



TOWN OF JACKSON

PLANNING & BUILDING DEPARTMENT

TRANSMITTAL MEMO

Town of Jackson

- ☒ Public Works/Engineering
- ☒ Building
- ☐ Environmental Stewardship
- ☒ Town Attorney
- ☒ Police

Joint Town/County

- ☒ Parks and Recreation
- ☒ Pathways
- ☒ Housing Department

Teton County

- ☐ Planning Division
- ☐ Engineer
- ☐ Surveyor
- ☐ Assessor
- ☐ Clerk and Recorder
- ☐ Road and Levee

State of Wyoming

- ☒ Teton Conservation
- ☒ WYDOT
- ☐ TC School District #1

- ☐ Game and Fish

- ☐ DEQ

Federal Agencies

- ☐ Army Corp of Engineers

Utility Providers

- ☐ Qwest
- ☐ Lower Valley Energy
- ☐ Bresnan Communications

Special Districts

- ☒ START
- ☒ Jackson Hole Fire/EMS
- ☒ Regional Transportation

Date: November 15, 2024	REQUESTS: The applicant is submitting a request for a Sketch Plan for a new mixed-use hotel & condominium development within five buildings at the following 11 properties: 45 and 65 Mercill, 330, 350, 360, and 370 N Glennwood, and 325, 335, 345, 355, and 375 N Cache St. For questions, please call Tyler Valentine at 307-733-0440 x 1305 or email the address shown to the left. Thank you.
Item #: P24-170	
Planner: Tyler Valentine Phone: 733-0440 ext. 1305 Email: tvalentine@jacksonwy.gov	
Owners: Mogul Hospitality Partners-Jackson LLC PO Box 998 Midway, UT 84049 Applicant: Same	
Please respond by: December 4, 2024 (with Comments)	

RESPONSE: For Departments not using SmartGov, please send responses via email to planning@jacksonwy.gov



PLANNING PERMIT APPLICATION
Planning & Building Department

150 E Pearl Ave. | ph: (307) 733-0440
P.O. Box 1687 | www.townofjackson.com
Jackson, WY 83001

For Office Use Only

Fees Paid _____ Date & Time Received _____
Application #s _____

Please note: Applications received after 3 PM will be processed the next business day.

PROJECT.

Name/Description: _____
Physical Address: _____
Lot, Subdivision: _____ PIDN: _____

PROPERTY OWNER.

Name: _____ Phone: _____
Mailing Address: _____ ZIP: _____
E-mail: _____

APPLICANT/AGENT.

Name: _____ Phone: _____
Mailing Address: _____ ZIP: _____
E-mail: _____

DESIGNATED PRIMARY CONTACT.

_____ Property Owner _____ Applicant/Agent

TYPE OF APPLICATION. Please check all that apply; review the type of application at www.townofjackson/200/Planning

Use Permit

_____ Basic Use
_____ Conditional Use
_____ Special Use

Relief from the LDRs

_____ Administrative Adjustment
_____ Variance
_____ Beneficial Use Determination
_____ Appeal of an Admin. Decision

Physical Development

_____ Sketch Plan
_____ Development Plan
_____ Design Review

Subdivision/Development Option

_____ Subdivision Plat
_____ Boundary Adjustment (replat)
_____ Boundary Adjustment (no plat)
_____ Development Option Plan

Interpretations

_____ Formal Interpretation
_____ Zoning Compliance Verification

Amendments to the LDRs

_____ LDR Text Amendment
_____ Map Amendment

Miscellaneous

_____ Other: _____
_____ Environmental Analysis

PRE-SUBMITTAL STEPS. To see if pre-submittal steps apply to you, go to www.townofjackson.com/200/Planning and select the relevant application type for requirements. Please submit all required pre-submittal steps with application.

Pre-application Conference #: _____ Environmental Analysis #: _____

Original Permit #: _____ Date of Neighborhood Meeting: _____

SUBMITTAL REQUIREMENTS. Please ensure all submittal requirements are included. The Planning Department will not hold or process incomplete applications. Partial or incomplete applications will be returned to the applicant. Go to www.townofjackson.com/200/Planning and select the relevant application type for submittal requirements.

