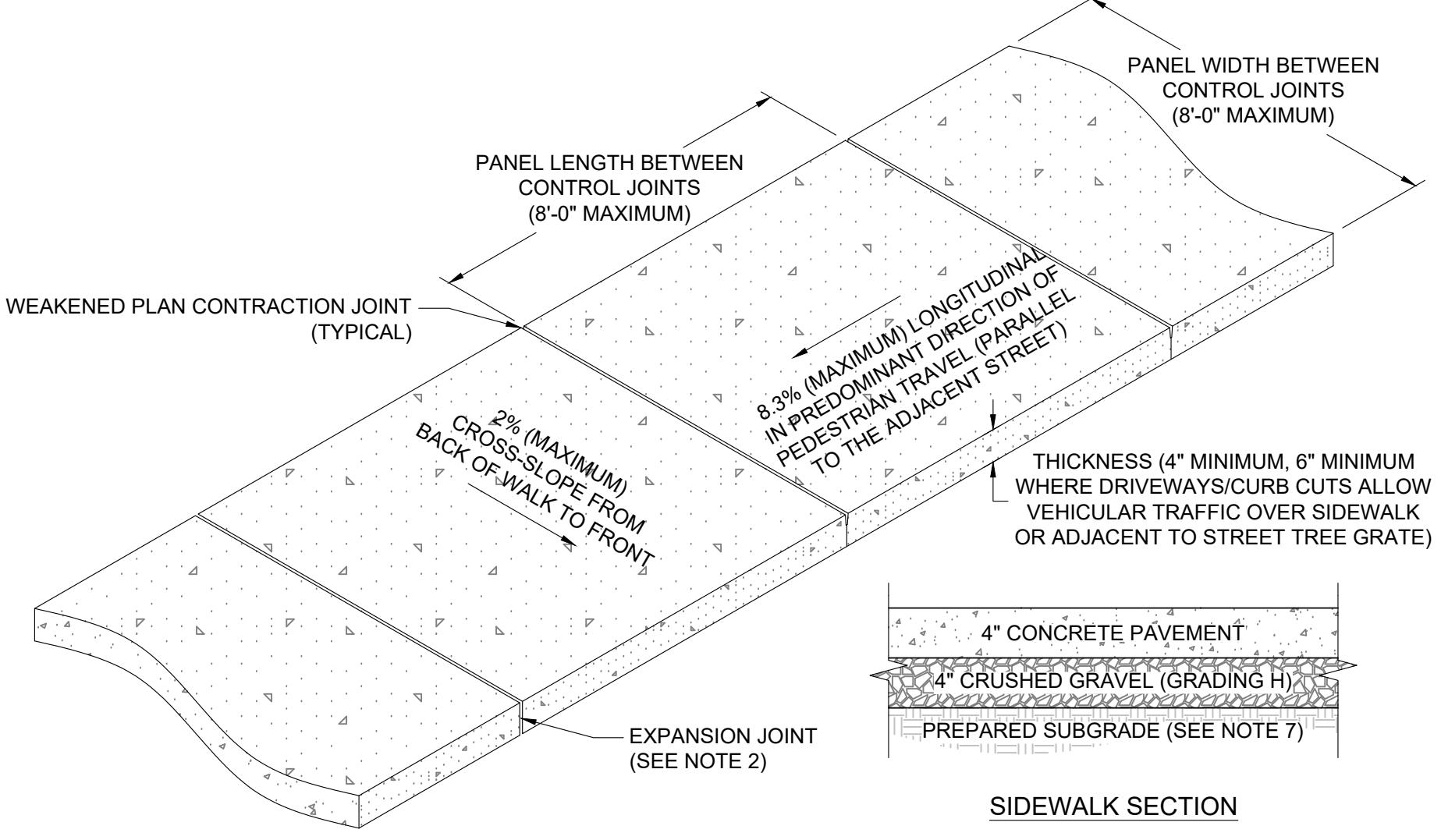




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26 JUNE 2024
11 JULY 2024
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04 MARCH 2025Project Number: 21036
Drawn By: KB
Reviewed By: BS
Approved By: BSSAN SEWER REALIGNMENT
OPTION 3

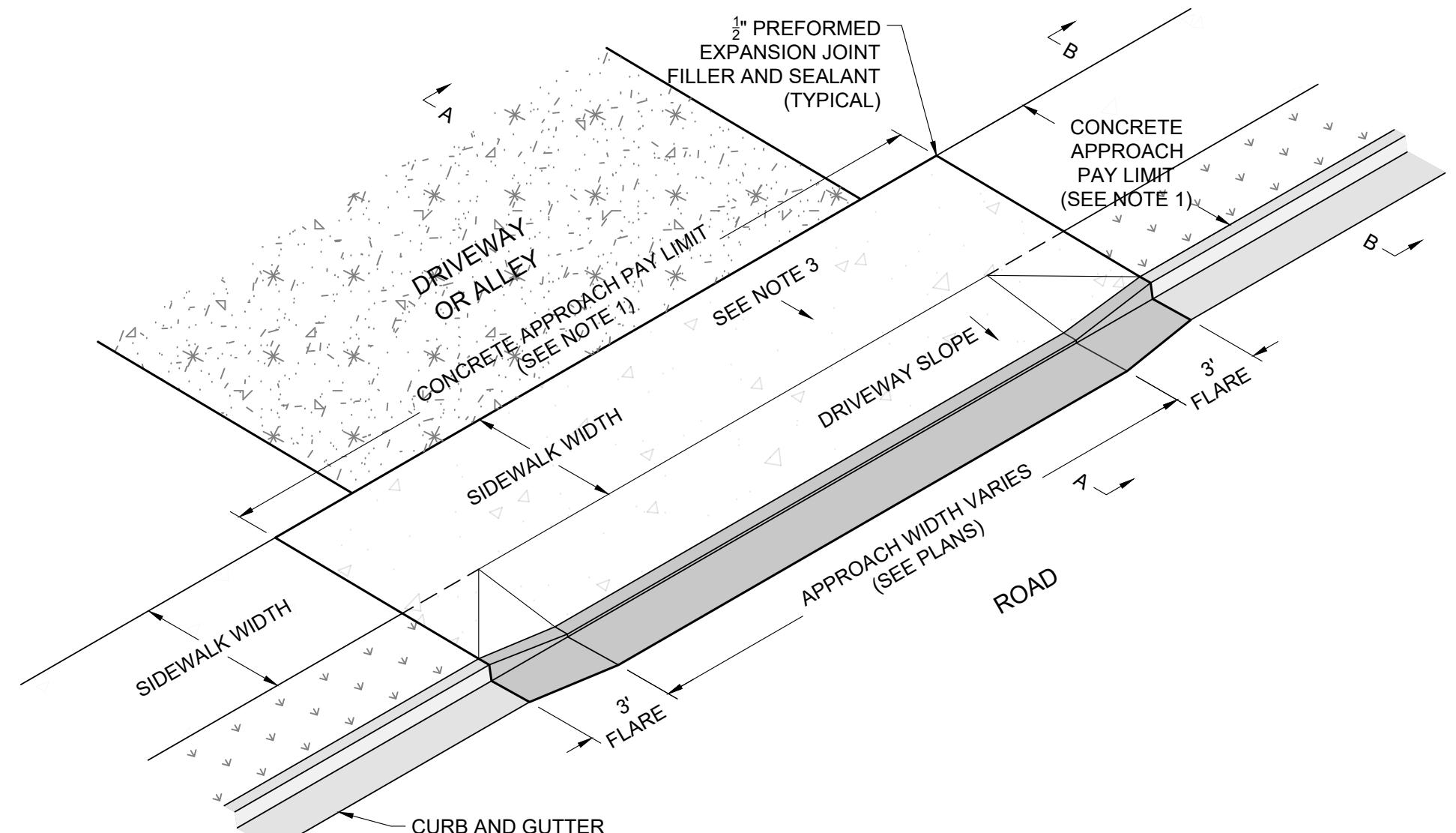


NOTE

1. SIDEWALK SHALL CONFORM TO ALL APPLICABLE ADA STANDARD REQUIREMENTS. SIDEWALKS SHALL CONFORM TO WPWSS SECTION 02776, EXCEPT THAT PORTLAND CEMENT CONCRETE CLASS 4000 CONCRETE CONFORMING WITH WPWSS SECTION 03304, PART 2.07.
2. EXPANSION JOINTS SHALL BE PLACED IN SIDEWALK AT THE SAME LOCATIONS AS THOSE IN CURB AND GUTTER WHEN SIDEWALK IS ADJACENT TO CURB. (PER WPWSS SECTION 02521, PART 3.04 SPACING SHALL NOT EXCEED 32'-0" ON CENTER.)
3. FOR SIDEWALKS GREATER THAN EIGHT FEET IN WIDTH, A LONGITUDINAL CONTROL JOINT SHALL BE INSTALLED AT THE CENTER OF THE WALK.
4. REMOVAL AND REPLACEMENT OF SIDEWALK SHALL TAKE PLACE IN FULL PANELS.
5. AGGREGATE BASE COURSE SHALL BE FOUR INCH MINIMUM THICKNESS, CONFORM TO WPWSS SECTION 02190, PART 2.03.
6. CLEAR VEGETATION AND STRIP TOPSOIL TO SUBGRADE, SCARIFY, CONDITION, AND COMPACT. PROOF ROLL IN THE PRESENCE OF THE ENGINEER.
7. MATERIAL STRIPPED TO DEPTH LOWER THAN SUBGRADE SHALL BE REPLACED WITH STRUCTURAL MATERIAL TO SUBGRADE ELEVATION.
8. WHERE UNSUITABLE SUBGRADE SOIL EXISTS, OVER EXCAVATION AND REPLACEMENT WILL BE REQUIRED. GEOGRID MAY BE SUBSTITUTED FOR OVER EXCAVATION UPON APPROVAL FROM GEOTECHNICAL ENGINEER.

TYPICAL CONCRETE SIDEWALK DETAIL

NOT TO SCALE REVISED TOJ ST-127

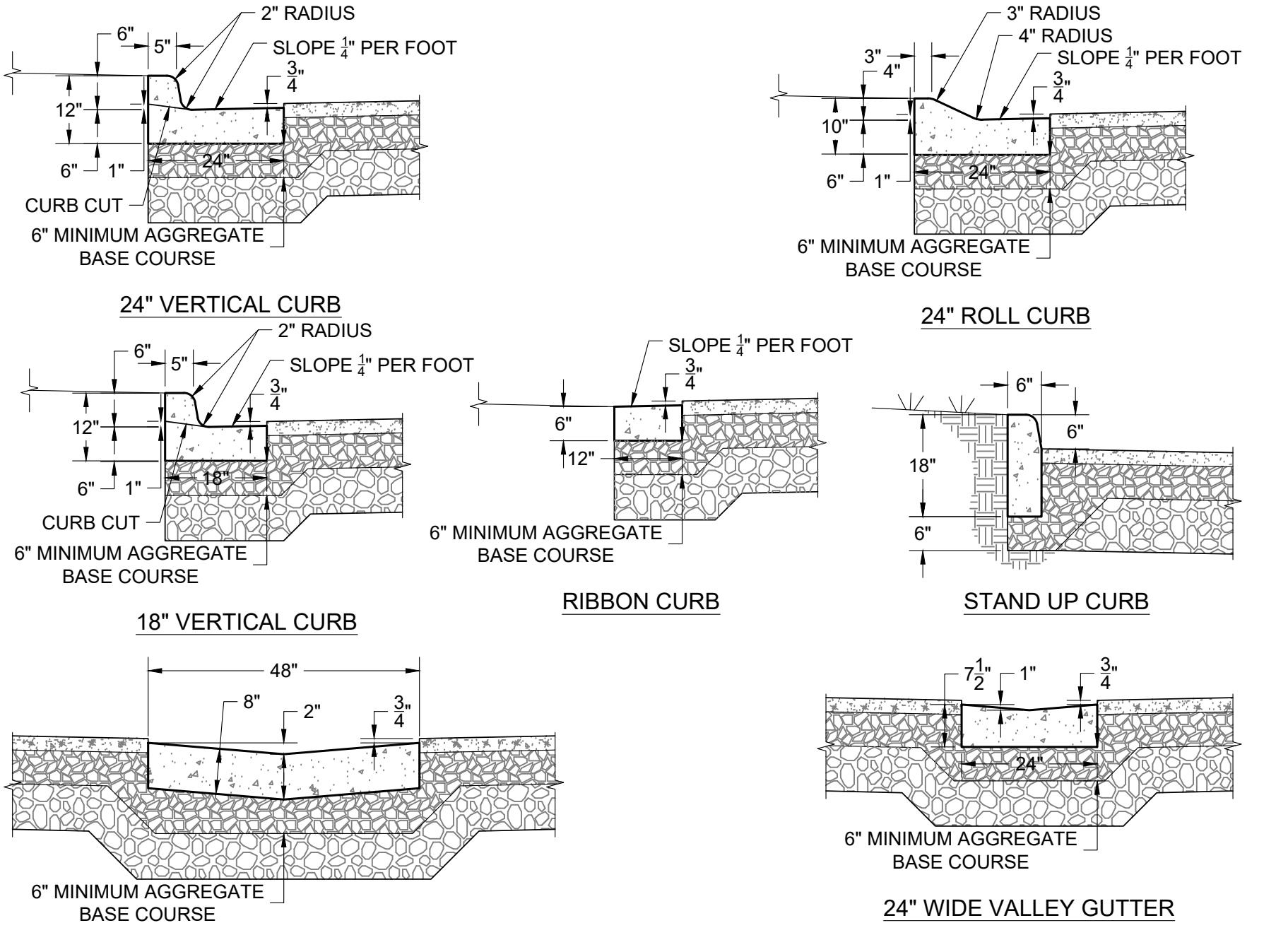


NOTE

1. DRIVEWAYS AND ALLEYWAY APPROACHES: LABELED AREAS ARE PAY LIMITS FOR "CONCRETE APPROACH" BID ITEM. THE FLARES AND/OR RAMPS SHALL BE THE SAME THICKNESS AS THE APPROACH.
2. RAMP SLOPE: DO NOT EXCEED A 1:12H RAMP SLOPE.
3. CROSS SLOPE: SLOPE SIDEWALK TOWARD STREET. DO NOT EXCEED 1X:50H (2%).

TYPE "A" DRIVEWAY/ALLEY APPROACH DETAIL

NOT TO SCALE TOJ ST-TBD

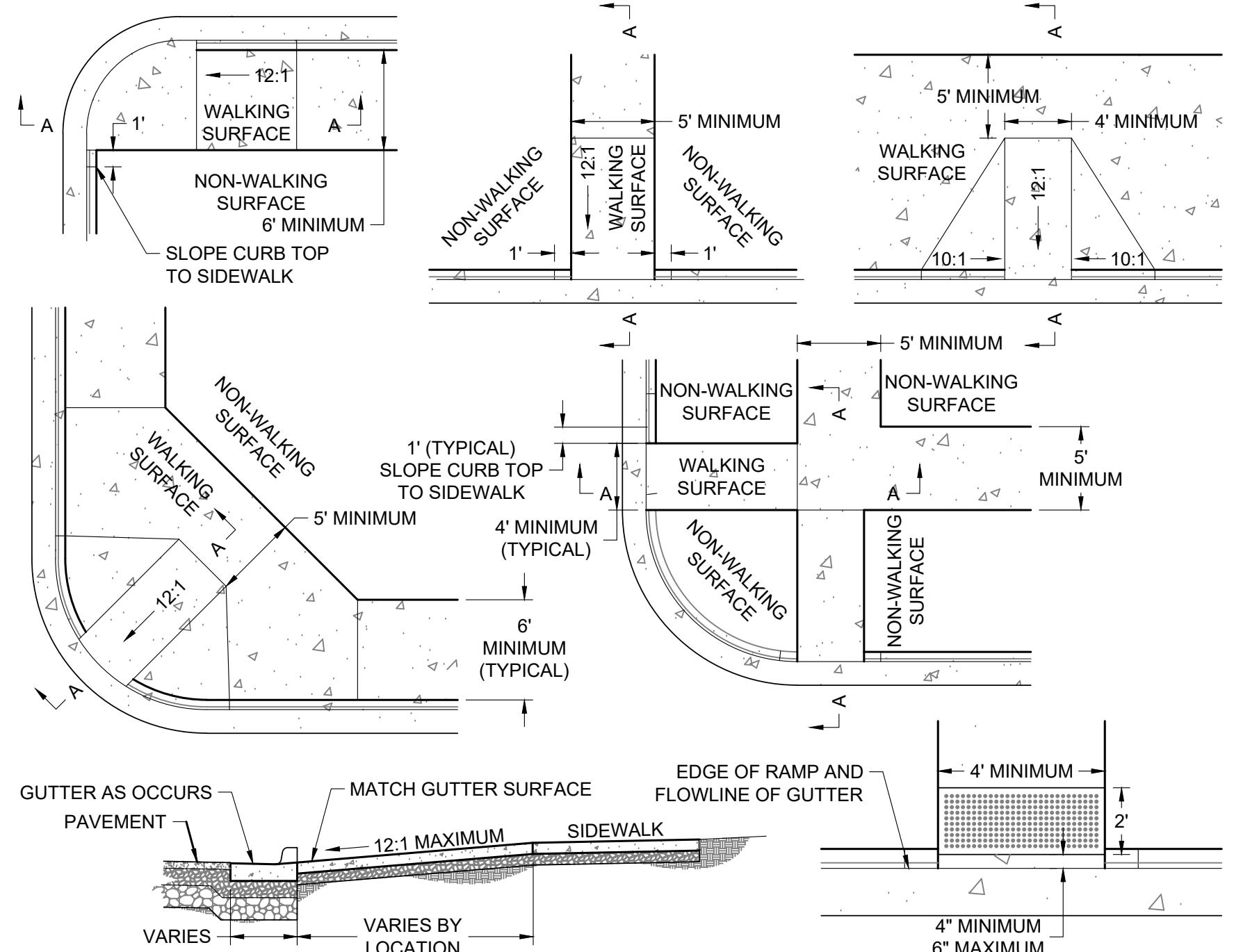
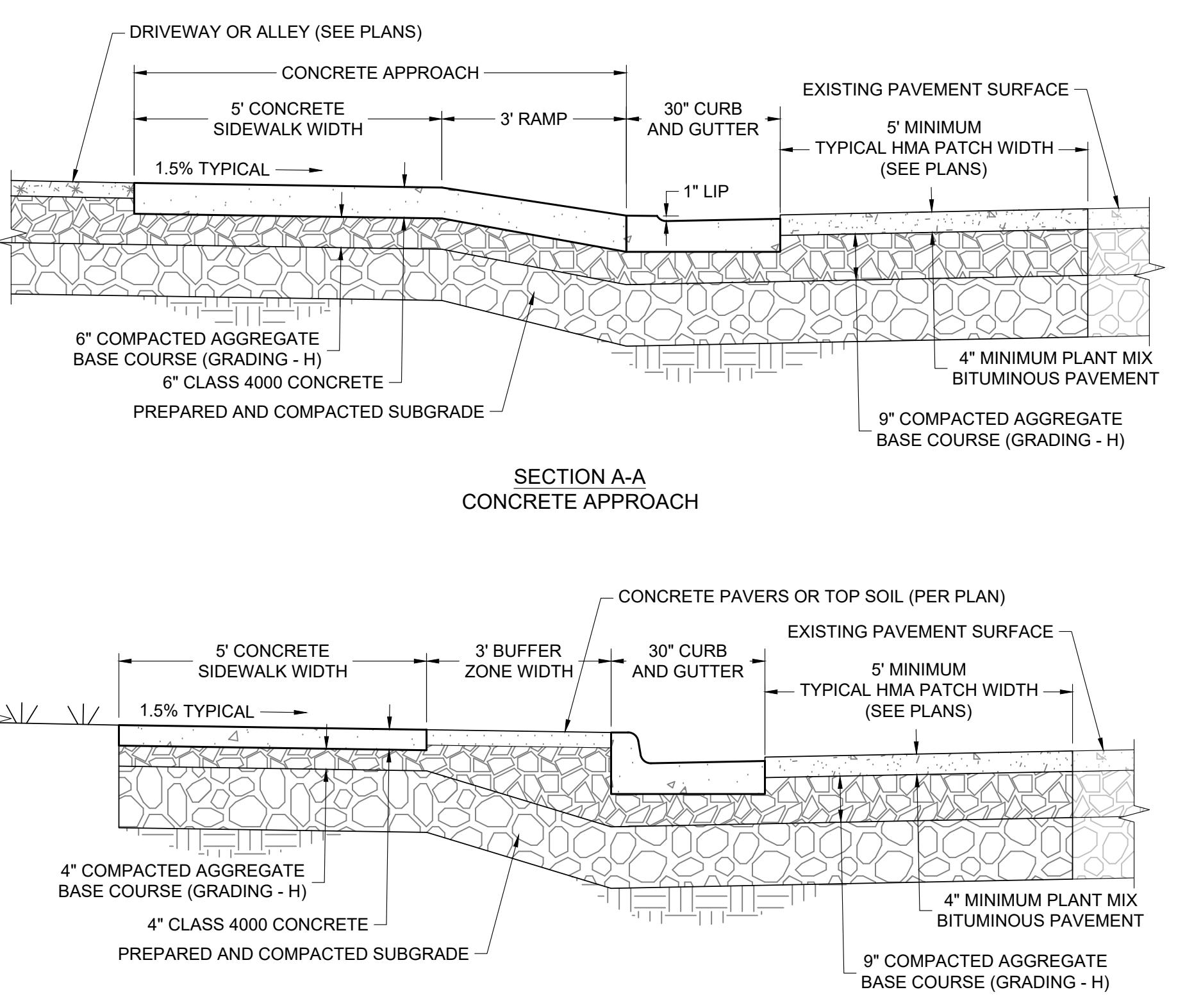


NOTE

1. CURBS SHALL CONFORM TO SPECIFICATION 32 16 13, EXCEPT THAT PORTLAND CEMENT CONCRETE SHALL BE FIBERMESH-REINFORCED CLASS 4000 CONCRETE CONFORMING WITH SPECIFICATION 32 13 13.
2. AGGREGATE BASE COURSE SHALL BE SIX INCH MINIMUM THICKNESS, CONFORM TO SPECIFICATION 31 05 16, PART 2.03, GRADING H, AND BE INSTALLED PER SPECIFICATION 32 11 23, PART 3.03.
3. REMOVAL AND REPLACEMENT OF CURB SHALL TAKE PLACE IN FULL PANELS.
4. VERTICAL CURB SHALL BE USED IN PREFERENCE TO ROLL CURB.

CURB SECTION DETAIL

NOT TO SCALE

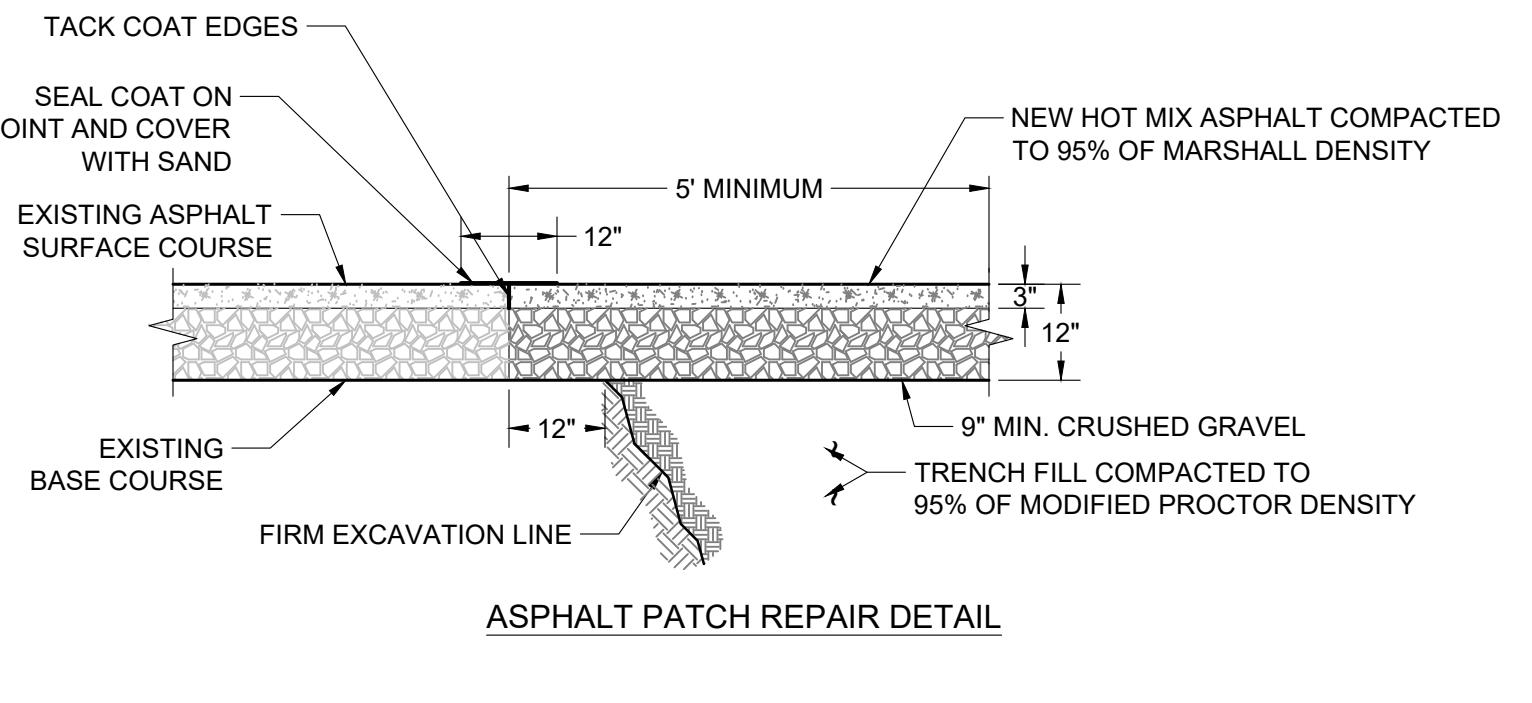


NOTE

1. SIDEWALKS SHALL CONFORM TO ALL APPLICABLE ADA STANDARD REQUIREMENTS.
2. LIP AT GUTTER TO BE NO MORE THAN $\frac{1}{2}$ " HIGH.
3. CONCRETE TO BE BROOM FINISH.
4. ALL PEDESTRIAN RAMPS SHALL INCLUDE PLACEMENT OF CAST IRON TRUNCATED DOME DETECTION PANELS IN A BRICK RED COLOR. (PANELS SHALL BE PROVIDED BY TOJ.)

PEDESTRIAN RAMP DETAIL

NOT TO SCALE TOJ ST-112

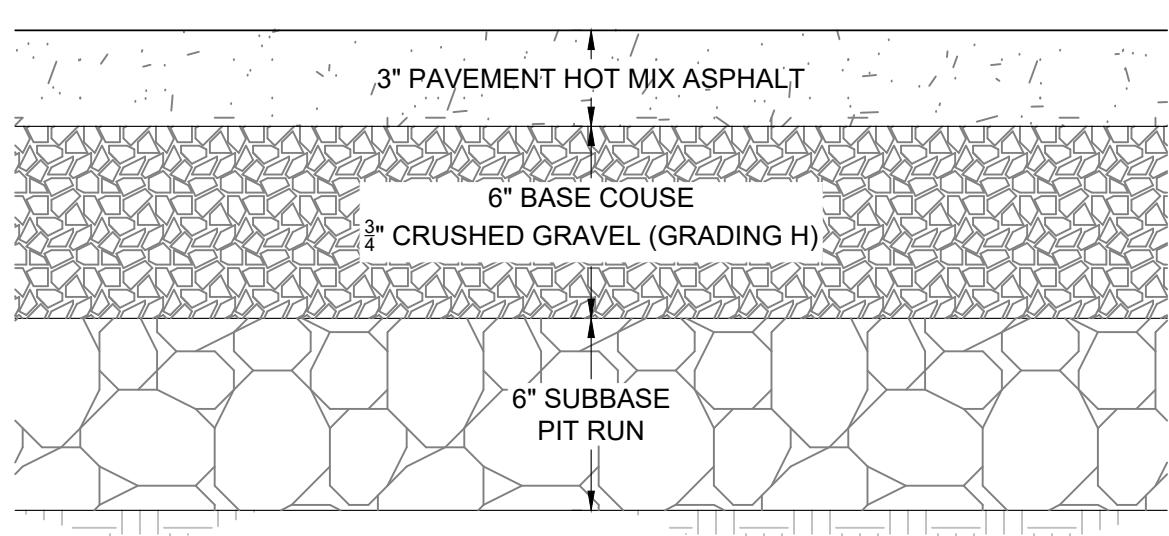


NOTE

1. REPLACEMENT ASPHALT SHALL BE 1" THICKER THAN EXISTING WITH A MINIMUM THICKNESS OF 3".
2. ASPHALT SHALL BE PLACED IN TWO (2) LIFTS, EACH NO LESS THAN $\frac{1}{2}$ " IN THICKNESS, AND COMPACTED TO 95% OF MARSHALL DENSITY.
3. BITUMINOUS MATERIAL SHALL MEET THE APPLICABLE REQUIREMENTS OF SECTION 02545 BITUMINOUS MATERIAL OF WYOMING PUBLIC WORKS STANDARDS AND SPECIFICATIONS.
4. PORTLAND CEMENT CONCRETE PAVEMENT SHALL MEET APPLICABLE REQUIREMENTS OF SECTION 02520, 02776 AND 03304 AS DIRECTED BY TOWN OF JACKSON PUBLIC WORKS DEPARTMENT.

CONCRETE AND ASPHALT PATCH DETAIL

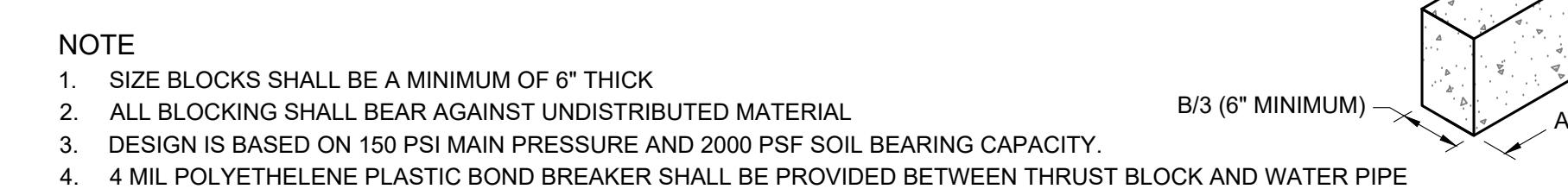
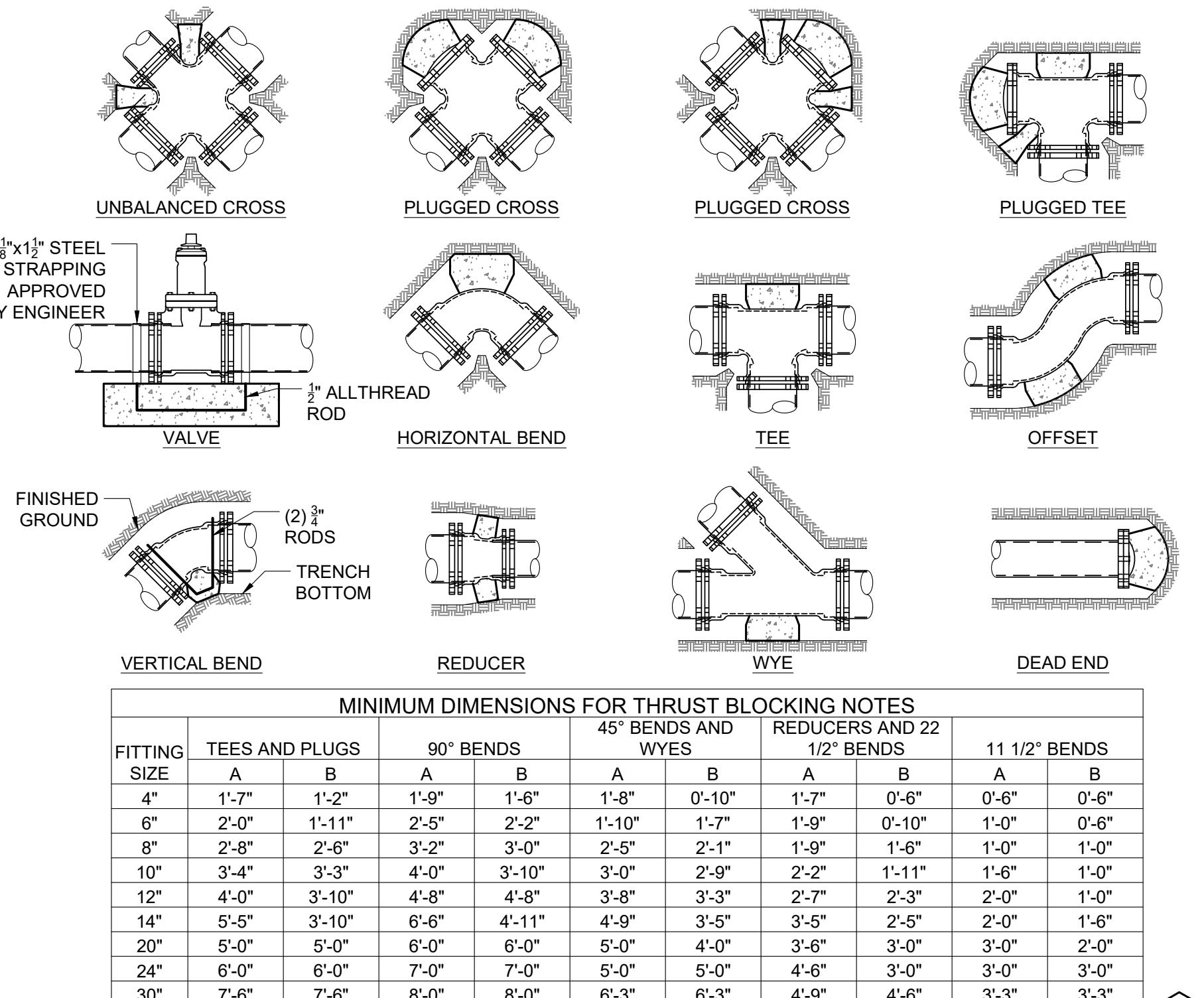
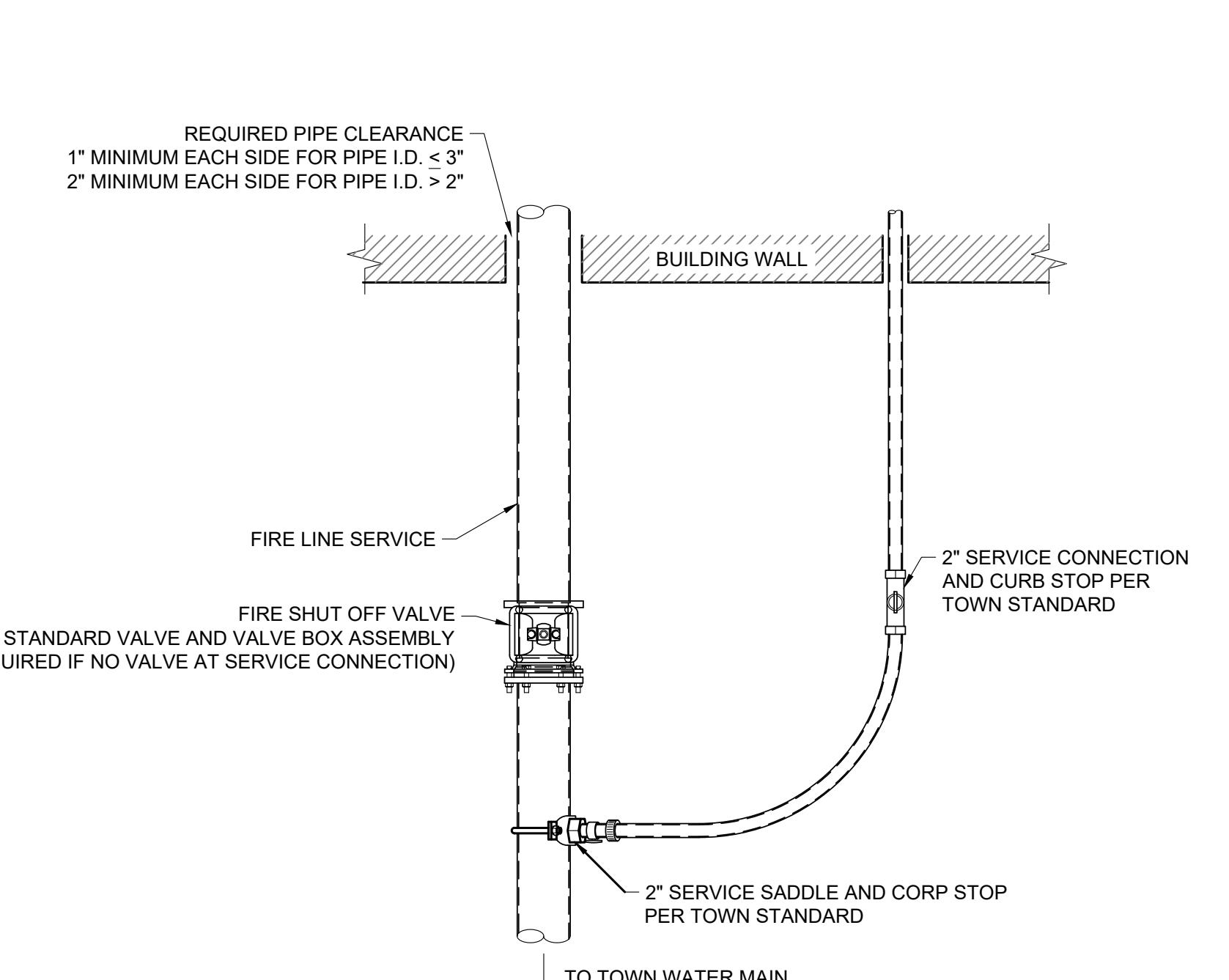
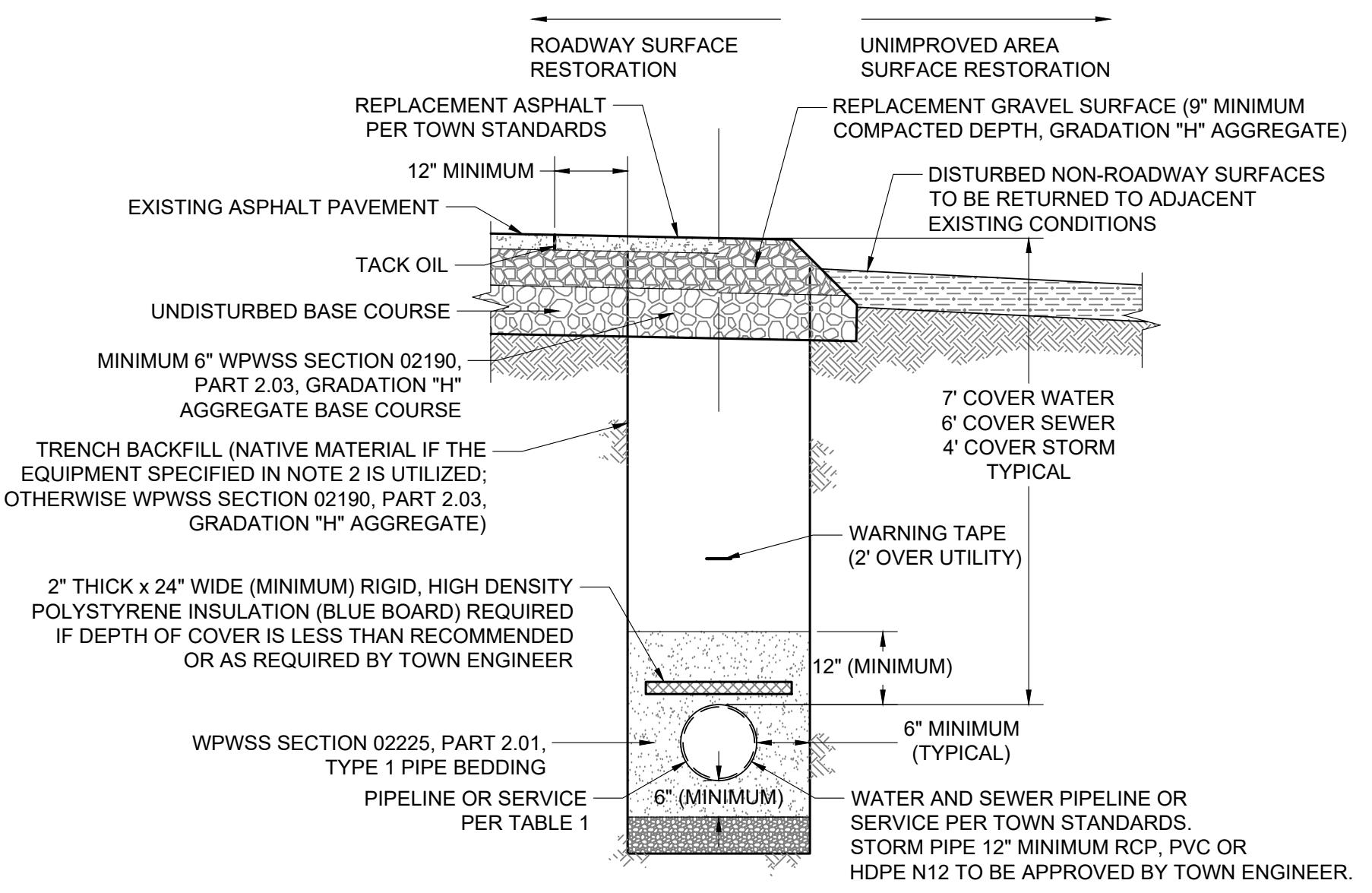
NOT TO SCALE TOJ ST-118



PAVEMENT SECTION

NOT TO SCALE

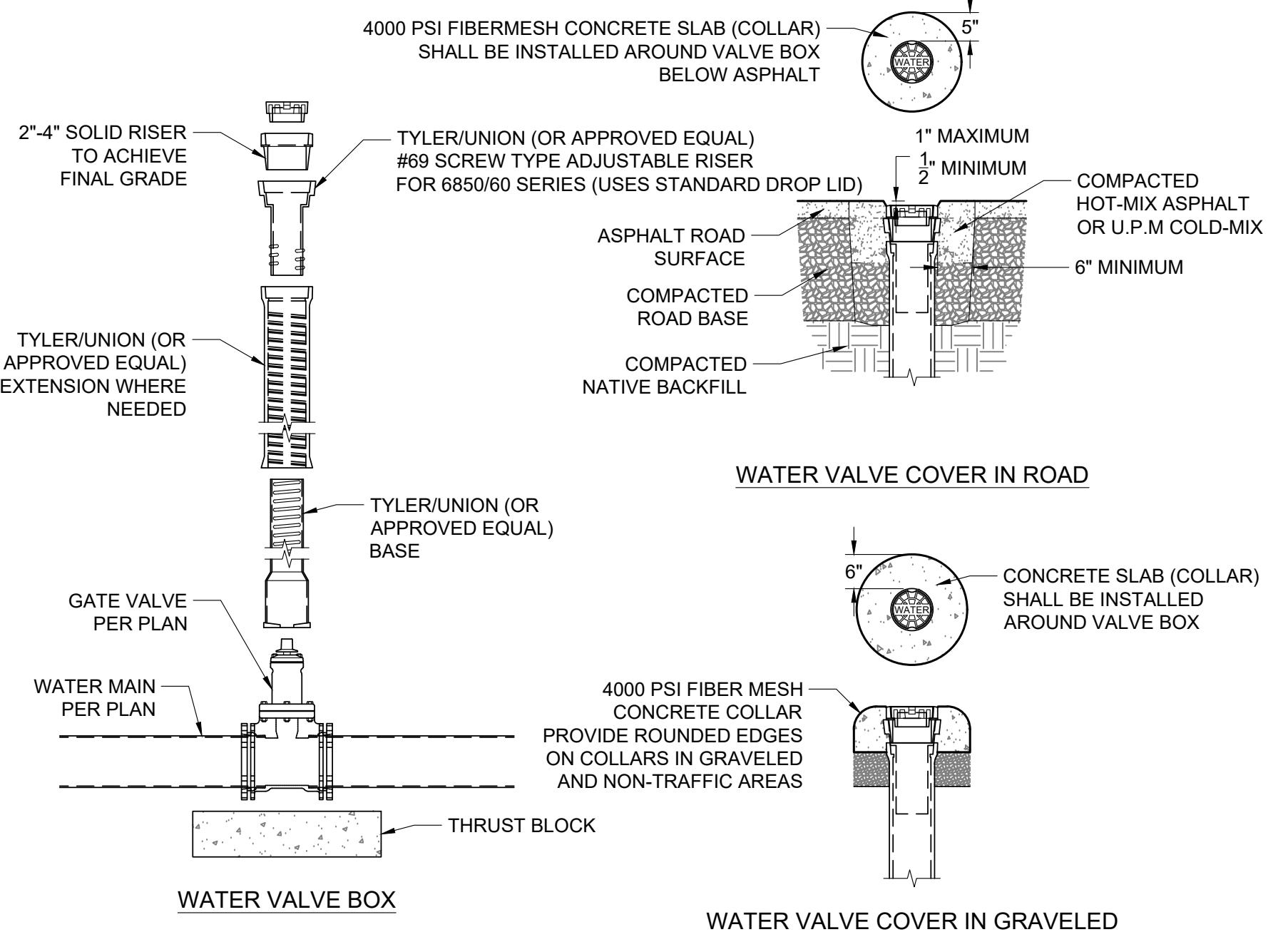
- PRELIMINARY -
SUBJECT TO CORRECTION
AND APPROVAL



PIPE TRENCH DETAIL
NOT TO SCALE
TOJ W-100, SS-100, STM-100

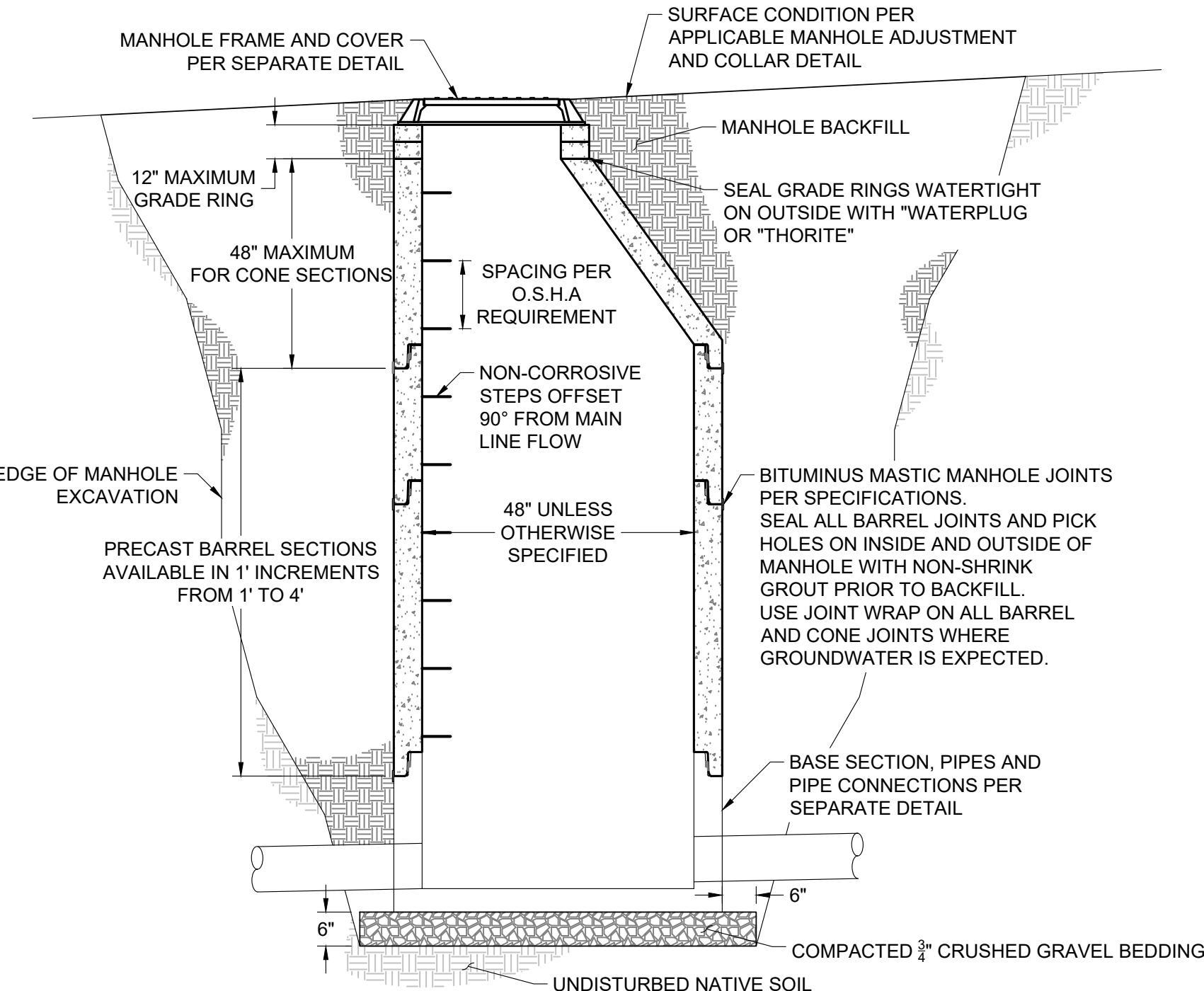
FIRE LINE WITH 2" WATER SERVICE DETAIL
NOT TO SCALE
TOJ W-110

THRUST BLOCK DETAIL
NOT TO SCALE
TOJ W-111



NOTE
1. ADJUST WATER VALVE BOX UPWARD OR DOWNWARD AS REQUIRED. FINAL ADJUSTMENT SHALL BE MADE AFTER PAVING AND BEFORE SEAL COATING.
2. THE TOWN SHALL INSPECT THE VERTICAL ALIGNMENT BEFORE AND AFTER BACKFILLING.
3. MUD PLUGS ARE REQUIRED TO BE PLACED IN ALL VALVE BOXES.

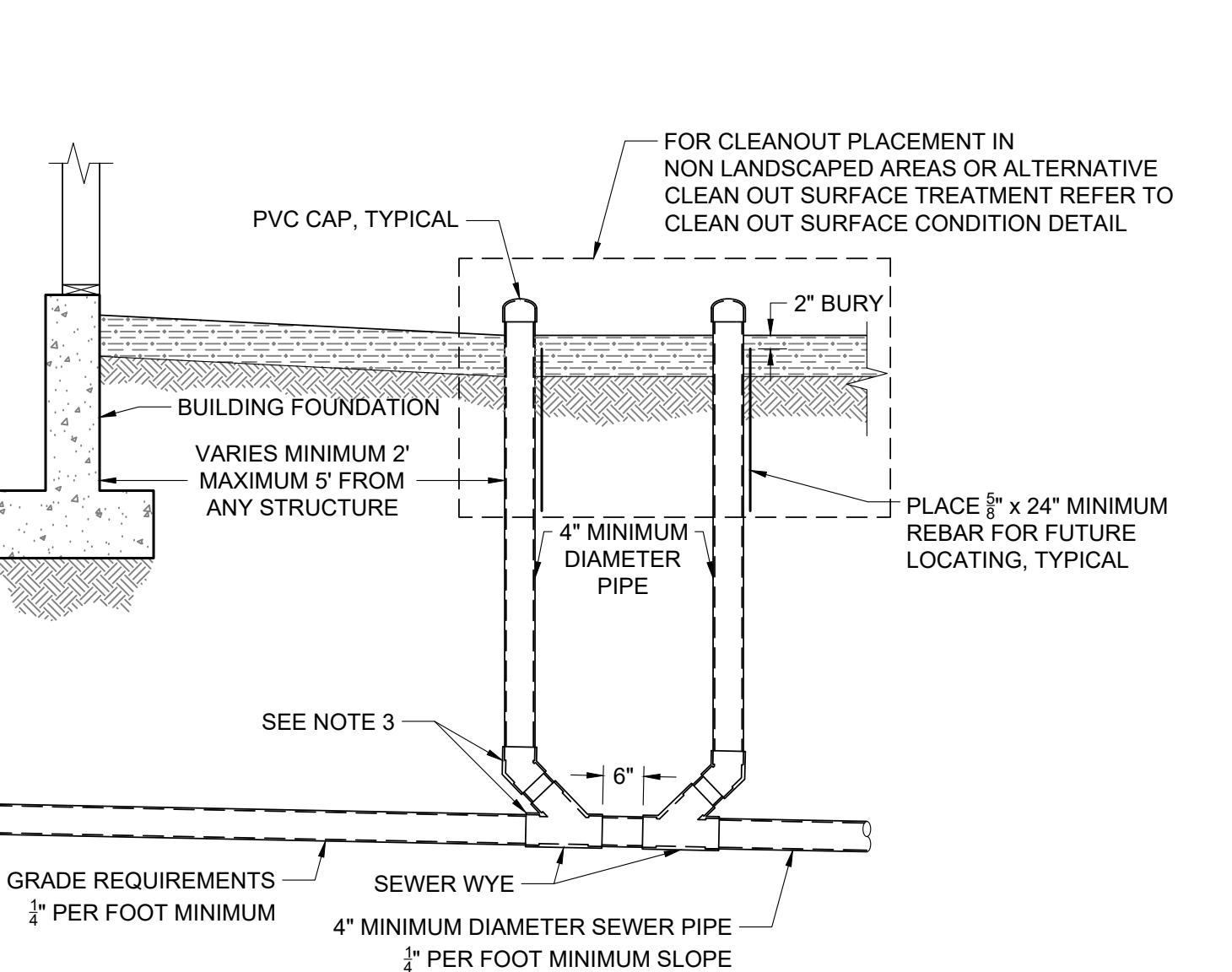
WATER GATE VALVE DETAIL
NOT TO SCALE
TOJ W-106



NOTE

1. REINFORCEMENT FOR PRECAST SECTIONS SHALL BE AS PER ASTM C-478.
2. C.I. RING AND COVER HS-20-44 ONE VENT HOLE OR AS SHOWN ON PLAN SHEET.
3. MOUND CRUSHED GRAVEL BASE AROUND TOP OF MANHOLE AT ROAD SURFACING LOCATIONS.
4. ALL SEWER MANHOLE BASES, BARRELS, AND CONES SHALL BE VACUUM TESTED.

STANDARD MANHOLE CONSTRUCTION DETAIL
NOT TO SCALE

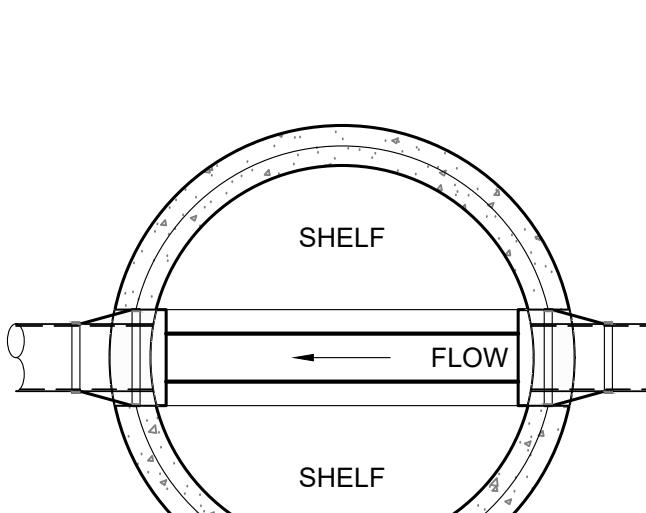


NOTE

1. PRIOR TO BACKFILLING THE TOWN PUBLIC WORKS DEPARTMENT MUST INSPECT ALL PIPE, FITTINGS, COUPLINGS, GRADE AND COMPLETE LEAK TESTING.
2. INSTALL AND COMPACT ALL BACKFILL MATERIAL PER TOWN PUBLIC WORKS DEPARTMENT STANDARD SPECIFICATIONS AND AS SHOWN WITHIN THE TRENCH DETAIL.
3. ALL PVC FITTINGS SHALL MEET ASTM D3034 SPECIFICATIONS, AND SHALL ALSO MEET ASTM D312 SPECIFICATIONS FOR RUBBER GASKETED BELL AND SPIGOT TYPE WITH INTEGRAL BELL.

BUILDING CLEANOUT DETAIL
NOT TO SCALE

REV TOJ SS-114

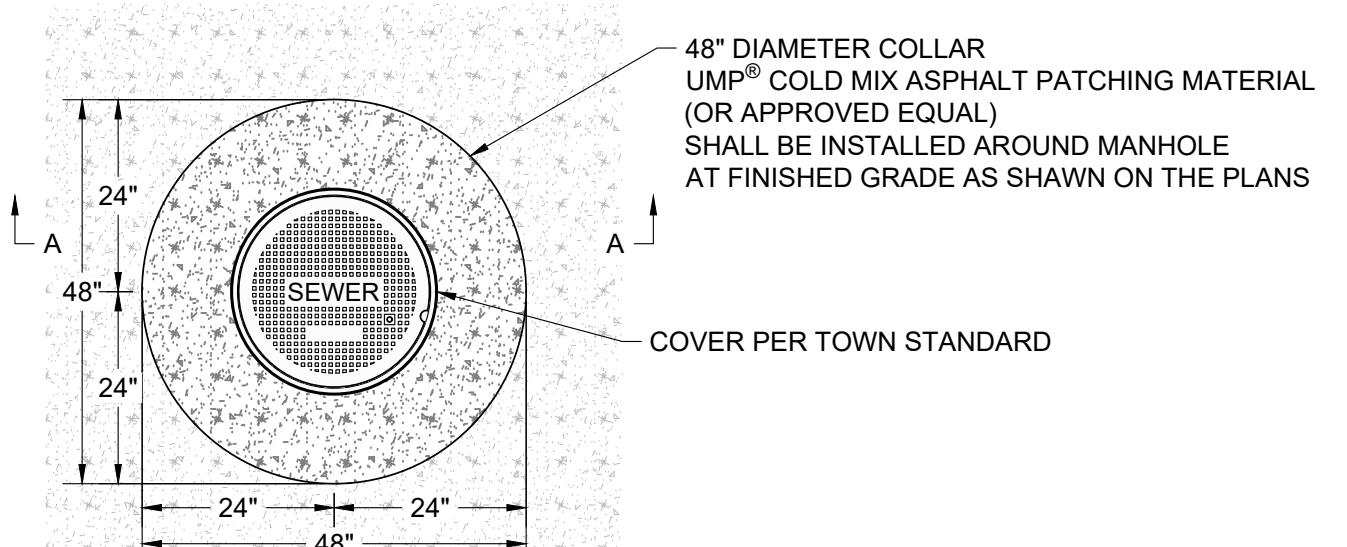


NOTE

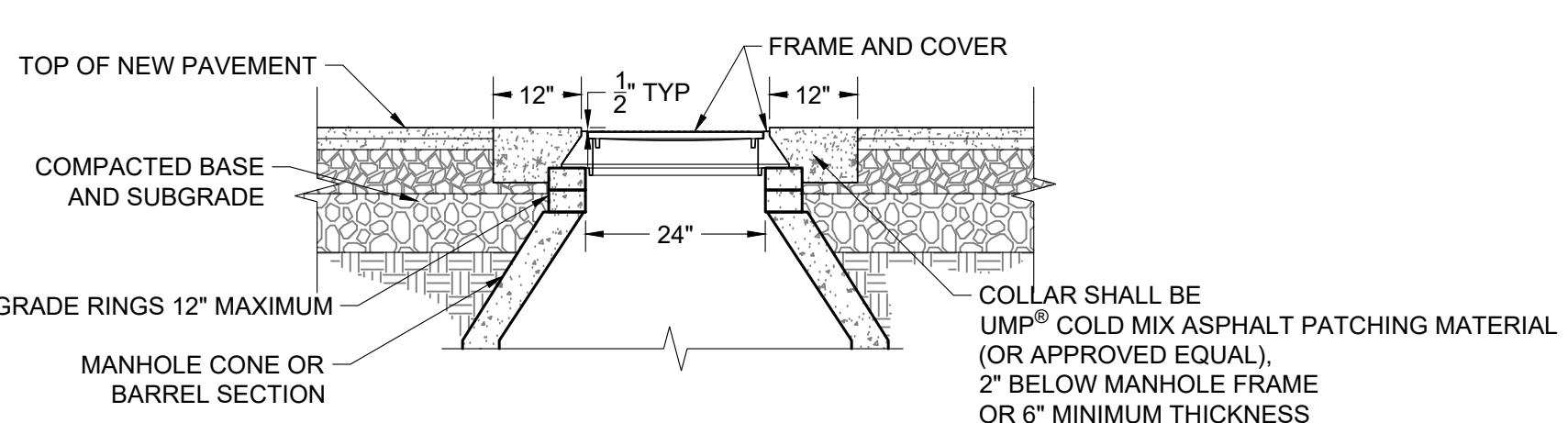
1. SLOPE ALL SHELVES TO CHANNEL AT 1" PER FOOT.
2. SLOPE OF CHANNEL SHALL BE 0.1% ACROSS MANHOLE FOR ALL MAIN RUNS. SEE PROFILE SHEETS.
3. MANHOLE INVERTS SHALL MATCH INFLOW AND OUTFLOW PIPE INVERTS
4. 4" SERVICE PIPES ENTERING MANHOLES SHALL HAVE CROWN OF 4" PIPE MATCH CROWN OF MAIN WHERE INVERTS INDICATE.

TYPICAL MANHOLE CHANNEL DETAILS
NOT TO SCALE

TOJ SS-107



COVER AND COLLAR PLAN



ADJUSTMENT DETAIL SECTION A-A

NOTE

1. ADJUST MANHOLE UPWARD WITH ADJUSTING RINGS UNDER FRAME. ADJUST MANHOLE DOWNWARD BY REMOVING A PORTION OF THE MANHOLE RISER AND REBUILDING TO PROPER HEIGHT. SLOPE MANHOLE RING AS REQUIRED TO MATCH STREET GRADE AND CROSS SLOPE. MAKE FINAL MANHOLE ADJUSTMENT AFTER PAVING AND BEFORE SEAL COATING.
2. IF SEWER MANHOLE IS WITHIN UNPAVED AREA USE TAPERED COLLAR. SEE TOWN SANITARY SEWER DETAIL SS-110.

MANHOLE ADJUSTMENT & COLLAR IN ASPHALT
NOT TO SCALE

TOJ SS-109

**TETON COUNTY
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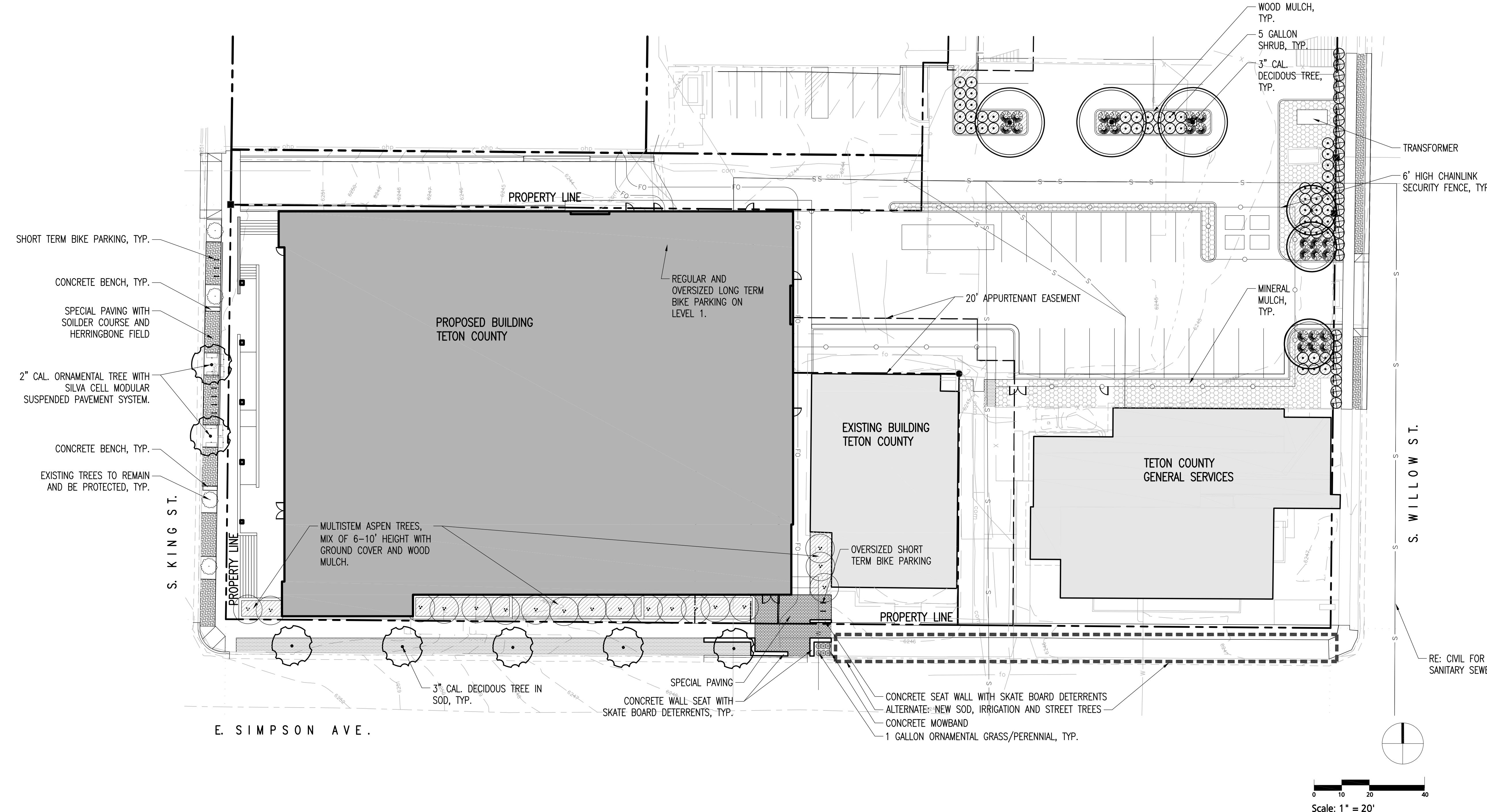
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NOTE: ALL PLANTINGS TO BE IRRIGATED WITH AUTOMATIC SYSTEM.



Scale: 1" = 20'

BIKE PARKING CALCULATIONS

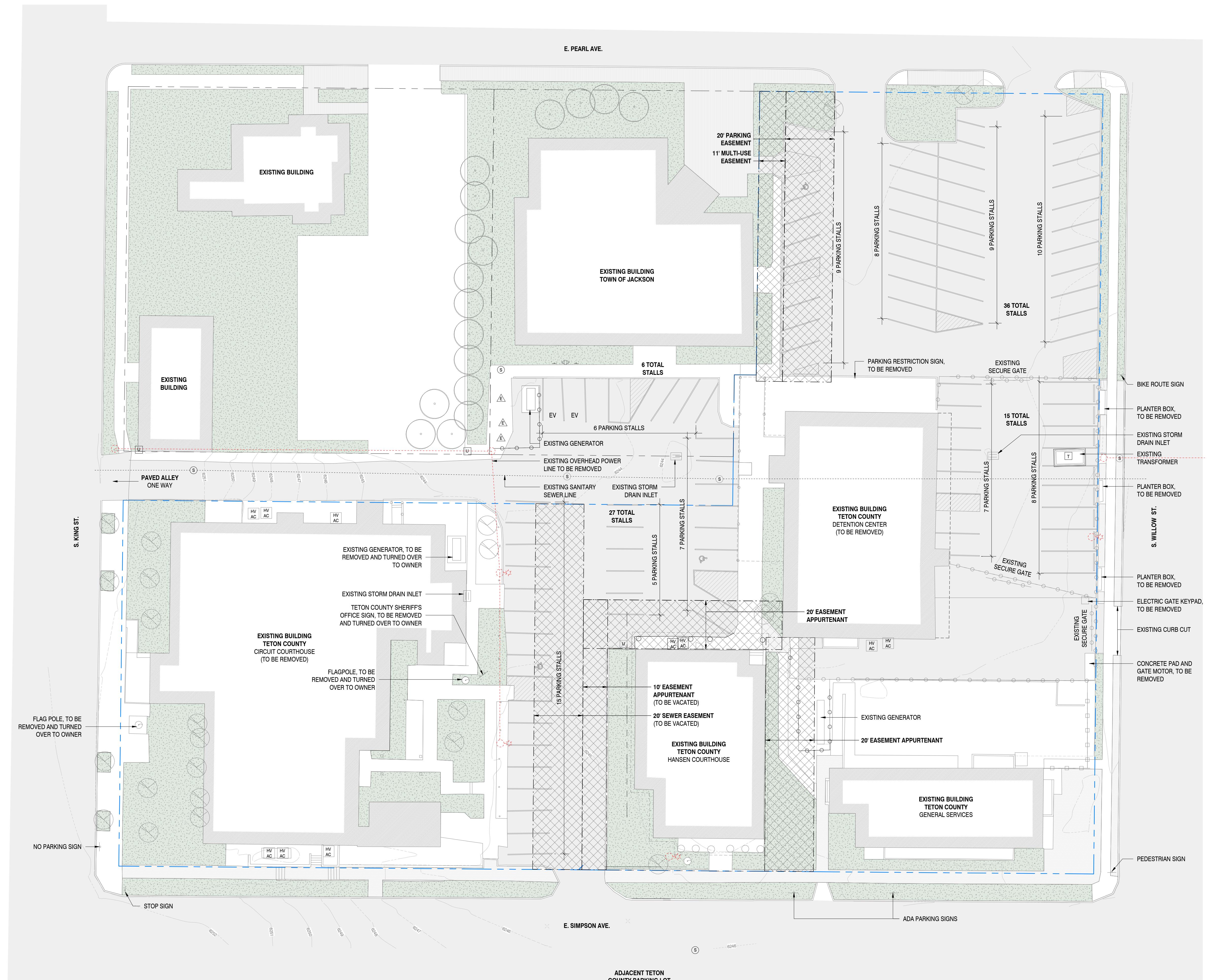
DESCRIPTION:	LDR REQUIRED:	PROVIDED:
	BIKE PARKING SPACES	BIKE PARKING SPACES
ASSEMBLY: 6,813 SF	1/2,200 SF	6,813/2,200=3 BIKE SPACES 1.5 SHORT TERM 1.5 LONG TERM (LOCATED LEVEL 1.5)
BUSINESS/OFFICE: 35,015 SF	1/1,650 SF	35,015/1,650=21 BIKE SPACES 15.75 SHORT TERM 5.25 LONG TERM (LOCATED LEVEL 1.5)
		TOTAL (17.25) 18 SHORT TERM (6.75) 7 LONG TERM (LOCATED LEVEL 1.5)

Issue
To: PRE-APPLICATION
CONCEPT PHOTOS PACKAGE
SCHEMATIC DESIGN
To: SKETCH PLAN
26 JUNE 2004
1 JULY 2004
06 DECEMBER 2004
04 MARCH 2005

Project Number: 24-031
Drawn By: Author
Reviewed By: Checker
Approved By: Approver

Landscape Plan

L-301



ZONING | 4.2.1 PUBLIC / SEMI-PUBLIC - TOWN (P/SP-ToJ)

1. STRUCTURE LOCATION AND MASS
 A. LSR (MIN): N/A
 B. LOT COVERAGE (MAX): N/A
 C. STREET SETBACK (MIN): N/A
 D. SIDE SETBACK (MIN): N/A
 E. REAR SETBACK (MIN): N/A
 F. HEIGHT (MAX): N/A
 G. STORIES (MAX): N/A
 H. STORIES (LO) (MAX): N/A
 I. FAR (MAX): N/A

2. MAXIMUM SCALE OF DEVELOPMENT: N/A

3. BUILDING DESIGN
 NON-RESIDENTIAL BUILDING DESIGN GUIDELINES (DIV.5.8)
 - North Half of Lot 2 in 'Downtown Design Area 2'

4. SITE DEVELOPMENT: N/A
 5. LANDSCAPING (DIV. 5.9): N/A
 6. FENCING: N/A
 7. ENVIRONMENTAL STANDARDS
 NATURAL RESOURCE BUFFERS
 • CACHE CREEK: 20' N/A
 • FLAT CREEK NORTH OF HANSEN AVE. 25': N/A
 • FLAT CREEK SOUTH OF HANSEN AVE. 50': N/A
 • WETLAND: 30' N/A
 • WILDLAND SETBACK (MIN) (7.7.4.D): N/A
 • NATURAL RESOURCE OVERLAY (NRO) (SEC 5.2.1): N/A

8. SCENIC STANDARDS
 EXTERIOR LIGHTING
 • LIGHT TRESPASS PROHIBITED
 • LIGHTS OVER 600 INTL LUMENS FULLY SHIELDED
 • LUMENS PER SF OF DEV. (MAX): 3 LUMENS
 • LUMEN PER SITE (MAX): 10,000 LUMENS
 • UNSHIELDED FIXTURES: 10,000 LUMENS
 • LIGHT COLOR: <3000 KELVIN

9. NATURAL HAZARDS TO AVOID
 PROJECT IS NOT LOCATED WITH:
 • STEEP SLOPES (LOT AVERAGE CROSS-SLOPE \geq 10%)
 • AREAS OF UNSTABILE SOILS
 • FAULT AREAS
 • FLOODPLAINS
 • WILDLAND URBAN INTERFACE

10. SIGNS (DIV. 5.6): N/A

11. GRADING, EROSION CONTROL, STORMWATER
 GRADING (SEC. 5.7.2)
 EROSION CONTROL (SEC. 5.7.3)
 • EROSION SHALL BE CONTROLLED AT ALL TIMES
 STORM WATER MANAGEMENT (SEC. 5.7.4)
 • NO INCREASE IN PEAK FLOW RATE OR VELOCITY
 ACROSS PROPERTY LINES

12. REQUIRED PHYSICAL DEVELOPMENT PERMITS
 • SKETCH PLAN (SEC. 8.3.2)
 • DEVELOPMENT PLAN (SEC. 8.3.3)
 • BUILDING PERM (SEC. 8.3.4)
 • DRC REVIEW (SEC. 12.6)

PARKING REQUIREMENTS (TABLE 4-5)
 INSTITUTIONAL & ASSEMBLY USES
 (COURTS, INSTITUTIONAL OFFICES, DETENTION
 CENTER)
 RATIO: INDEPENDENT CALCULATION BASED ON
 EXISTING USE

EXISTING ON-SITE PARKING COUNT
 103 STANDARD STALLS
 4 ACCESSIBLE STALLS
 10 TOTAL STALLS
 SHARED TOWN & COUNTY LOT: 36 STALLS
 INCLUDING TOJ PARKING EASEMENT: 9 STALLS
 TOWN LOT: 6 STALLS
 TETON COUNTY LOTS: 67 STALLS

PROPOSED ON-SITE PARKING COUNT
 115 STANDARD STALLS
 5 ACCESSIBLE STALLS INCLUDING 1 VAN SPACE
 120 TOTAL STALLS
 SHARED TOWN & COUNTY LOT: 51 STALLS
 INCLUDING TOJ PARKING EASEMENT: 10 STALLS
 TOWN LOT: 9 STALLS
 TETON COUNTY LOTS: 60 STALLS

TOTAL PROPERTY PARKING INCREASED BY 11 STALLS.
 NO ANTICIPATED CHANGE OF USE OR DEMAND

EVSE REQUIREMENTS (DIV. 6.2.2)
 USE: OTHER COMMERCIAL
 ELEC CAPABLE: 30%
 EVSE INSTALLED: 5%

36 STALLS
 6 STALLS

LEGEND

PROJECT PROPERTY LINE
 ADJACENT PROPERTY LINES
 EXISTING EASEMENTS
 EXISTING SANITARY SEWER LINE
 SEWER MANHOLE
 ELECTRIC TRANSFORMER
 UTILITY VAULT
 TELEPHONE BOX
 CHAIN LINK FENCE
 WOOD FENCE
 SITE LIGHT POLE
 OVERHEAD POLE
 PROPOSED BUILDING ENTRY/EXIT

WORK NOTES

Issue
 ToJ: PRE-APPLICATION
 CONCEPT PRICING PACKAGE
 SCHEMATIC DESIGN
 ToJ: SKETCH PLAN

**AndersonMasonDale
Architects**

**TETON COUNTY
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24-031

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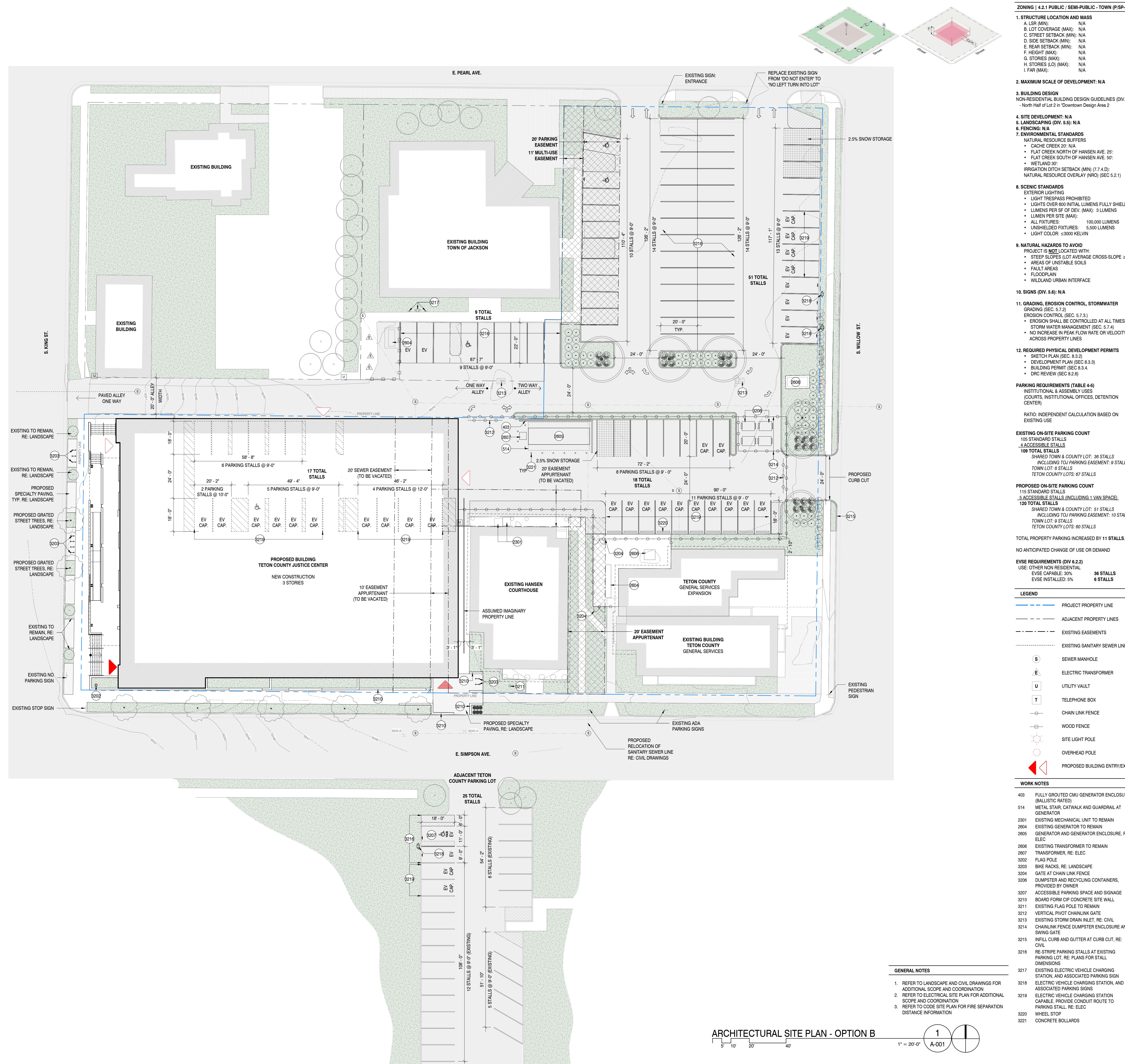
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Ward
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 Greenwood Village, CO 80111
 Telephone: 303-378-4390

Project Number: 24-031
 Drawn By: AMW
 Reviewed By: STH
 Approved By: JRT

**EXISTING/CONCEPT DEMO SITE
PLAN**

A-000



**TETON COUNTY
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24-031

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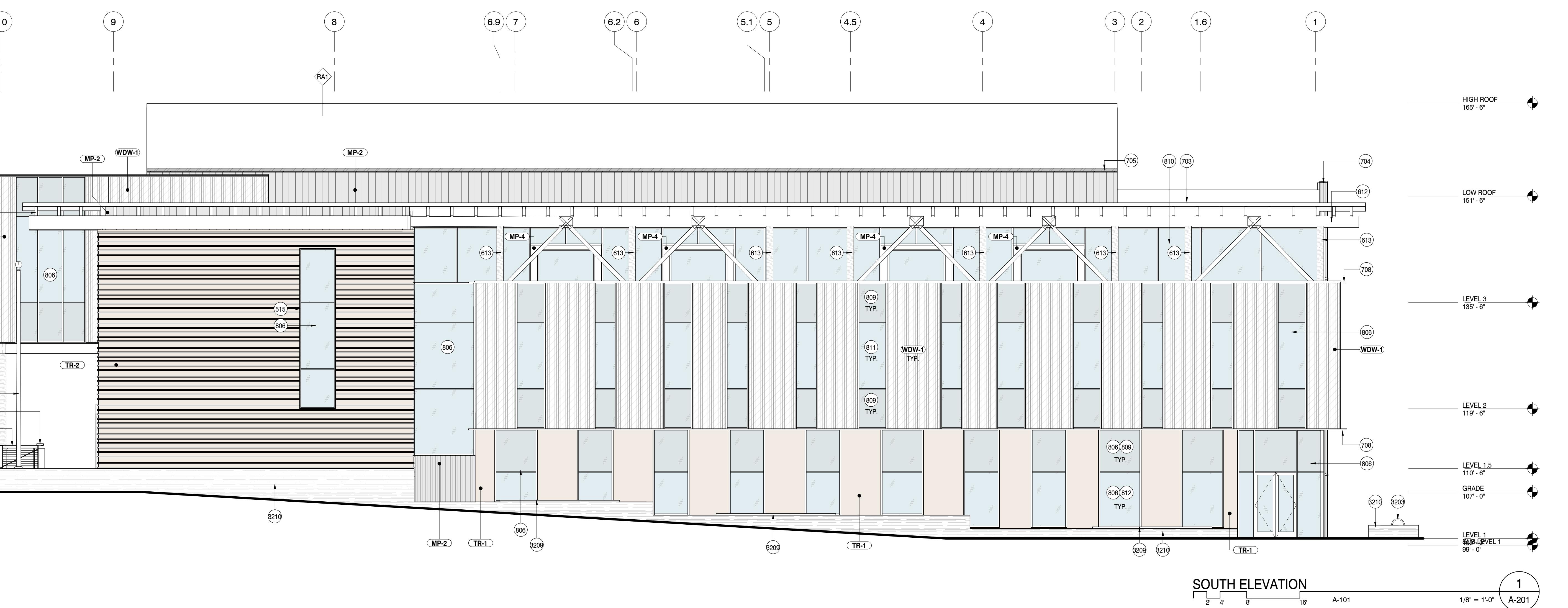
Issue
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Date
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Project Number:
Drawn By:
Reviewed By:
Approved By:

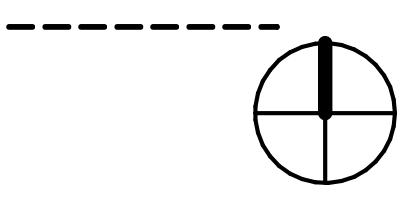
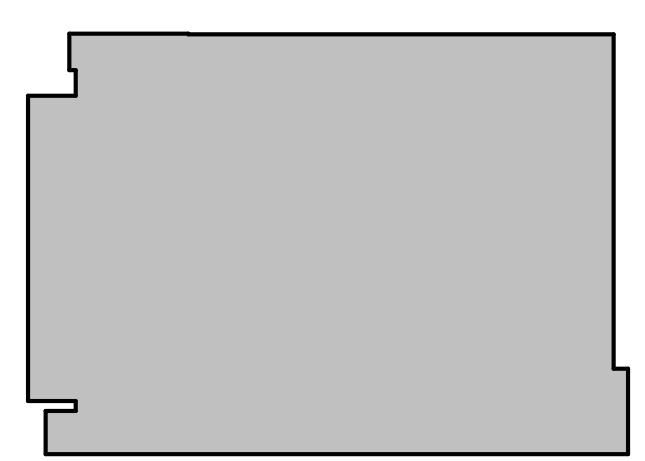
24-031
ICM
STH
JRT

SOUTH EXTERIOR ELEVATION
AND PERSPECTIVE

A-201



SOUTH ELEVATION
2' 4" 8' 10' 16' A-101 1/8" = 1'-0" A-201



**TETON COUNTY
JUSTICE CENTER**

180 S King Street
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24-031

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WORK NOTES

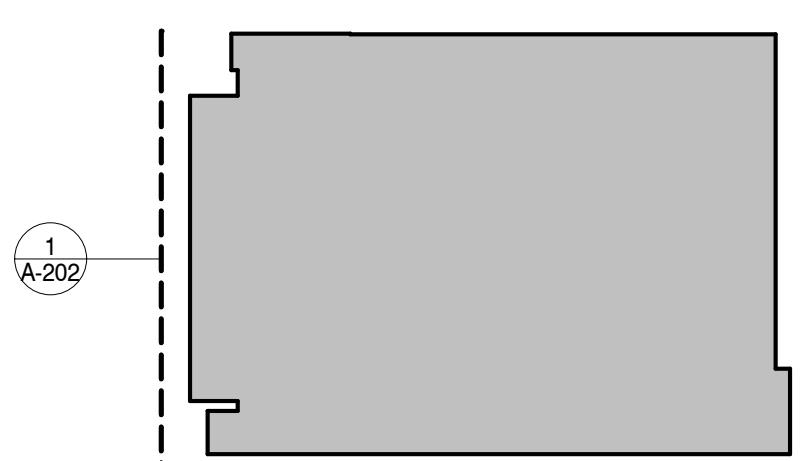
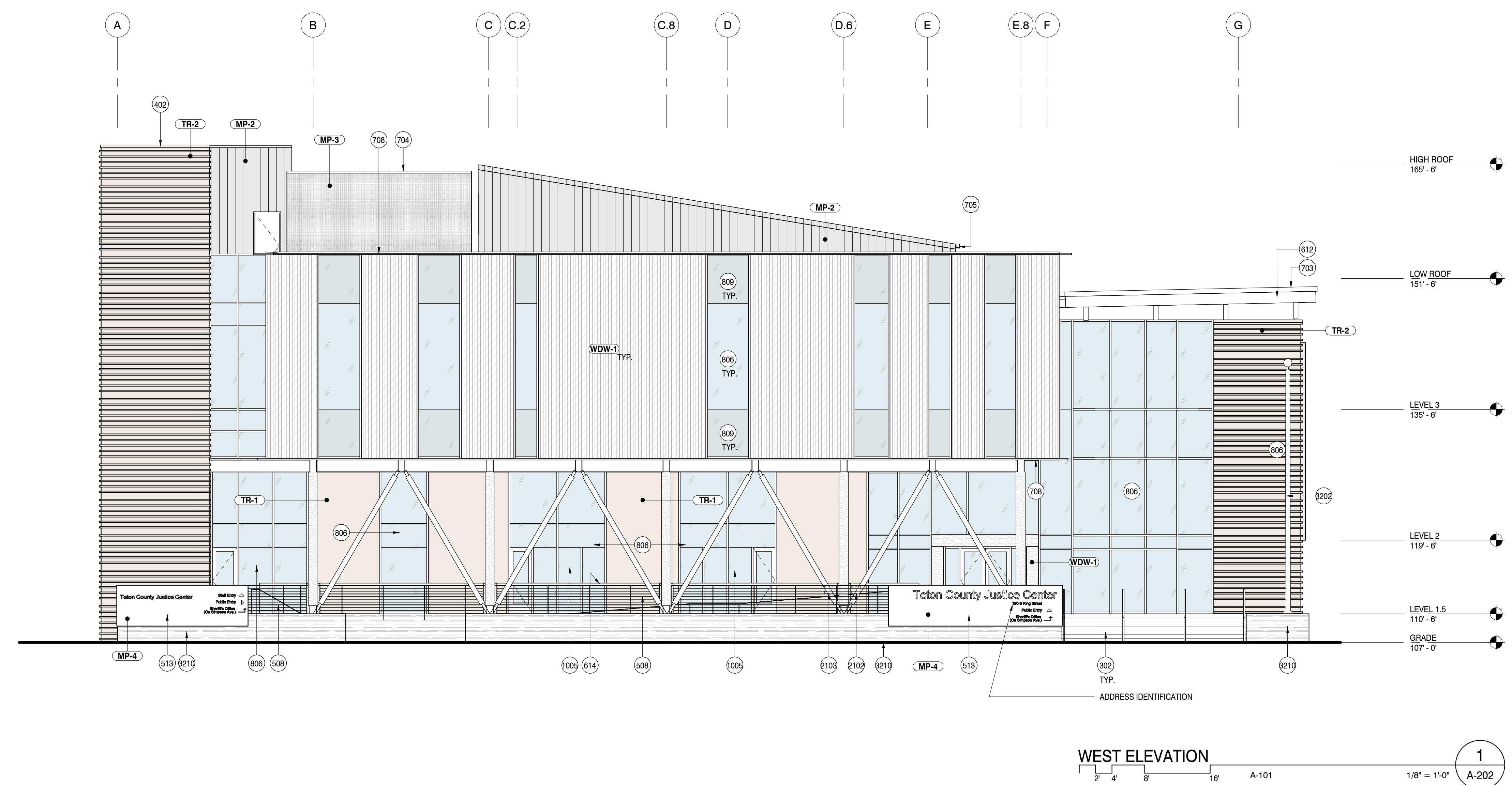
302 CIP CONCRETE STAIR WITH DECORATIVE METAL HANDRAIL
402 PRE-CAST COPING
508 DECORATIVE METAL GUARDRAIL
513 HOT ROLLED STEEL BUILDING IDENTITY SIGN
612 GLULAM ROOF/CEILING FRAMING, RE: STRUCTURE
614 2X12 DOUG FIR HAND RAIL CAP
703 ROOF EDGE, METAL GRAVEL STOP DETAIL
704 METAL COPING
705 GUTTER AND RAIN LEADER TO LOW ROOF
708 STEEL PLATE AND METAL COPING DETAIL
806 GLAZED ALUMINUM CURTAINWALL ASSEMBLY
809 GLAZED ALUMINUM CURTAINWALL SPANDREL GLASS
1005 GLAZED GLASS PANEL PARTITIONS
2102 FIRE SUPPRESSION
2103 FIRE DEPARTMENT CONNECTION, RE: FIRE SUPPRESSION
3202 FLAG POLE
3210 BOARD FORM CIP CONCRETE SITE WALL

LEGEND

TR-1	FLAT TERRA COTTA PANEL
BRK-1	ALTERNATE - ANCHORED STONE VENEER
TR-2	FLUTED TERRA COTTA PANEL
BRK-1	ALTERNATE - ANCHORED STONE VENEER
WDW-1	WOOD LOOK ALUMINUM SIDING ALTERNATE - THERMALLY MODIFIED TAG WOOD SIDING
MP-1	METAL PANEL - DARK
MP-2	METAL PANEL - LIGHT
MP-3	PERFORATED METAL PANEL
MP-4	BLACKENED HOT ROLLED STEEL

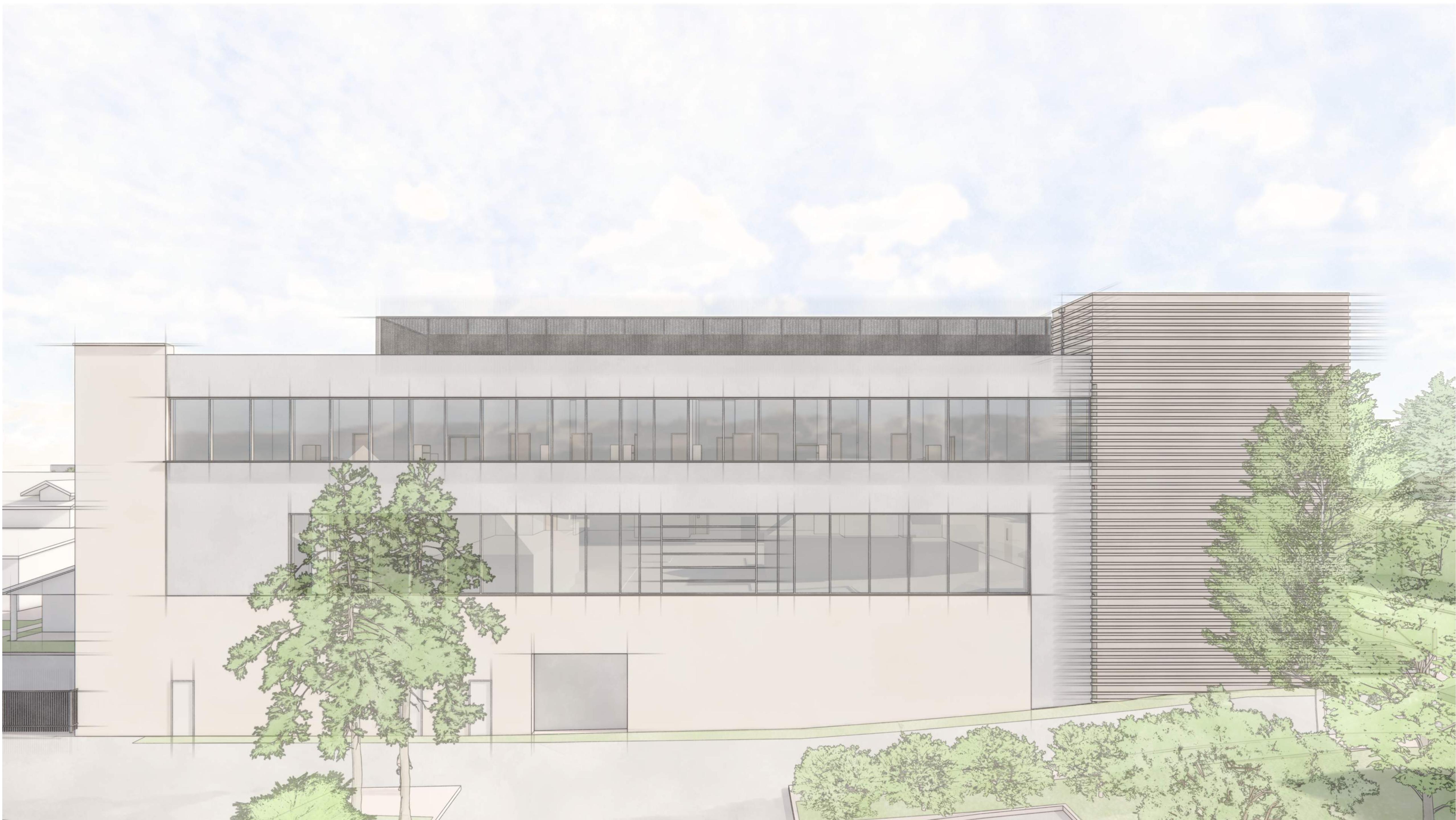
GENERAL NOTES

1. RE: PPD FOR MATERIAL QUALITY AND BASIS OF DESIGN MATERIALS / PRODUCTS
2. RE: PPD FOR DESCRIPTION OF SILLS, COPINGS, FLASHING, AND TRIM



WEST EXTERIOR ELEVATION AND PERSPECTIVE

A-202



AndersonMasonDale Architects

WORK NOTES

CIP CONCRETE STAIR WITH DECORATIVE METAL
HANDRAIL

PRE-CAST COPING

DECORATIVE METAL GUARDRAIL

HOT ROLLED STEEL BUILDING IDENTITY SIGN

2X12 DOUG FIR HAND RAIL CAP

METAL COPING

PRE-FORMED METAL SILL

STEEL PLATE AND METAL COPING DETAIL

AUTOMATIC OVERHEAD COILING DOOR

GLAZED ALUMINUM CURTAINWALL ASSEMBLY

AUTOMATIC ALL GLASS OVERHEAD COILING
DOOR

GLAZED ALUMINUM CURTAINWALL, SPANDREL
GLASS

BOARD FORM CIP CONCRETE SITE WALL

TETON COUNTY JUSTICE CENTER

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24-031

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issue
oJ: PRE-APPLICATION
CONCEPT PRICING PACKAGE
SCHEMATIC DESIGN

Project Number:
Drawn By:
Reviewed By:
Approved By:

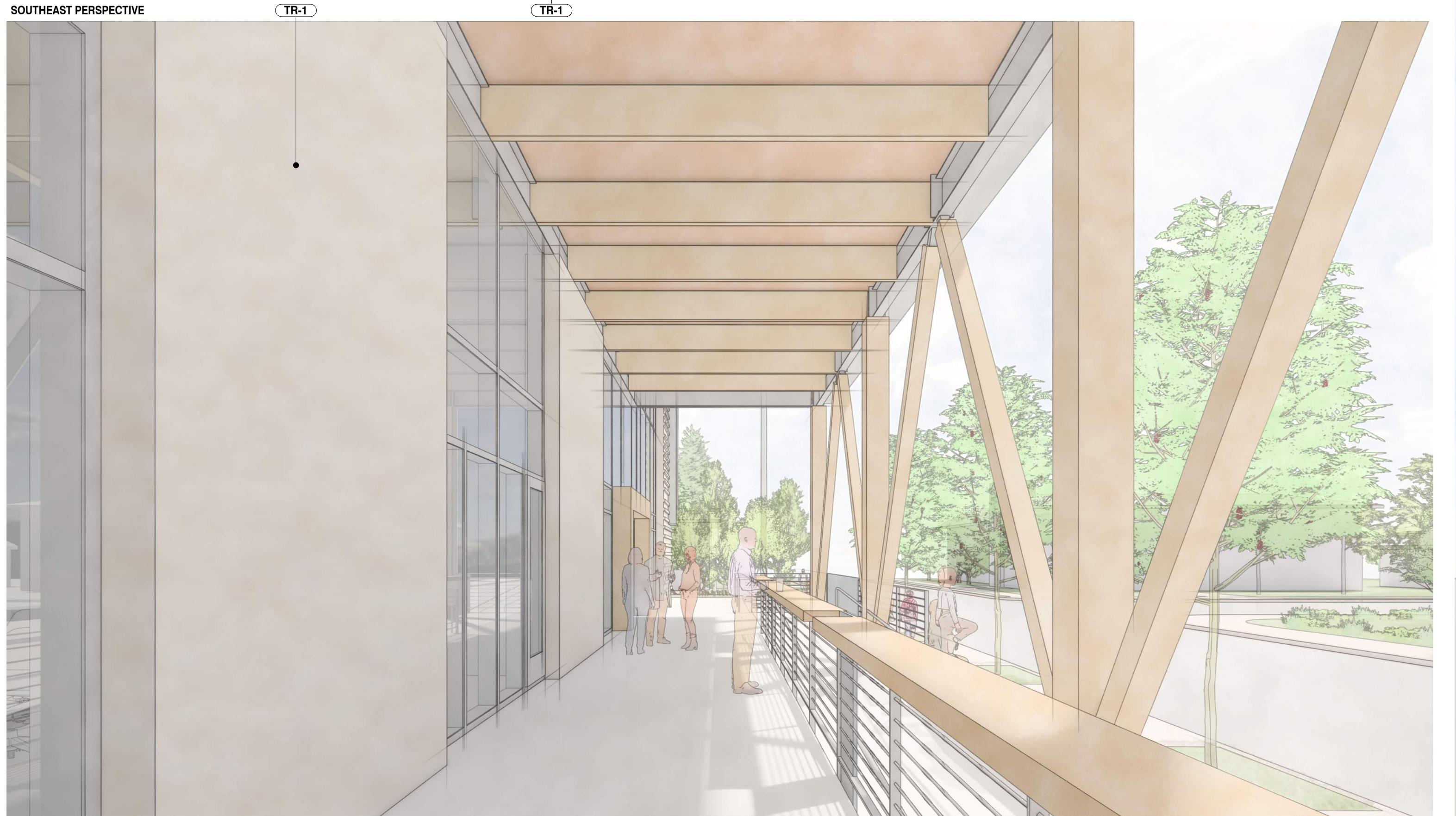
NORTH EXTERIOR ELEVATION AND PERSPECTIVE

NORTH ELEVATION

A circular callout label with a vertical line pointing down to a dashed horizontal line. The label contains the number '1' at the top and the text 'A-203' below it.

NORTH EXTERIOR ELEVATION AND PERSPECTIVE

A-203



Fasching Consulting



March 5th, 2025

Mr. Brian Simon, PE
Senior Principal Engineer
Jorgensen
1315 Highway 89 S., Suite 201
PO Box 9550
Jackson, WY 83002

**RE: Traffic Impact Study
Teton County Courthouse Redevelopment (Fasching Consulting Job 24-15)**

Dear Mr. Simon:

This traffic impact study provides transportation information associated with the Teton County Courthouse campus located in Jackson, WY between Simpson Avenue, Pearl Avenue, Willow Street, and King Street. The site location is shown in **Figure 1**.

The County is planning to redevelop the Courthouse and detention facility such that these two buildings are razed and a combined building will be constructed where the Courthouse currently stands. The facility will provide a greater number of hearing rooms as well as expanded detention holding facilities. A site plan is shown in **Figure 2**, and the redevelopment would afford a combined parking area that will serve the Courthouse and Town Hall (they are separate today). Vehicular access to the campus will be mostly similar as it exists today including:

- One-way alley (outbound) onto King Street, along the north side of the Courthouse
- Two driveways onto Pearl Avenue that will be converted to two-way flows and serve the entire campus.
- One secured driveway onto Willow Street (although the secured area it serves will be reconfigured)

The existing driveway onto Simpson Avenue is proposed to be closed. The parking area on the south side of Simpson Avenue also serves the existing Courthouse campus is planned to remain.

This memo provides information relative to existing traffic demands around the campus, parking demands of the same, and gives a sense of functionality with respect to the future access scheme. It is recognized that additional assessment may be needed pending staff review and findings of this initial assessment.

EXISTING CONDITIONS

Adjacent Street Traffic

AM and PM peak hour turning movement data were collected at the site's access point intersections. Since the parking area east of Town Hall would become integrated into the redevelopment, its access intersections were also counted, as was the intersection of Willow Street/Pearl Avenue since it is in close proximity to the eastern Town Hall parking access point (which would also end up serving Courthouse functions as well). Currently, the western access is signed for inbound traffic only and the eastern access is signed for outbound-only traffic. The parking spaces within the lot is angled along both sides to emphasize a one-way counter-clockwise circulation.

Figure 3 shows turning movement count data that were collected on December 19th, 2025. The busiest intersection in the mix was Pearl Avenue/Willow Street which served a total of 536 vehicles per hour (vph) during the AM peak hour and 618 vph during the PM peak hour. Bicycles were not observed during either peak hour at this intersection, but pedestrian activity was noted; 20 pedestrian-crossings were observed during the AM peak hour and 43 during the PM peak hour. Most of the pedestrian traffic was traveling north and south crossing Pearl Avenue.

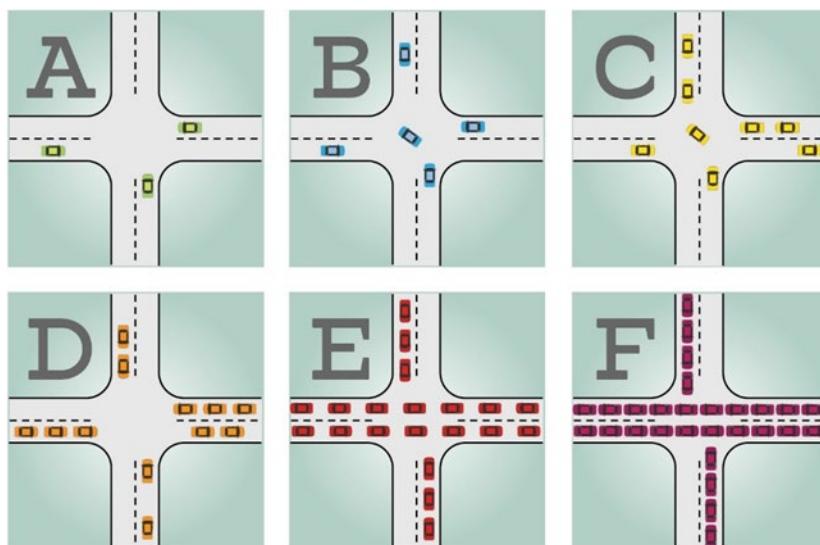
Intersection Levels of Service were calculated for this intersection given the counts shown in **Figure 3**. Detailed calculations were conducted using Synchro software to assess operations given current traffic demands (reflective of December 19th, 2024). This software employs techniques documented in the Highway Capacity Manual (Transportation Research Board, 7th Edition). The worksheets are attached.

LOS is a qualitative measure of traffic operational conditions, based on roadway capacity and vehicle delay, described by a letter designation ranging from A to F. A LOS A represents nearly free-flow travel indicative of very little delay, while LOS F represents congested conditions and excessive delay. The LOS is defined by the amount of delay drivers endure, on average, during a peak hour, and the Highway Capacity Manual procedures are geared toward calculating the average delay for each movement or lane group (as compared to free-flow conditions if the intersection did not exist).

Table 1 shows the LOS scale for signalized and unsignalized intersections. These scales differ since delay at a stop-signed intersection is more tense for the driver in that they are making a decision on whether to proceed based on traffic gaps, versus simply waiting for a green light. A graphical depiction of intersection LOS is provided immediately after **Table 1**.

Table 1. Level of Service (LOS)/Delay Scales

LOS	Signalized Intersections	Unsignalized Intersections
A	<10 Sec/Veh	<10 Sec/Veh
B	10-20 Sec/Veh	10-15 Sec/Veh
C	20-35 Sec/Veh	15-25 Sec/Veh
D	35-55 Sec/Veh	25-35 Sec/Veh
E	55-80 Sec/Veh	35-50 Sec/Veh
F	>80 Sec/Veh	>50 Sec/Veh



The current Teton County TIS Guidelines objective with respect to LOS is to achieve no worse than a LOS D during the peak hours of the day. This is indicative of a delay of no more than 55 seconds per vehicle through a signalized intersection and no more than 35 seconds per vehicle through a stop-sign controlled intersection. Collected data pertaining to heavy vehicle presence, peak hour factor (which measures the concentration of traffic within the peak hour), pedestrian presence, and bicycle presence have all been entered into the calculations.

The results of the LOS analyses show that the Willow/Pearl intersection approaches are currently operating at a LOS B or better during both peak hours given its current all-way stop traffic control. The HCM All-way stop methodology does not properly account for pedestrian activity, so the actual delays and LOS's might be slightly worse given the moderate level of pedestrian activity tallied in the counts.

Site Accesses

Figure 3 shows that the peak hour turning movements at the access intersections were much less than those at Pearl Avenue and Willow Street. The Town Hall lot was the busiest in which 25 vph hour entered during the AM peak hour and 22 exited during the PM peak hour. (the reflections were 6 vph out during the AM peak hour and 9 vph during the PM peak hour). The

lost immediately east of Town Hall lot is currently served by an in-only access and an out-only access onto Pearl Avenue that are internally connected via a counter-clockwise circulation scheme.

The access onto Willow Street serves a secure area, and very little traffic was recorded using this access during each peak hour. The alley access onto King Street is signed for one-way use (westbound), and only one vehicle used it during the AM peak hour and 2 movements occurred during the PM peak hour. This alley access serves the parking area at the center of the campus which is used by the Courthouse, jail staff, and Town staff. This lot is currently not connected to the lot east of Town Hall.

The Simpson Avenue access also serves the central parking area, and movements into and out of the campus at this location totaled 16 vph during the AM peak hour and 13 vph during the PM peak hour. The parking area access south of Simpson Avenue saw 5 vph during the AM peak hour and 2 vph during the PM peak hour. **Table 2** was created to illustrate the peak hour trips associated with each use within the campus including Town Hall, detention facilities, and Courthouse operations.

Table 2 Campus User Trip Generation

Campus Use	AM Peak Hour Trips ¹	PM Peak Hour Trips ¹
Town Hall (lot to its east)	31	31
Detention Facility (secured access onto Willow Street)	2	1
Courthouse (Access onto Simpson and parking lot access on south side of Simpson)	20	15
Totals	53	47

1. Includes inbound and outbound movements.

Parking

A parking survey of the campus was also conducted. The data were collected on January 14th, 2025 which was considered to be a busy day. **Figure 4** shows the parking lot count data collected between 1:30 and 2:00 PM on January 14th. The on-street parking was also collected along all four sides of the campus as it is realized that some of the patrons choose to park along the street. Willow Street was open for parking at this time and parked vehicles were recorded making use of Willow. This roadway restricts parking during other times of the year to accommodate the bike lanes along each side.

The results shown in **Figure 4** were assessed with respect to the capacity of each parking area, which is shown in **Table 3**

Table 3. Campus Parking Data (January 14th, 2025)

Parking Area	Counted Parked Vehicles	Approx. Space Capacity	Percentage Utilized
East of Town Hall	29	36	81%
Secured Detention Facility	7	14	50%
Central Area	18	33	55%
South of Simpson Ave.	17	26	65%
Along Pearl Ave. ¹	23	24	96%
Along Willow St. ^{1,2}	16	24	67%
Along Simpson Ave. ¹	20	25	80%
Along King St. ¹	9	20	45%
Totals	138	202	68%

1. Includes both sides of the roadway.

2. Parallel parking allowed in winter. Parking lanes are used as bicycle lanes during summer.

The parking on and around the site was 68 percent utilized on January 14th, 1:45 PM (plus or minus 15 minutes). The on-street parking along Pearl Avenue was the most highly utilized with essentially all of its parking being used. The businesses along the north side also generate parking along this roadway. The second highest utilization was the Town Hall lot and the parallel parking along Simpson Avenue which were each 80 percent utilized.

While on-street parking along the adjacent roads serves the uses, it is not all being generated by campus uses. For purposes of this study, 50 percent was assumed to be associated with either the Courthouse or Town Hall. Given that assumption and the parking counts in the lots, the Courthouse and Town Hall collectively generated an estimated 104 parked vehicles on January 14th.

FUTURE CONDITIONS

The proposed site plan shows that the Simpson Avenue access would be removed with the parking being reconfigured on the east side of the campus such that it is directly connected to the alley out to King Street. The primary accesses to the entire campus will be provided via Pearl Avenue. This section provides a sense of future conditions given this site plan and access scheme.

Trip Generation

A courthouse or justice center is not a land use category included in the Institute of Transportation Engineers' (ITE) Trip Generation manual. For purposes of this effort, future trip-making was estimated by applying growth factors to the existing driveway counts. The day of the traffic counts (December 19th, 2024) was subsequently found to possibly not be as busy a day as originally thought for Courthouse activity. As such, the Courthouse-specific traffic counts were increased 50 percent in an attempt to represent a typical day peak hour traffic. No such adjustments were made to the turning movements at the Pearl Avenue accesses which primarily serve Town functions.

Subsequent to applying the 50 percent growth to the Courthouse traffic, all counts (Courthouse and Town Hall driveways) were increased another 25 percent to reflect county-wide growth over the next 20 years. This was based on data prepared by the Wyoming Department of Administration & Information, Economic Analysis Division growth out to 2040 which could occur at a rate of approximately 0.6 percent per year. A 25 percent flat increase reflects an approximate 1.1 percent annual increase over 20 years and could therefore be considered as a conservatively high growth factor.

Resulting trip generation estimates for the 20-year planning horizon is shown in **Table 4**. The 20-year trip generation estimates for the entire campus are estimated to reach 81 trips during the AM peak hour and 70 trips during the PM peak hour.

It should be noted that the trip estimates of **Table 4** reflect movements into and out of the site. There are also trips taking place associated with the Courthouse and Town Hall that park along the adjacent streets. While not quantified here, this component's impact is captured in the collected turning movement data and the factors being applied to those counts in developing 20-year traffic forecasts.

Table 4. Future Trip Generation Estimates

Use/Planning Horizon	AM Peak Hour		PM Peak Hour	
	In	Out	In	Out
<i>Courthouse and Detention Facility</i>				
Existing Counts	20	2	4	12
Typical Day ¹	30	3	6	18
20-Year Projection	38	4	8	23
<i>Town Hall</i>				
Existing counts	25	6	9	22
Typical Day	25	6	9	22
20-Year Projection	31	8	11	28
<i>Totals²</i>				
Existing Counts	45	8	13	34
Typical Day	55	9	15	40
20-Year Projection	69	12	19	51

1.Existing Dec 19th counts increased 50 percent to estimate a typical day.

2.Sum of above values.

Traffic Assignment

The Courthouse parking and Town Hall parking will be in a combined lot given the proposed site plan. The accesses onto Pearl Avenue will serve the vast majority of the combined generation. The one-way alley onto King Street, the secure area adjacent to Willow Street, and the remote lot south of Simpson Avenue will each serve much less site traffic than the Pearl Avenue accesses.

From the counts and based on surrounding area, the largest component of campus traffic is projected to be oriented to/from the northwest of the site since to/from the state highway that passes through town. A distant second highest distribution orientation is projected to be southerly oriented, in part due to Snow King Road serving as a parallel east-west route to the state highway relative to serving western Jackson. Realizing this in combination with most of the parking being located in the northeast quadrant of the site, the site driveway peak hour

turning movement estimates were developed and are shown in **Figure 5**. The Pearl Avenue accesses are projected to see a western orientation of site trips entering and exiting. the two Pearl Avenue accesses are projected to serve the vast majority of the campus trips.

For this analysis, the Pearl Avenue access points have been analyzed under a scheme in which they would both provide two-way traffic, which is a shift from the current one-way counter-clockwise scheme. The redevelopment will reorient the parking on the east side of Town Hall from being angled for one-way use to being 90-degree head-in parking with two-way traffic aisles. This would then allow both Pearl Avenue driveways to serve in- and out-bound movements.

Total Long-Term Traffic

The traffic projections shown in **Figure 5** were combined with projected background traffic which was estimated by increasing existing traffic by 25 percent, again conservatively based on the previously referenced state agency forecasts. **Figure 6** shows the total 20-year traffic projections at the access intersections as well as at the Pearl Avenue/Willow Street intersection. In addition, intersection LOS's are shows for the Pearl Avenue/Willow Street intersection.

The results show that LOS's would meet the LOS D or better criteria at the Pearl Avenue/Willow Street intersection. Functionally, these intersections are all projected to operate without issue with respect to the LOS calculations. However, there are other considerations associated with the access intersections:

- Spacing along Pearl Avenue between the eastern access and the Willow Street stop bar is only 12 feet (plus or minus). Any eastbound traffic queuing along Pearl Avenue would block, fully or in part, this access. Outbound movements are being served today, but the bigger concern is the left-in movement at the eastern access which would be blocked from entering by the queue; the left-in driver would end up being “stuck” in the westbound lane until the queue clears. This eastern access should ideally be signed so as to prohibit inbound left turn movements to alleviate this situation. So, all westbound to southbound inbound left-turn movements from Pearl Avenue should ideally be served by the western access.
- The Pearl Avenue access widths are approximately 20 to 21 feet wide, which will be tight for two-way traffic where turning vehicles will occur. Ideally, these should be widened if they are to serve two-way traffic flows.

PARKING

A total of 104 parked vehicles were estimated to be parked in association with the Town Hall and the Courthouse today, as previously mentioned. Two methodologies were applied in estimating future demand. A simple approach entails applying the 25 percent growth to the estimated 104 vehicles that yields a 20-year projected total of 130 parked vehicles in association with the campus uses.

A second approach pertains to applying a series of assumptions/parameters to anticipated operations. This second approach was initially applied to existing operation conditions to assess whether the result is comparable to the 104 parked vehicles previously determined

The following information was obtained concerning current Courthouse activity:

- 26 full time staff associated with clerk's office, attorney's office, and courts. Assume 15 percent utilize non-single-occupant vehicles (SOV) to commute, resulting in **23 parked vehicles of demand at peak**.
- 78 Sheriff staff, assume 33 percent are parked on site during the day, resulting in **26 parked vehicles of demand at peak**.
- Sheriff patrol vehicles, plan for **6 parked vehicles of demand at peak**.
- Visitors and deliveries, plan for **4 parked vehicles of demand at peak**.

From the above, the total parking demand associated with current County Courthouse operations totals 59 vehicles.

The following information was obtained concerning current Town activity:

- 22 full time staff, assume 15 percent utilize non-single-occupant vehicles (SOV) to commute, **resulting in 19 parked vehicles of demand at peak**.
- 5 occasional visits associated with council, assume 20 percent are on site, resulting in **1 parked vehicle of demand at peak**
- Police patrol vehicles, information from Town staff yields up to **4 parked vehicles of demand at peak**
- 38 police staff, assume 33 percent are on-site, resulting in **13 parked vehicles of demand at peak**.
- Visitors and deliveries, plan for **4 parked vehicles of demand at peak**.

From the above, the total parking demand associated with current Town operations totals 41 parked vehicles.

Courthouse operations yielded an estimate of 59 parked vehicles and Town operations yielded an additional 41, resulting in a total of 100 parked vehicles using the second estimation approach. This compares well with the 104 vehicles cited earlier based on parking lot counts and assuming one-half of the on-street parking along these uses' frontage. This suggests that

the same assumptions / parameters would be a reasonable approach in predicting future demand for the campus as an alternative to simply applying a growth factor.

With respect to the future conditions at the Courthouse, increases are anticipated which include 42 staff associated with the clerk's office/attorney's office/courts (26 exist today) and 84 Sheriff staff (78 exist today). Deliveries/visitor parking needs to continue to reserve 4 spaces, and 6 reserved spots for Sheriff patrol vehicles also needs to continue. In addition, the General Services Building that is nearing completion, will house approximately 20 staff which will generate parking needs above and beyond the Courthouse functions. As such, the future Courthouse and General Services Building parking demand is comprised of:

- 42 full time staff associated with clerk's office, attorney's office, and courts. Assume 15 percent utilize non-single-occupant vehicles (SOV) to commute, results in **36 parked vehicles of demand at peak**.
- 84 Sheriff staff, assume 33 percent are parked on site during the day, results in **28 parked vehicles of demand at peak**.
- Sheriff patrol vehicles, plan for **6 parked vehicles of demand at peak**.
- Visitors and deliveries, plan for **4 parked vehicles of demand at peak**.
- 20 staff in the General Services Building. Assume 15 percent utilize non-SOV to commute, resulting in **17 parked vehicles of demand at peak**.

From the above assumptions regarding each component, the total future parking demand associated with County Courthouse operations totals 91 vehicles.

The number of spaces that are being proposed on site (including the parking lot south of Simpson Avenue and excluding 9 spaces that are currently leased by the Town) is 112 spaces. As such, the Courthouse technically meets its parking requirements based on this analysis (112 spaces of off-street supply versus 91 spaces of demand).

Many of the parking spaces will not necessarily be reserved / signed exclusively for Courthouse use, and the reality is that the Town functions will continue to use the parking located on the site as occurs today. When incorporating the Town's functions, a total of 132 parked vehicles of demand is estimated for the entire campus (91 plus 41) which exceeds the on-site supply by 18 spaces. But when also considering 50 percent of the on-street parking capacity along the campus block (excluding Willow Street since it will likely be configured to accommodate bike lanes during the summer), and additional 34 spaces would be available. With this, the supply increases to 146 spaces which would exceed the demand of 132 vehicles when combining the Courthouse and Town function uses.

Jury pools, when needed, will also generate parking demand. However, summoning jury pools is not an activity that occurs on a regular continuous basis, so jury pool demands were not

included in this analysis. On large jury pool days, on-street parking will play a key role adjacent to the campus as well as within a block, possibly two. The parking structure on Milward Street can also be utilized to meet some of this demand on an extreme day. Ideally, juror summons notices will contain information regarding alternative modes of travel to encourage non-auto use as well as including information concerning parking options including the parking structure.

TRANSPORTATION DEMAND MANAGEMENT (TDM)

The traffic projections shown in this study do not incorporate any additional TDM measures that are not already implemented today, but the County and Town could explore implementing additional measures to encourage users to travel by other than automobile. Possible considerations include:

- Bike lockers
- Bike Share programs
- Showers/locker rooms
- Information on alternatives including bus schedules
- Paid parking
- Employee Transit passes (if not already provided)
- Telecommuting allowance

Additional TDM measures can be explored for this campus as a means of limiting its trips

SUMMARY AND RECOMMENDATIONS

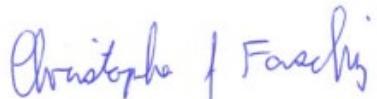
The following provides a summary of the findings and recommendations as a result of this traffic assessment.

- The eastern Pearl Avenue Access should be signed so as to prohibit left-in movements.
- Both Pearl Avenue access drives should be widened at Pearl Avenue to better accommodate two-way traffic flows.
- Additional TDM measures should be identified and researched to help limit vehicular trips associated with the campus.
- On-street parking along the campus will continue to serve the combined Town and Courthouse uses. With respect to the Courthouse function (and the General Service Building), adequate parking will be provided on-site to meet projected demands.

Adjacent on-street parking is also needed to meet combined Courthouse and Town demands. Parking beyond the campus area, including the parking structure at Milward Street, will be needed to serve when large jury pools are summoned.

If you have any questions, please feel free to call.

Sincerely,
Fasching Consulting, LLC



Christopher J. Fasching, PE
President/Owner



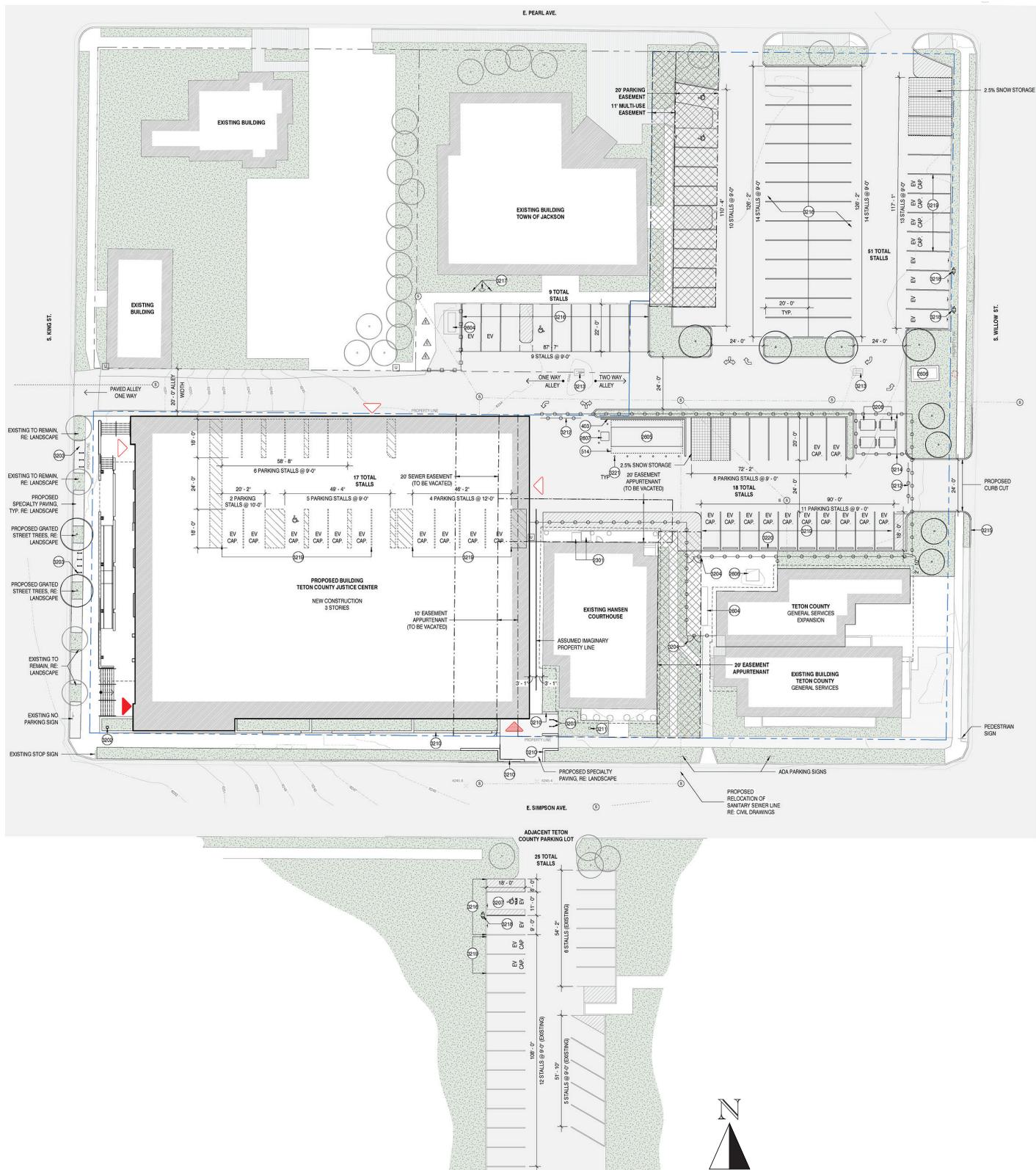
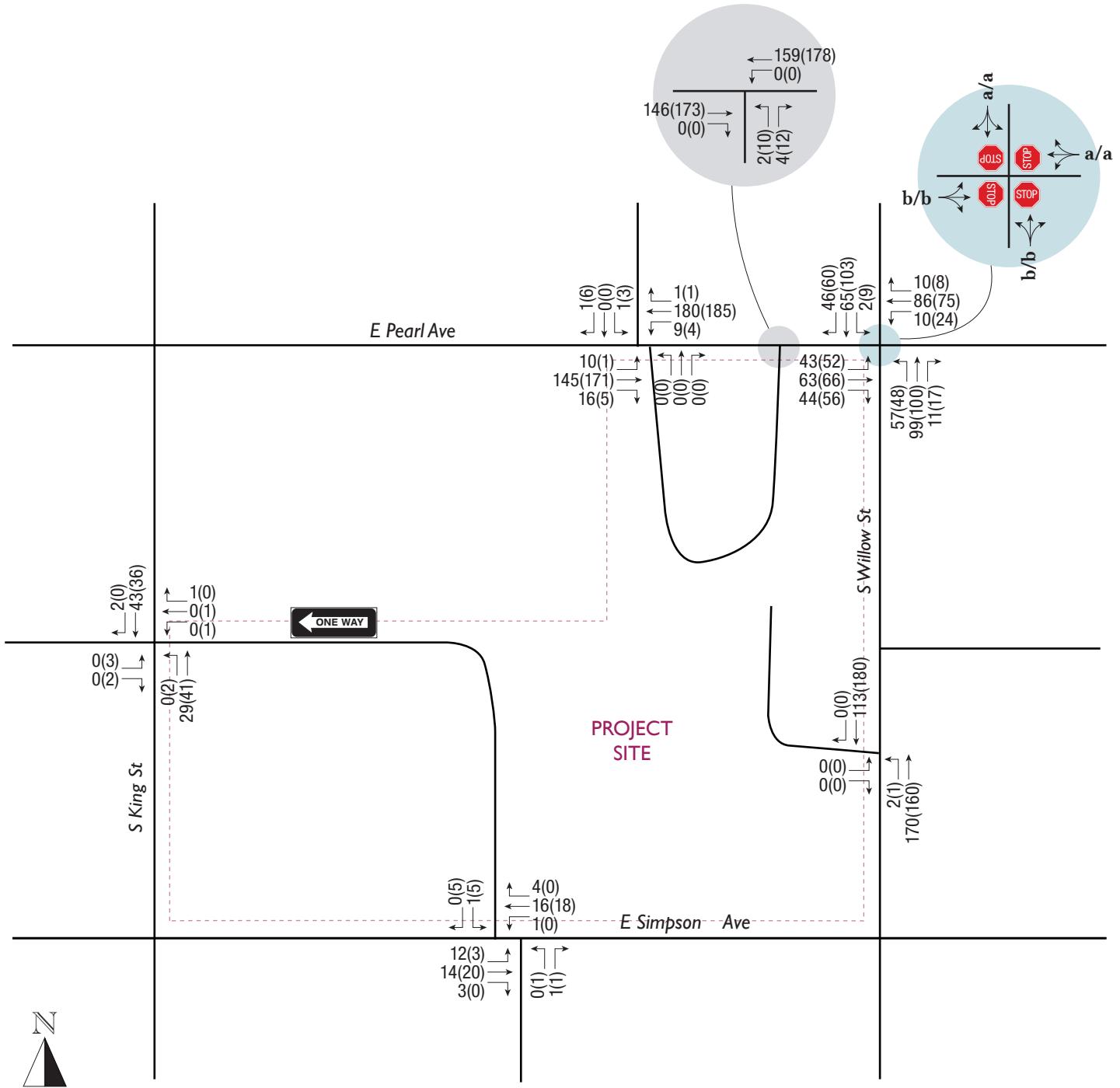
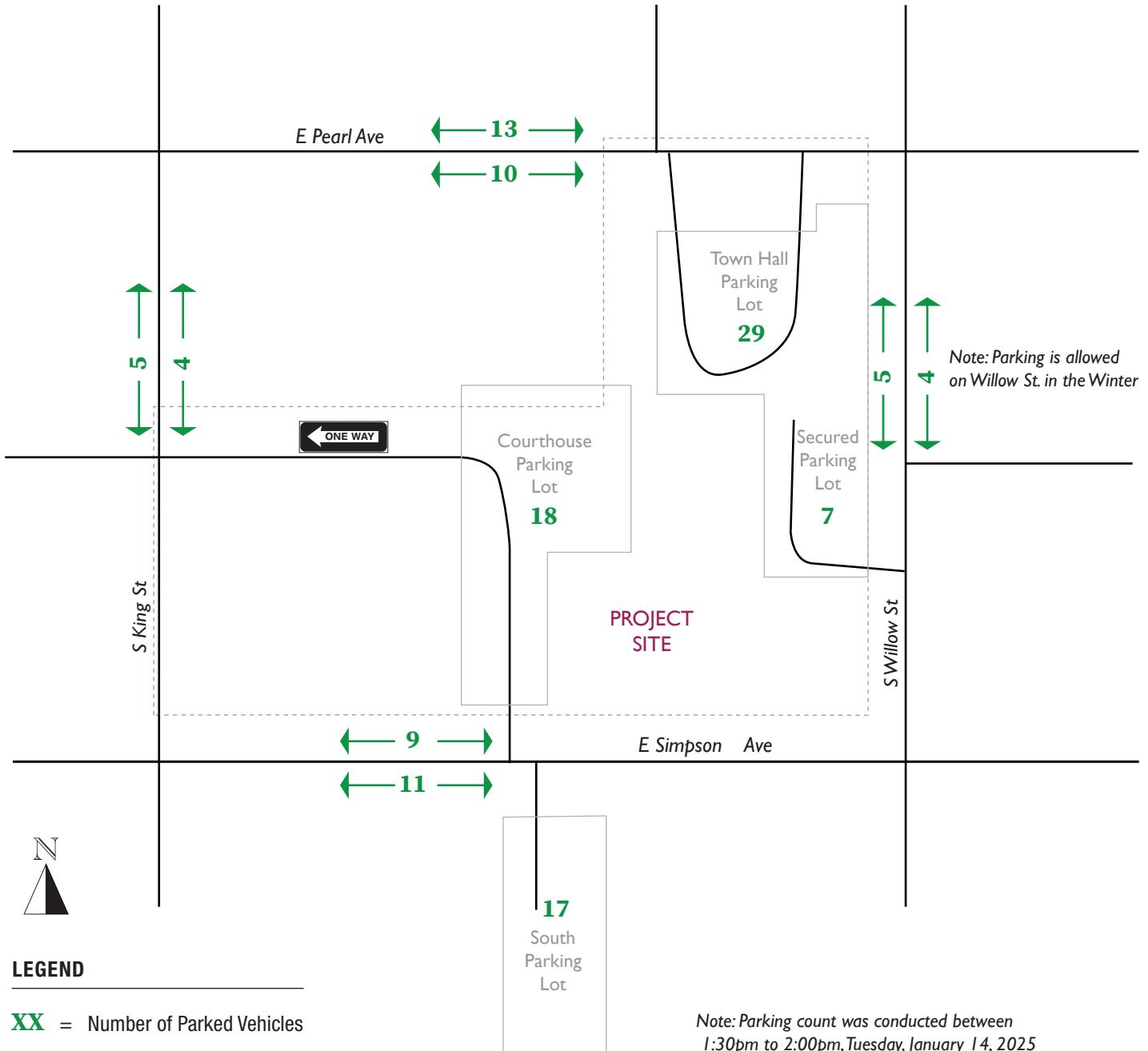


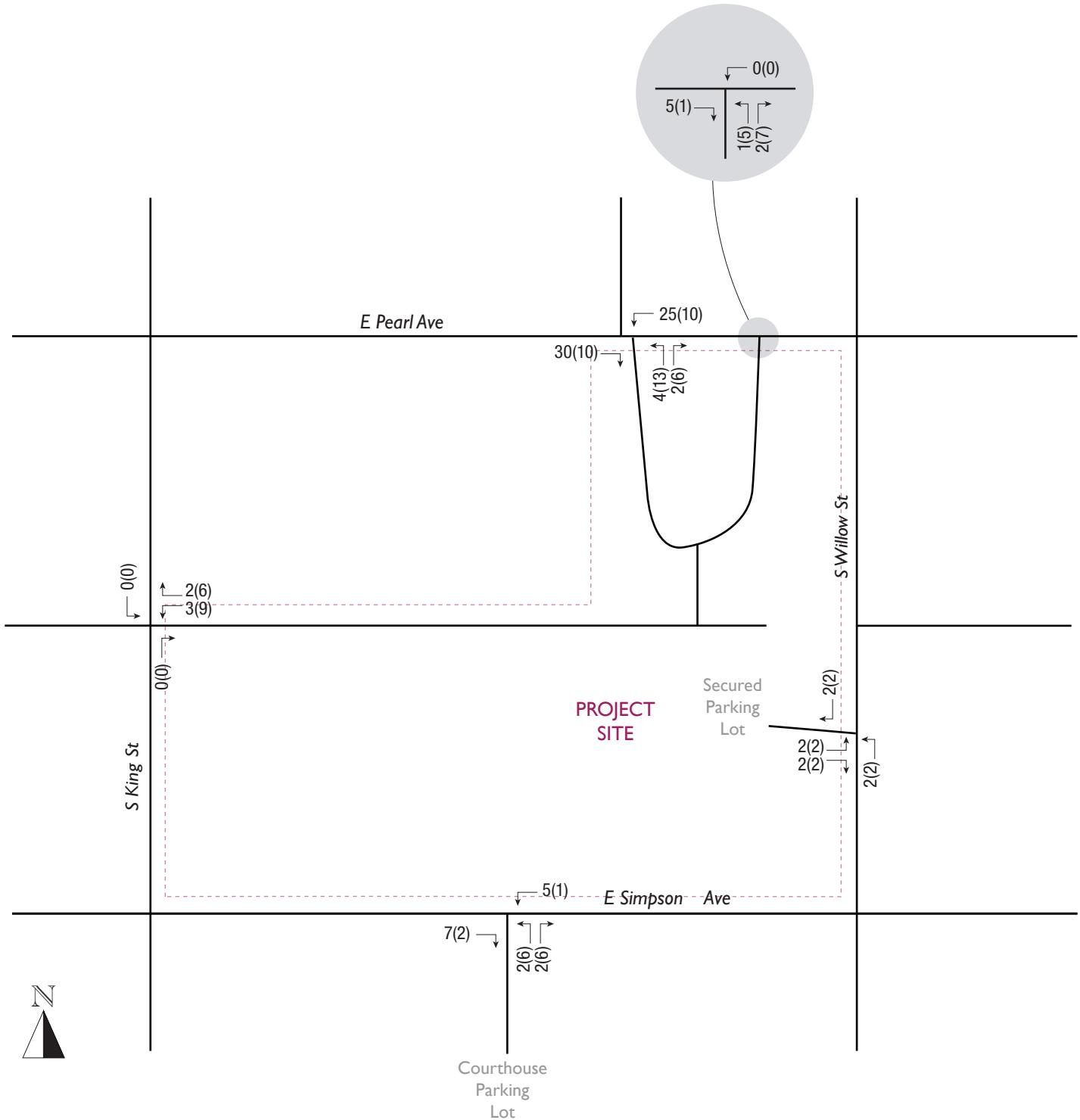
Figure 2

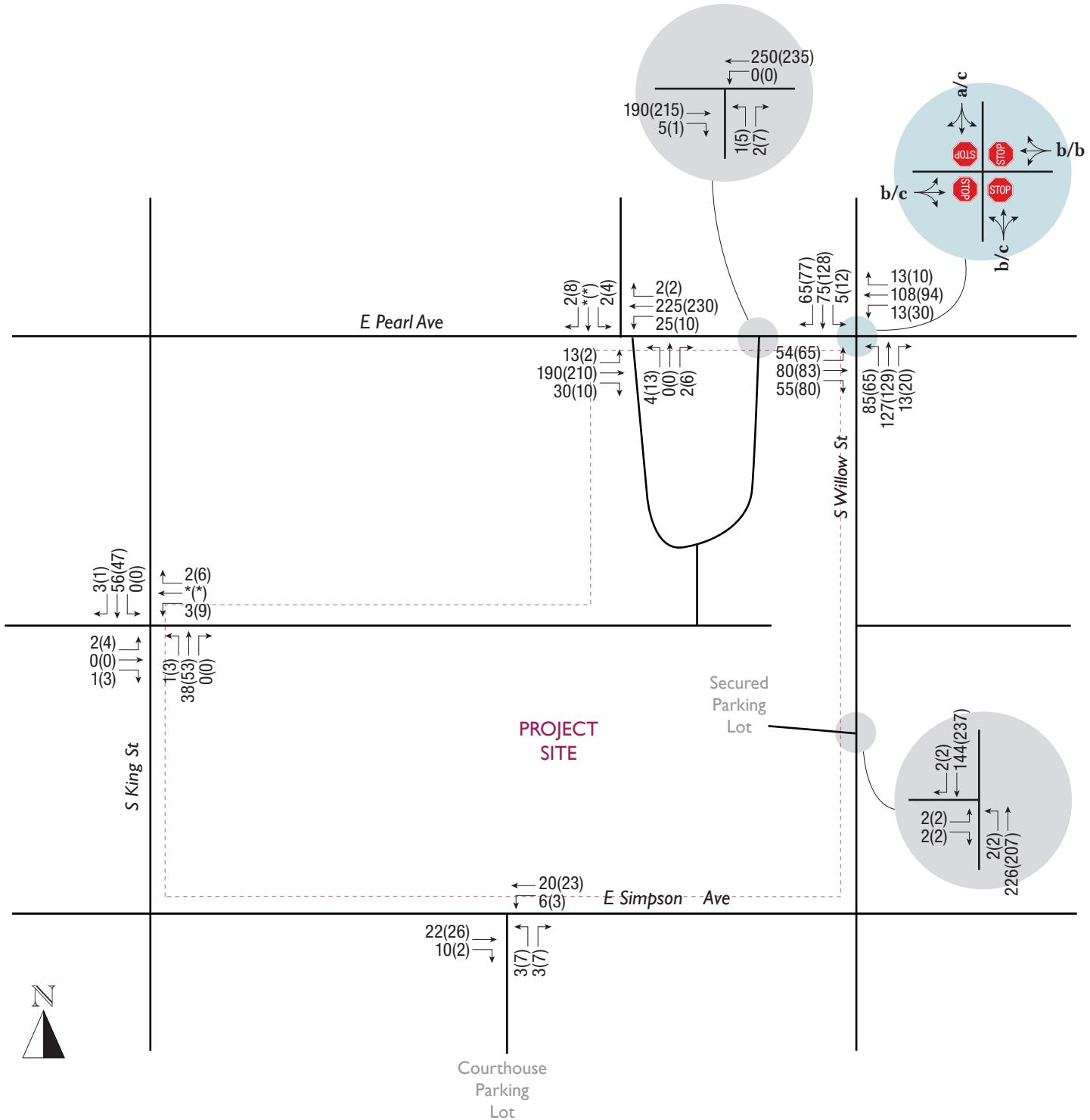
Site Plan

Jackson WY 03/04/25









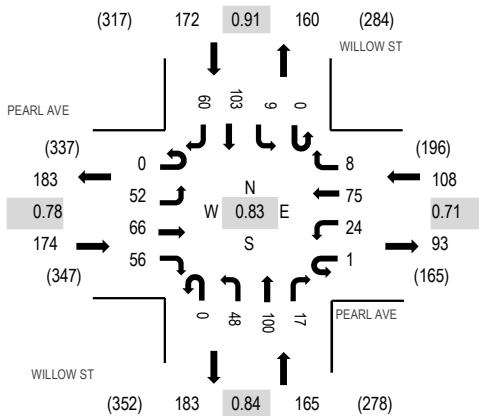
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Date: Thursday, December 19, 2024

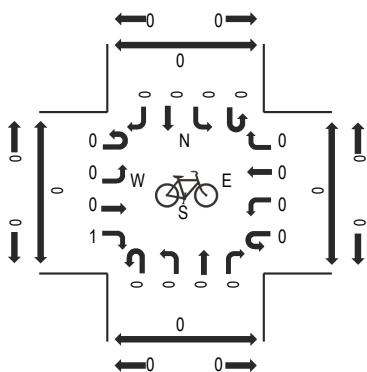
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Peak 15-Minutes: 05:00 PM - 05:15 PM

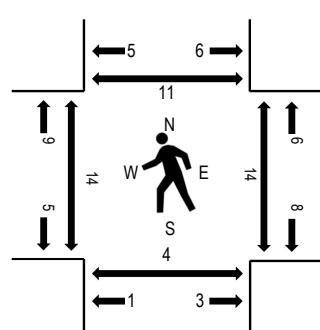
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				WILLOW ST Northbound				WILLOW ST Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	Total	West	East	South	North	
4:00 PM	0	11	16	17	0	1	20	2	0	12	15	2	0	1	25	16	138	568	1	2	0	3
4:15 PM	0	9	14	24	0	2	18	3	0	6	21	2	0	1	19	16	135	617	4	7	0	2
4:30 PM	0	12	13	16	0	7	19	2	0	8	21	8	0	3	25	12	146	619	2	0	0	0
4:45 PM	0	13	14	9	0	5	12	3	0	8	37	4	0	3	25	16	149	614	6	3	1	2
5:00 PM	0	15	24	20	0	7	29	3	0	19	19	4	0	2	31	14	187	570	4	8	3	4
5:15 PM	0	12	15	11	1	5	15	0	0	13	23	1	0	1	22	18	137	2	3	0	5	
5:30 PM	0	15	17	18	0	1	18	3	0	8	24	1	0	1	23	12	141	3	0	1	2	
5:45 PM	0	4	14	14	0	2	16	2	0	5	15	2	0	1	23	7	105	3	3	0	0	
Count Total	0	91	127	129	1	30	147	18	0	79	175	24	0	13	193	111	1,138	25	26	5	18	
Peak Hour	0	52	66	56	1	24	75	8	0	48	100	17	0	9	103	60	619	14	14	4	11	

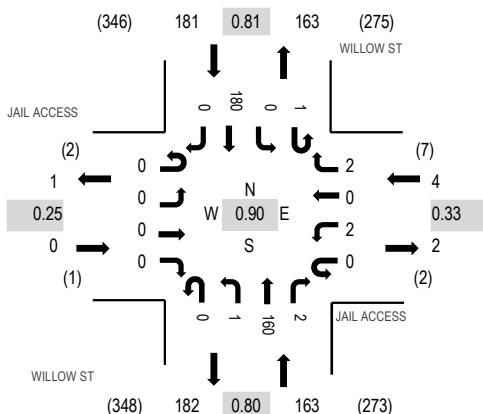
Location: 2 WILLOW ST & JAIL ACCESS PM

Date: Thursday, December 19, 2024

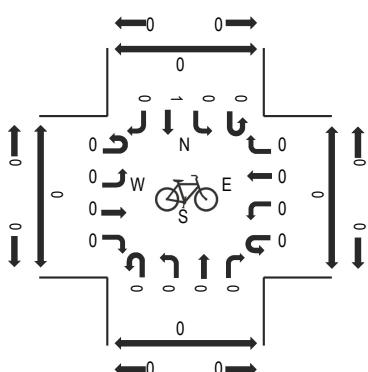
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Peak 15-Minutes: 05:00 PM - 05:15 PM

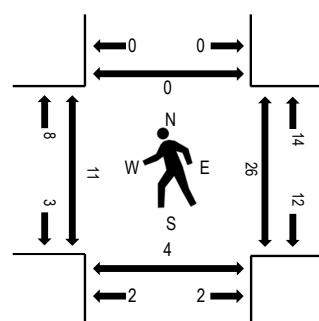
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	JAIL ACCESS				JAIL ACCESS				WILLOW ST				WILLOW ST				Rolling Hour	Pedestrian Crossings				
	Eastbound		Westbound		Northbound		Southbound		Total		West	East	South		North	West		East	South	North		
4:00 PM	0	0	0	1	0	0	0	3	0	0	26	0	0	0	44	0	74	319	5	2	3	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	28	0	0	0	43	0	71	342	3	8	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	37	0	1	0	45	0	83	348	2	0	4	0
4:45 PM	0	0	0	0	0	1	0	0	0	0	49	2	0	0	39	0	91	339	5	11	0	0
5:00 PM	0	0	0	0	0	0	1	0	0	0	39	0	0	0	57	0	97	308	2	8	0	0
5:15 PM	0	0	0	0	0	1	0	1	0	1	35	0	0	0	39	0	77	2	7	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	33	0	0	0	41	0	74	2	1	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	1	22	0	0	0	37	0	60	1	0	0	0	0
Count Total	0	0	0	1	0	2	0	5	0	2	269	2	1	0	345	0	627	22	37	7	0	0
Peak Hour	0	0	0	0	0	2	0	2	0	1	160	2	1	0	180	0	348	11	26	4	0	0

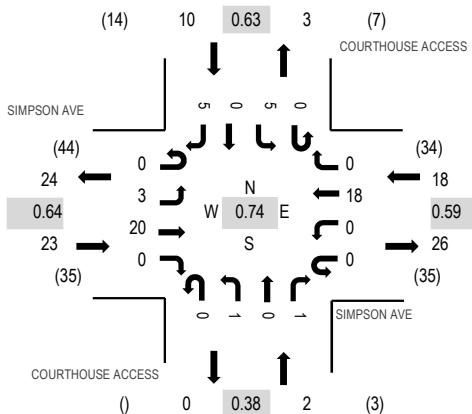
Location: 3 COURTHOUSE ACCESS & SIMPSON AVE PM

Date: Thursday, December 19, 2024

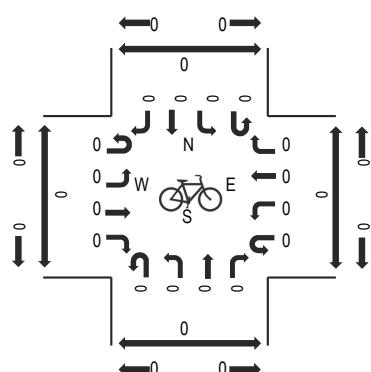
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Peak 15-Minutes: 04:30 PM - 04:45 PM

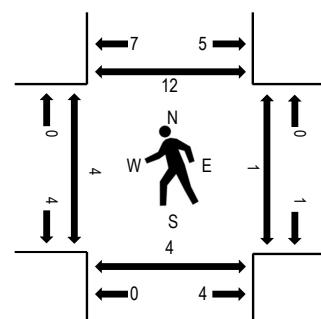
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	SIMPSON AVE Eastbound				SIMPSON AVE Westbound				COURTHOUSE ACCESS Northbound				COURTHOUSE ACCESS Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		West	East	South	North	
4:00 PM	0	0	9	0	0	0	2	0	0	0	0	0	0	1	0	1	13	53	0	1	1	4
4:15 PM	0	1	3	0	0	0	5	0	0	0	0	0	0	1	0	1	11	49	1	0	1	4
4:30 PM	0	0	6	0	0	0	8	0	0	0	0	0	0	3	0	1	18	48	3	0	2	3
4:45 PM	0	2	2	0	0	0	3	0	0	1	0	1	0	0	0	2	11	40	0	0	0	1
5:00 PM	0	2	3	0	0	0	2	0	0	1	0	0	0	0	0	1	9	33	1	1	1	2
5:15 PM	0	0	2	0	0	0	6	0	0	0	0	0	0	1	0	1	10	0	1	1	2	0
5:30 PM	0	1	2	0	0	0	6	0	0	0	0	0	0	0	1	1	10	0	0	0	0	0
5:45 PM	0	1	1	0	0	0	2	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0
Count Total	0	7	28	0	0	0	34	0	0	2	0	1	0	6	0	8	86	5	3	6	16	
Peak Hour	0	3	20	0	0	0	18	0	0	1	0	1	0	5	0	5	53	4	1	4	12	

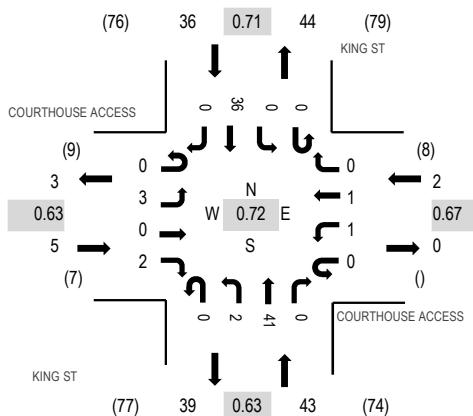
Location: 4 KING ST & COURTHOUSE ACCESS PM

Date: Thursday, December 19, 2024

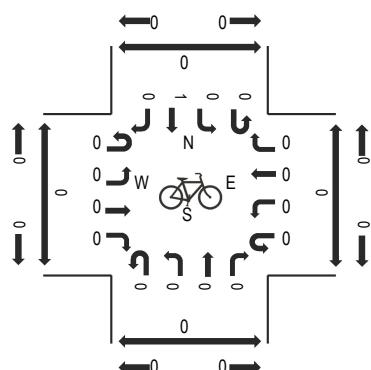
Peak Hour: 04:00 PM - 05:00 PM

Peak 15-Minutes: 04:00 PM - 04:15 PM

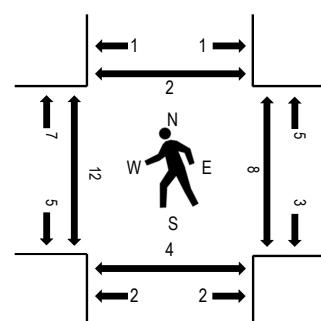
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	COURTHOUSE ACCESS				COURTHOUSE ACCESS				KING ST				KING ST				Rolling Hour	Pedestrian Crossings					
	Eastbound		Westbound		Northbound		Southbound		U-Turn		Left		Thru		Right			Total	West	East	South	North	
4:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	17	0	0	0	30	86	4	4	3	2	
4:15 PM	0	2	0	0	0	0	0	0	0	1	7	0	0	0	11	0	21	81	0	2	0	0	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	6	0	13	75	4	0	0	0
4:45 PM	0	1	0	0	0	1	1	0	0	1	10	0	0	0	8	0	22	78	4	2	1	0	
5:00 PM	0	1	0	0	0	0	1	1	0	1	8	0	0	0	13	0	25	79	4	3	0	1	
5:15 PM	0	0	0	0	0	0	0	1	0	0	10	0	0	0	4	0	15	2	5	0	0	0	
5:30 PM	0	0	0	0	0	0	0	3	0	0	4	0	0	0	9	0	16	0	3	0	0	0	
5:45 PM	0	0	0	1	0	0	0	0	0	1	7	0	0	0	11	3	23	2	1	1	0	0	
Count Total	0	4	0	3	0	1	2	5	0	4	70	0	0	0	73	3	165	20	20	5	3	0	
Peak Hour	0	3	0	2	0	1	1	0	0	2	41	0	0	0	36	0	86	12	8	4	2	0	

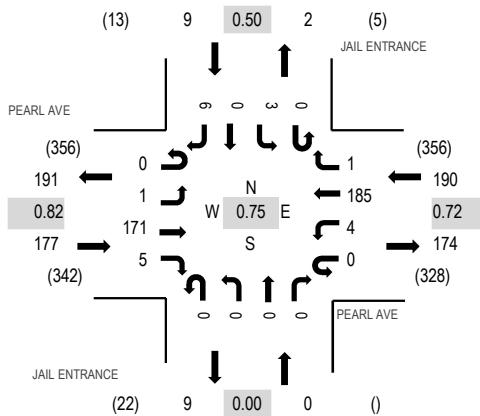
Location: 5 JAIL ENTRANCE & PEARL AVE PM

Date: Thursday, December 19, 2024

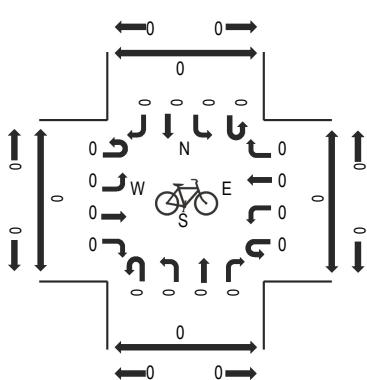
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Peak 15-Minutes: 05:00 PM - 05:15 PM

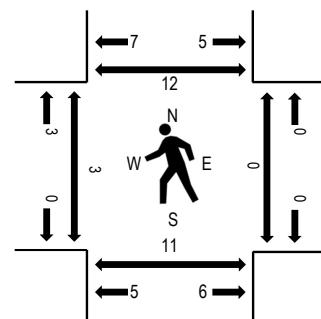
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

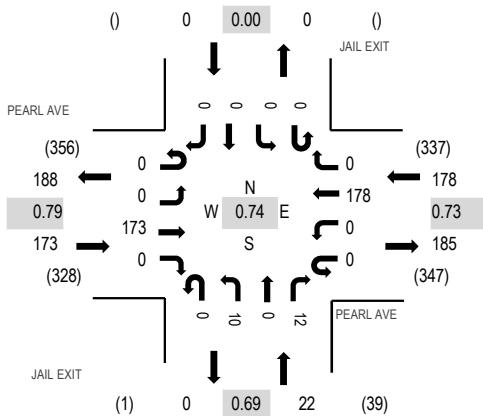


Note: Total study counts contained in parentheses.

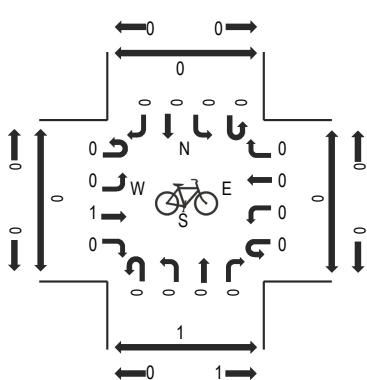
Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				JAIL ENTRANCE Northbound				JAIL ENTRANCE Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		West	East	South	North	
4:00 PM	0	1	44	4	0	1	49	0	0	0	0	0	0	0	0	0	99	345	2	0	3	3
4:15 PM	0	1	42	1	0	0	43	0	0	0	0	0	0	0	0	0	87	371	2	0	1	0
4:30 PM	0	0	38	2	0	0	41	1	0	0	0	0	0	0	1	0	86	369	1	0	1	1
4:45 PM	0	0	35	0	0	1	36	0	0	0	0	0	0	0	0	1	73	376	3	0	2	5
5:00 PM	0	1	52	1	0	3	63	0	0	0	0	0	0	3	0	2	125	366	0	0	4	2
5:15 PM	0	0	36	3	0	0	45	1	0	0	0	0	0	0	0	0	85	0	0	0	0	4
5:30 PM	0	0	48	1	0	0	41	0	0	0	0	0	0	0	0	3	93	0	0	0	5	1
5:45 PM	0	0	29	3	0	2	29	0	0	0	0	0	0	0	0	0	63	0	0	0	3	0
Count Total	0	3	324	15	0	7	347	2	0	0	0	0	0	4	0	9	711	8	0	19	16	
Peak Hour	0	1	171	5	0	4	185	1	0	0	0	0	0	3	0	6	376	3	0	11	12	

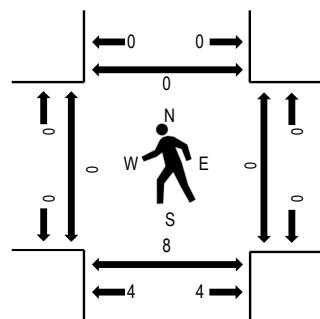
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				JAIL EXIT Northbound				JAIL EXIT Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		West	East	South	North	
4:00 PM	0	0	42	0	0	0	48	0	0	0	2	0	3	0	0	0	95	342	0	0	3	0
4:15 PM	0	0	44	0	0	0	40	0	0	0	3	0	4	0	0	0	91	373	0	0	1	0
4:30 PM	0	0	39	0	0	0	39	0	0	0	3	0	2	0	0	0	83	365	0	0	1	0
4:45 PM	0	0	35	0	0	0	36	0	0	0	1	0	1	0	0	0	73	373	0	0	2	0
5:00 PM	0	0	55	0	0	0	63	0	0	3	0	5	0	0	0	0	126	362	0	0	4	0
5:15 PM	0	0	36	0	0	0	45	0	0	1	0	1	0	0	0	0	83		0	0	0	0
5:30 PM	0	0	47	1	0	0	38	0	0	3	0	2	0	0	0	0	91		0	0	1	0
5:45 PM	0	0	29	0	0	0	28	0	0	3	0	2	0	0	0	0	62		0	0	0	0
Count Total	0	0	327	1	0	0	337	0	0	19	0	20	0	0	0	0	704		0	0	12	0
Peak Hour	0	0	173	0	0	0	178	0	0	10	0	12	0	0	0	0	373		0	0	8	0

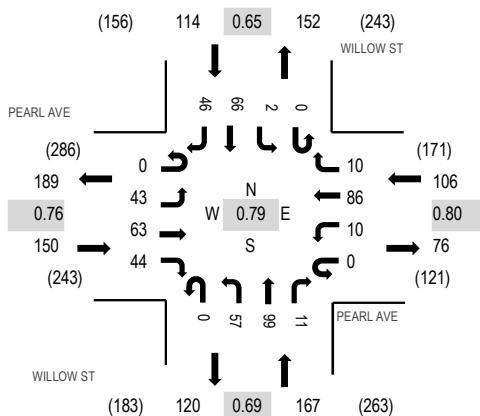
Location: 1 WILLOW ST & PEARL AVE AM

Date: Thursday, December 19, 2024

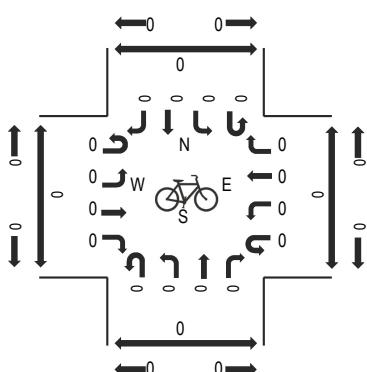
Peak Hour: 08:00 AM - 09:00 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

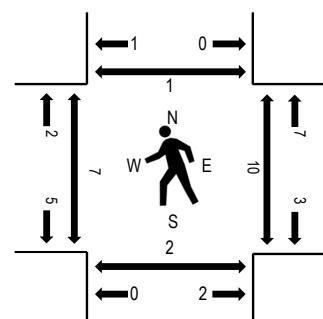
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				WILLOW ST Northbound				WILLOW ST Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	Total	West	East	South	North	
7:00 AM	0	1	8	7	0	2	8	2	0	3	15	0	0	1	2	1	50	296	0	0	0	0
7:15 AM	0	3	4	4	0	5	8	0	0	6	11	1	0	2	5	2	51	415	3	1	1	2
7:30 AM	0	9	17	5	0	2	17	1	0	8	12	0	0	1	10	5	87	502	1	3	1	0
7:45 AM	0	13	8	14	0	2	16	2	0	15	22	3	0	0	5	8	108	530	1	10	3	3
8:00 AM	0	20	13	18	0	2	25	6	0	14	45	3	0	1	14	8	169	537	0	6	0	0
8:15 AM	0	7	16	8	0	2	24	3	0	12	21	1	0	1	27	16	138		1	2	0	0
8:30 AM	0	10	17	11	0	2	14	1	0	15	16	4	0	0	13	12	115		2	0	1	0
8:45 AM	0	6	17	7	0	4	23	0	0	16	17	3	0	0	12	10	115		4	2	1	1
Count Total	0	69	100	74	0	21	135	15	0	89	159	15	0	6	88	62	833		12	24	7	6
Peak Hour	0	43	63	44	0	10	86	10	0	57	99	11	0	2	66	46	537		7	10	2	1

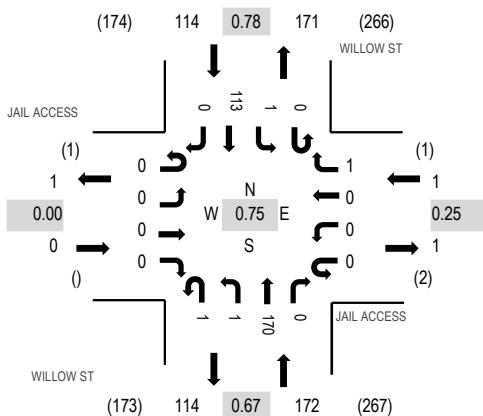
Location: 2 WILLOW ST & JAIL ACCESS AM

Date: Thursday, December 19, 2024

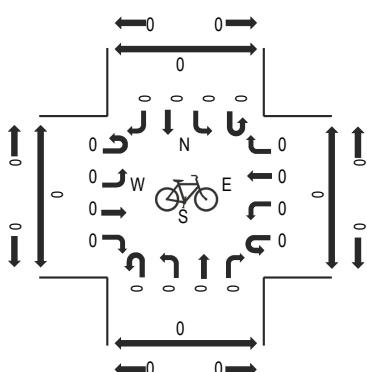
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

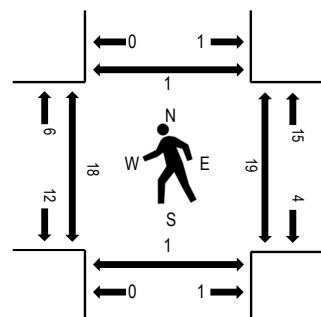
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	JAIL ACCESS				JAIL ACCESS				WILLOW ST				WILLOW ST				Rolling Hour	Pedestrian Crossings					
	Eastbound		Westbound		Northbound		Southbound		Total		West	East	South		North	West		East	South	North			
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	18	0	0	10	0	28	156	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	17	0	3	0	10	0	30	224	4	0	0
7:30 AM	0	0	0	0	0	0	0	0	2	0	0	0	22	0	0	0	15	0	39	263	2	1	0
7:45 AM	0	0	0	0	0	0	0	0	1	1	37	0	0	0	0	20	0	59	287	7	11	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	64	0	0	0	0	32	0	96	286	5	6	0	
8:15 AM	0	0	0	0	0	0	0	0	0	0	32	0	0	0	0	37	0	69	4	2	1	0	
8:30 AM	0	0	0	0	0	0	0	1	0	0	37	0	0	0	1	24	0	63	2	0	0	0	
8:45 AM	0	0	0	0	0	0	0	0	0	0	35	1	0	0	0	22	0	58	1	9	0	0	
Count Total	0	0	0	0	0	0	0	1	3	1	262	1	3	1	170	0	442	25	29	1	3		
Peak Hour	0	0	0	0	0	0	0	1	1	1	170	0	0	1	113	0	287	18	19	1	1		

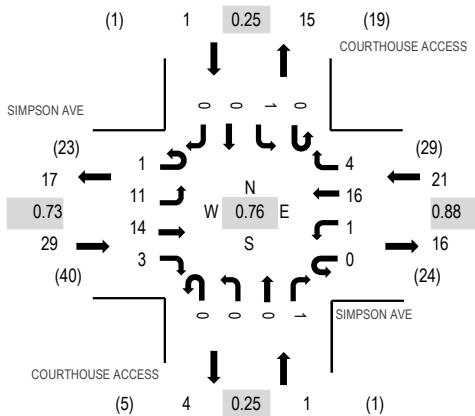
Location: 3 COURTHOUSE ACCESS & SIMPSON AVE AM

Date: Thursday, December 19, 2024

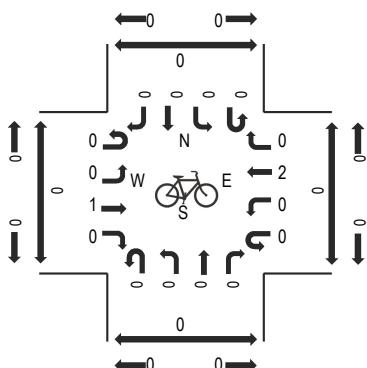
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

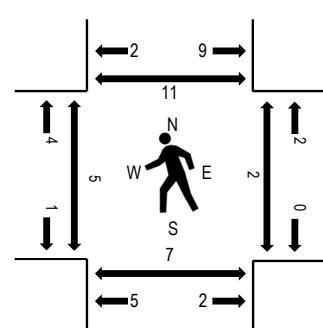
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	SIMPSON AVE				SIMPSON AVE				COURTHOUSE ACCESS				COURTHOUSE ACCESS				Rolling Hour	Pedestrian Crossings				
	Eastbound				Westbound				Northbound				Southbound					West	East	South	North	
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right						
7:00 AM	0	1	1	0	0	0	4	0	0	0	0	0	0	0	0	0	6	35	0	0	0	0
7:15 AM	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	5	43	1	0	1	0
7:30 AM	0	1	2	0	0	1	3	0	0	0	0	0	0	0	0	0	7	52	0	0	1	1
7:45 AM	0	3	6	1	0	0	6	0	0	0	0	0	0	1	0	0	17	51	3	1	1	8
8:00 AM	0	3	3	2	0	0	4	1	0	0	0	1	0	0	0	0	14	36	2	1	5	0
8:15 AM	1	4	3	0	0	0	3	3	0	0	0	0	0	0	0	0	14	0	0	0	0	2
8:30 AM	0	1	2	0	0	0	2	1	0	0	0	0	0	0	0	0	6	0	0	1	2	
8:45 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	0
Count Total	1	13	22	4	0	1	22	6	0	0	0	1	0	1	0	0	71	6	2	9	13	
Peak Hour	1	11	14	3	0	1	16	4	0	0	0	1	0	1	0	0	52	5	2	7	11	

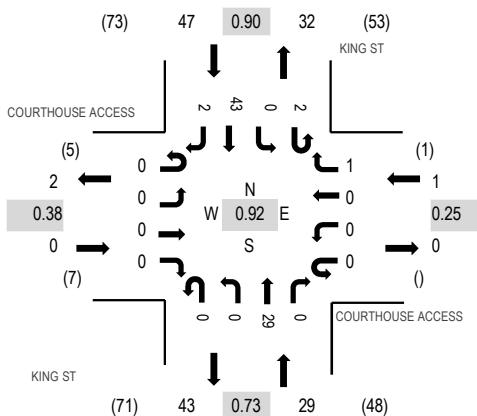
Location: 4 KING ST & COURTHOUSE ACCESS AM

Date: Thursday, December 19, 2024

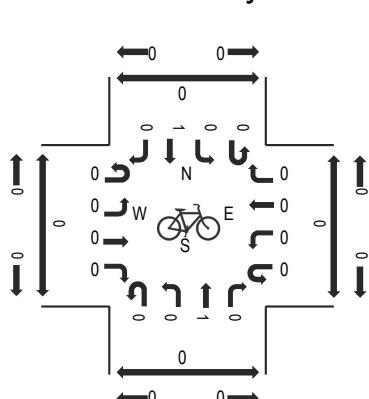
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 08:15 AM - 08:30 AM

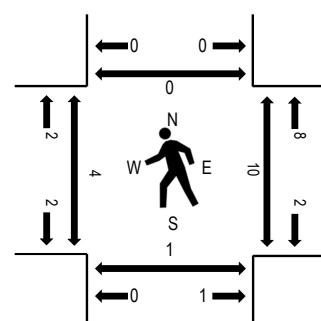
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	COURTHOUSE ACCESS				COURTHOUSE ACCESS				KING ST				KING ST				Rolling Hour	Pedestrian Crossings					
	Eastbound				Westbound				Northbound				Southbound					West	East	South	North		
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right							
7:00 AM	0	0	0	1	0	0	0	0	1	1	5	0	0	0	5	0	13	58	1	0	0	0	
7:15 AM	0	3	0	1	0	0	0	0	0	0	3	0	0	0	6	1	14	64	0	0	0	0	
7:30 AM	0	0	0	1	0	0	0	0	0	0	2	0	0	0	8	0	11	71	1	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	12	0	20	77	1	2	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	6	0	0	0	12	1	19	71	1	3	1	0	
8:15 AM	0	0	0	0	0	0	0	1	0	0	10	0	1	0	9	0	21	1	3	0	0	0	
8:30 AM	0	0	0	0	0	0	0	0	0	0	5	0	1	0	10	1	17	1	2	0	0	0	
8:45 AM	0	1	0	0	0	0	0	0	0	1	6	0	1	0	5	0	14	1	2	0	0	0	
Count Total	0	4	0	3	0	0	0	1	1	2	45	0	3	0	67	3	129	7	12	1	0	0	
Peak Hour	0	0	0	0	0	0	0	1	0	0	29	0	2	0	43	2	77	4	10	1	0	0	

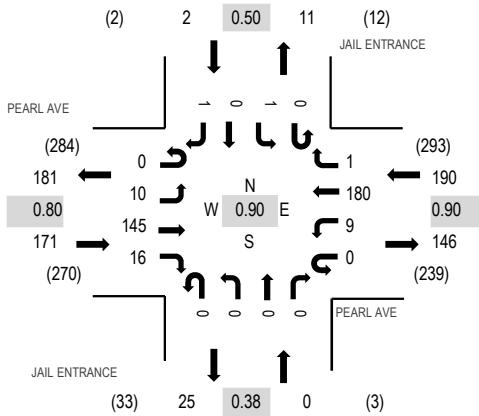
Location: 5 JAIL ENTRANCE & PEARL AVE AM

Date: Thursday, December 19, 2024

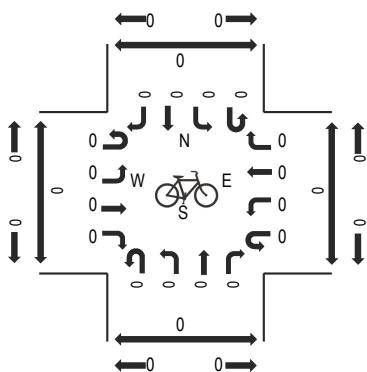
Peak Hour: 08:00 AM - 09:00 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

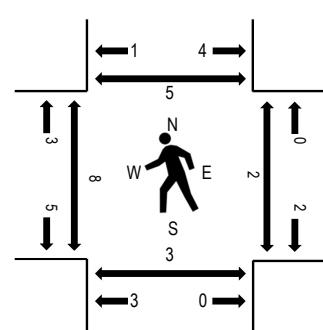
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

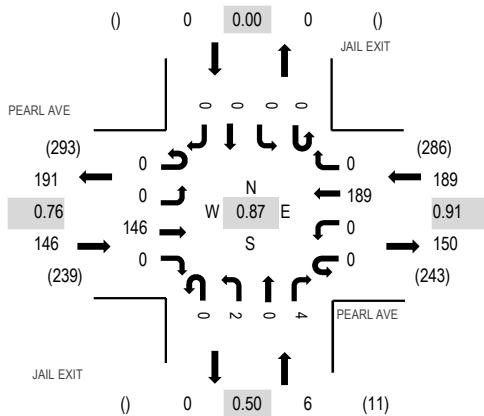


Note: Total study counts contained in parentheses.

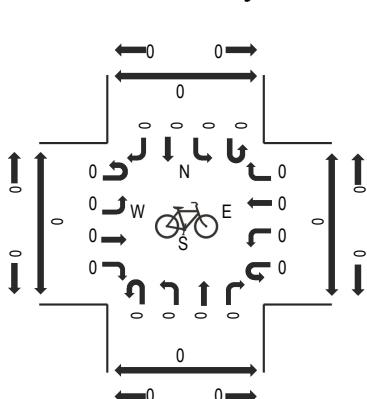
Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				JAIL ENTRANCE Northbound				JAIL ENTRANCE Southbound				Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		West	East	South	North
7:00 AM	0	0	17	0	0	0	14	0	0	1	0	1	0	0	0	0	33	205	0	0	0
7:15 AM	0	0	9	2	0	0	16	0	0	0	0	0	0	0	0	0	27	273	0	0	0
7:30 AM	0	0	31	1	0	1	30	0	0	0	0	0	0	0	0	0	63	336	0	0	1
7:45 AM	0	1	35	3	0	1	41	0	0	1	0	0	0	0	0	0	82	356	0	0	2
8:00 AM	0	3	48	3	0	0	45	1	0	0	0	0	0	1	0	0	101	363	3	0	0
8:15 AM	0	1	31	5	0	2	51	0	0	0	0	0	0	0	0	0	90	0	0	0	0
8:30 AM	0	2	36	4	0	1	39	0	0	0	0	0	0	0	1	0	83	2	1	1	0
8:45 AM	0	4	30	4	0	6	45	0	0	0	0	0	0	0	0	0	89	3	1	2	4
Count Total	0	11	237	22	0	11	281	1	0	2	0	1	0	1	0	1	568	8	2	6	11
Peak Hour	0	10	145	16	0	9	180	1	0	0	0	0	0	1	0	1	363	8	2	3	5

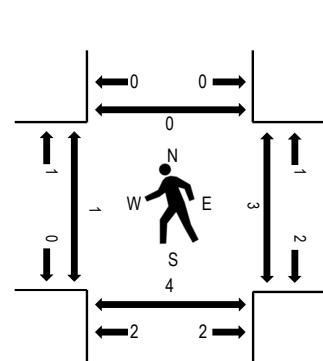
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	PEARL AVE Eastbound				PEARL AVE Westbound				JAIL EXIT Northbound				JAIL EXIT Southbound				Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	Total	West	East	South	North	
7:00 AM	0	0	17	0	0	0	12	0	0	2	0	0	0	0	0	0	31	195	0	0	0	0
7:15 AM	0	0	10	0	0	0	0	17	0	0	0	0	0	0	0	0	27	262	0	0	0	0
7:30 AM	0	0	31	0	0	0	29	0	0	0	0	0	0	0	0	0	60	318	0	0	1	0
7:45 AM	0	0	35	0	0	0	39	0	0	3	0	0	0	0	0	0	77	337	0	0	2	0
8:00 AM	0	0	50	0	0	0	47	0	0	0	0	1	0	0	0	0	98	341	0	0	0	0
8:15 AM	0	0	31	0	0	0	52	0	0	0	0	0	0	0	0	0	83	0	0	1	0	0
8:30 AM	0	0	36	0	0	0	41	0	0	0	0	2	0	0	0	0	79	0	0	1	0	0
8:45 AM	0	0	29	0	0	0	49	0	0	2	0	1	0	0	0	0	81	1	3	2	0	0
Count Total	0	0	239	0	0	0	286	0	0	7	0	4	0	0	0	0	536	1	3	7	0	0
Peak Hour	0	0	146	0	0	0	189	0	0	2	0	4	0	0	0	0	341	1	3	4	0	0

Intersection

Intersection Delay, s/veh

10

Intersection LOS

A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	43	63	44	10	86	10	57	99	11	2	65	46
Future Vol, veh/h	43	63	44	10	86	10	57	99	11	2	65	46
Peak Hour Factor	0.76	0.76	0.76	0.80	0.80	0.80	0.69	0.69	0.69	0.65	0.65	0.65
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	57	83	58	13	108	13	83	143	16	3	100	71
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Approach												
Opposing Approach	WB			WB			NB			SB		
Opposing Lanes	1			1			1			1		
Conflicting Approach Left	SB			NB			EB			WB		
Conflicting Lanes Left	1			1			1			1		
Conflicting Approach Right	NB			SB			WB			EB		
Conflicting Lanes Right	1			1			1			1		
HCM Control Delay, s/veh	10			9.6			10.7			9.5		
HCM LOS	A			A			B			A		

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	34%	29%	9%	2%
Vol Thru, %	59%	42%	81%	58%
Vol Right, %	7%	29%	9%	41%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	167	150	106	113
LT Vol	57	43	10	2
Through Vol	99	63	86	65
RT Vol	11	44	10	46
Lane Flow Rate	242	197	133	174
Geometry Grp	1	1	1	1
Degree of Util (X)	0.336	0.274	0.194	0.238
Departure Headway (Hd)	5.005	5.109	5.276	4.937
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	709	709	684	732
Service Time	3.103	3.109	3.284	2.937
HCM Lane V/C Ratio	0.341	0.278	0.194	0.238
HCM Control Delay, s/veh	10.7	10	9.6	9.5
HCM Lane LOS	B	A	A	A
HCM 95th-tile Q	1.5	1.1	0.7	0.9

Intersection

Intersection Delay, s/veh 10.1

Intersection LOS B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	52	66	56	24	75	8	48	100	17	9	103	60
Future Vol, veh/h	52	66	56	24	75	8	48	100	17	9	103	60
Peak Hour Factor	0.78	0.78	0.78	0.71	0.71	0.71	0.84	0.84	0.84	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	67	85	72	34	106	11	57	119	20	10	113	66
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Approach	EB			WB			NB			SB		
Opposing Approach	WB			EB			SB			NB		
Opposing Lanes	1			1			1			1		
Conflicting Approach Left	SB			NB			EB			WB		
Conflicting Lanes Left	1			1			1			1		
Conflicting Approach Right	NB			SB			WB			EB		
Conflicting Lanes Right	1			1			1			1		
HCM Control Delay, s/veh	10.4			9.8			10.3			9.9		
HCM LOS	B			A			B			A		

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	29%	30%	22%	5%
Vol Thru, %	61%	38%	70%	60%
Vol Right, %	10%	32%	7%	35%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	165	174	107	172
LT Vol	48	52	24	9
Through Vol	100	66	75	103
RT Vol	17	56	8	60
Lane Flow Rate	196	223	151	189
Geometry Grp	1	1	1	1
Degree of Util (X)	0.284	0.314	0.221	0.264
Departure Headway (Hd)	5.203	5.075	5.286	5.027
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	691	712	680	714
Service Time	3.23	3.075	3.316	3.055
HCM Lane V/C Ratio	0.284	0.313	0.222	0.265
HCM Control Delay, s/veh	10.3	10.4	9.8	9.9
HCM Lane LOS	B	B	A	A
HCM 95th-tile Q	1.2	1.3	0.8	1.1

Intersection

Intersection Delay, s/veh 11.2
Intersection LOS B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	54	80	55	13	108	13	85	127	13	5	75	65
Future Vol, veh/h	54	80	55	13	108	13	85	127	13	5	75	65
Peak Hour Factor	0.78	0.78	0.78	0.71	0.71	0.71	0.84	0.84	0.84	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	69	103	71	18	152	18	101	151	15	5	82	71
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Approach												
Opposing Approach	EB			WB			NB			SB		
Opposing Lanes	WB			EB			SB			NB		
Conflicting Approach Left	1			1			1			1		
Conflicting Lanes Left	SB			NB			EB			WB		
Conflicting Approach Right	1			1			1			1		
Conflicting Lanes Right	NB			SB			WB			EB		
HCM Control Delay, s/veh	11.3			10.7			12.1			10		
HCM LOS	B			B			B			A		

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	38%	29%	10%	3%
Vol Thru, %	56%	42%	81%	52%
Vol Right, %	6%	29%	10%	45%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	225	189	134	145
LT Vol	85	54	13	5
Through Vol	127	80	108	75
RT Vol	13	55	13	65
Lane Flow Rate	268	242	189	159
Geometry Grp	1	1	1	1
Degree of Util (X)	0.403	0.357	0.286	0.235
Departure Headway (Hd)	5.411	5.303	5.464	5.302
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	664	678	657	676
Service Time	3.45	3.344	3.509	3.346
HCM Lane V/C Ratio	0.404	0.357	0.288	0.235
HCM Control Delay, s/veh	12.1	11.3	10.7	10
HCM Lane LOS	B			A
HCM 95th-tile Q	1.9			0.9

Intersection

Intersection Delay, s/veh 15.2

Intersection LOS C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↖			↖			↖			↖	
Traffic Vol, veh/h	65	83	80	30	94	10	65	129	20	12	128	77
Future Vol, veh/h	65	83	80	30	94	10	65	129	20	12	128	77
Peak Hour Factor	0.76	0.76	0.76	0.80	0.80	0.80	0.69	0.69	0.69	0.65	0.65	0.65
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	86	109	105	38	118	13	94	187	29	18	197	118
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Approach	EB			WB			NB			SB		
Opposing Approach	WB				EB			SB			NB	
Opposing Lanes	1				1			1			1	
Conflicting Approach Left	SB				NB			EB			WB	
Conflicting Lanes Left	1				1			1			1	
Conflicting Approach Right	NB				SB			WB			EB	
Conflicting Lanes Right	1				1			1			1	
HCM Control Delay, s/veh	15.4				12.5			15.8			15.8	
HCM LOS	C				B			C			C	

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	30%	29%	22%	6%
Vol Thru, %	60%	36%	70%	59%
Vol Right, %	9%	35%	7%	35%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	214	228	134	217
LT Vol	65	65	30	12
Through Vol	129	83	94	128
RT Vol	20	80	10	77
Lane Flow Rate	310	300	168	334
Geometry Grp	1	1	1	1
Degree of Util (X)	0.527	0.509	0.306	0.545
Departure Headway (Hd)	6.112	6.112	6.58	5.877
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	589	588	544	614
Service Time	4.158	4.163	4.64	3.924
HCM Lane V/C Ratio	0.526	0.51	0.309	0.544
HCM Control Delay, s/veh	15.8	15.4	12.5	15.8
HCM Lane LOS	C	C	B	C
HCM 95th-tile Q	3.1	2.9	1.3	3.3

Meeting Agenda

Project Name:

Wember Inc. Project Number:

Meeting Date:

Location:

Purpose:

Teton County Justice Center

2023.18

December 10, 5:30 PM

Firehouse #1 Training Room, 60 E. Pearl Ave. Jackson WY, On-line
Neighborhood Meeting (pre-sketchn plan submittal)

1. Introduction

- a. Teton County
 - i. Introduction
 - ii. Sarah Mann – Director – General Services
 - iii. Josiah Nash – Facilities Maintenance Manager
- b. Wember / Berning Project Management
 - i. Tom Farrens – Main Contact (307) 677-2219
 - ii. Jason Berning – (307) 699-3733
- c. Anderson Mason Dale - Architect
 - i. Introduction
 - ii. Role on Project
 - iii. James Taylor – Principal
 - iv. Stephan Hall – PM
 - v. CLB Architects – Cary Lakeman – Senior PM

2. PROJECT INTRODUCTION/DESCRIPTION

- a. This meeting is required to be held prior to the submittal of the sketch plan review to the Town of Jackson. Additional meetings will be held prior to the start of construction with the CMAR to inform the neighborhood of timelines and possible disruptions.
- b. This project will require a Conditional Use Permit and will include Public Hearing that will be advertised.
- c. All addresses within 200' of the site were invited to this meeting.
- d. The project is located at 180 S King Street. Jackson, WY 83001
- e. The Teton County Justice Center is projected to be approximately 78,000 to 83,000 sf. It will involve the demolition of the existing Teton County Courthouse and the construction of a new facility that will also include three courtrooms, a Detention Center, a Sheriff's Office, a Dispatch Center, and the County Attorney offices, along with other Courthouse-related functions. Additional small remodel projects at temporary office space, as identified by the owner, may be required prior to the start of the Justice Center project.

3. COMMUNICATIONS

- a. All questions are to be addressed to the Owner's Representative – Tom Farrens – Project Manager.
tfarrens@wemberinc.com

4. SCHEDULE

a. Proposed Project Timeline:

Schematic Design	June 2024 – December 2024
CMAR Selected	December 2024 – February 2025
Design Development	January 2025 - June 2025
Construction Documents	August 2025 – January 2026
Permitting/ GMP	December 2025 – February 2026
Construction	Spring 2026 – Fall 2028 (30 Months)
	(subject to change)

5. PRESENTATION

a. Review of the project drawings.

6. QUESTIONS

7. WRAP UP AND NEXT STEPS

a. Major Questions and comments will be included in the Sketch Plan Submittal.

8. ADJURNMENT

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Teton County Justice Center Neighborhood Meeting

December 10, 5:300 PM

Name	Address	Phone	E-mail Address
Mark Simpson	705 Simpson	307 690 6611	lea@kuvinka.com

7350 E. Progress PL STE 100
Greenwood Village, CO 80111
303 279 4120

www.wemberinc.com/blog
facebook.com/Wemberinc

NOTICE OF NEIGHBORHOOD MEETING

December 10th, 5:30 pm -6:30 pm

Location of Meeting: Jackson Hole Firehouse #1 training room at 60E. Pearl Ave, Jackson, WY

A neighborhood meeting will be held to inform interested parties about:

REQUEST: Information prior to Sketch Plan submission

PROPOSAL: Teton County Justice Center construction project

For information regarding this meeting or the proposal, please contact:

Teton County Owner's Representative - Tom Farrens at (307) 677-2219

Posting Date: December 3rd, 2024