Have you attached the following?

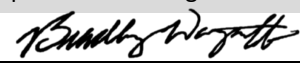
_____ **Application Fee.** Fees are cumulative. Go to www.townofjackson.com/200/Planning and select the relevant application type for the fees.

_____ **Notarized Letter of Authorization.** A notarized letter of consent from the landowner is required if the applicant is not the owner, or if an agent is applying on behalf of the landowner. Please see the Letter of Authorization template at <http://www.townofjackson.com/DocumentCenter/View/845/LetterOfAuthorization-PDF>.

_____ **Response to Submittal Requirements.** The submittal requirements can be found on the TOJ website for the specific application. If a pre-application conference is required, the submittal requirements will be provided to applicant at the conference. The submittal requirements are at www.townofjackson.com/200/Planning under the relevant application type.

Note: Information provided by the applicant or other review agencies during the planning process may identify other requirements that were not evident at the time of application submittal or a Pre-Application Conference, if held. Staff may request additional materials during review as needed to determine compliance with the LDRs.

Under penalty of perjury, I hereby certify that I have read this application and associated checklists and state that, to the best of my knowledge, all information submitted in this request is true and correct. I agree to comply with all county and state laws relating to the subject matter of this application, and hereby authorize representatives of Teton County to enter upon the above-mentioned property during normal business hours, after making a reasonable effort to contact the owner/applicant prior to entering.



Signature of Property Owner or Authorized Applicant/Agent

Date

Name Printed

Title



Table of Contents

1. Cover Letter
2. Concept Package and Site Plans
3. LDR Compliance Checklist
4. Responses to Pre-Application Conference Comments
5. Neighborhood Meeting Summary (Notes & List of Attendees)
6. Parking Summary & Breakdown
7. Housing Mitigation Plan
8. Housing Mitigation Checklist
9. Housing Mitigation Calculator
10. Construction Management Plan
11. Utility Re-Routing Proposal
12. Utility Usage Estimate



October 31, 2024

Paul Anthony
Planning Director, Jackson Planning Department
P.O. Box 1687
Jackson, WY 83001

RE: Sketch Plan Application: N Cache Lodging and Residential Development

Dear Paul,

Please accept this Sketch Plan Application for 375 – 325 North Cache Street, 45 & 65 Mercill Avenue and 330 – 370 North Glenwood Street (the “Project”). We have completed the required steps to proceed with our formal Sketch Plan Application and are looking forward to proceeding in the process. Mogul Capital has worked through the Sketch Plan Pre-Application process and held a productive conversation with the Town Staff and related department representatives. In addition, Mogul Capital held the required neighborhood meeting and included the notes from the discussion and list of attendees herein.

The Project is planned to primarily include a mix of hotel, market rate residential, and workforce housing. Amenities are also planned to benefit the hotel guests, residents, tourists, and residents of the Town of Jackson. The restaurant will be open and available to the public. The condominiums and hotel will also benefit from fitness spaces, pools, hot tubs, and a spa. As currently programmed, the Project includes 114 hotel rooms, 20 units of market rate residential units, and 19 workforce housing units. The Project will include subterranean parking structures that abide by the land development regulations and provide adequate parking for the Project.

The Project proposal includes five individual buildings, all of which abide by the current moratorium restrictions on commercial development. The Project is programmed around a central drop off and loading zone for the respective individual buildings, which is accessed primarily off Glenwood Street. Prior access was programmed off North Cache, which has subsequently been removed based off feedback from the Wyoming Department of Transportation.

Mogul Capital recognizes that this is an iterative process and is excited about the opportunity to present the updated plan set to the Town of Jackson. In response to feedback received from the Town of Jackson and its community members, Mogul Capital made dramatic adjustments to the plan set. A few noteworthy adjustments to the Project plans include the following:

- Building Size:
 - Mogul Capital listened to the community and Town of Jackson feedback through the Development Moratorium and has reduced the size of all commercial buildings to below the 35,000 sf cap on habitable space. This has resulted in a five building Project that provides significant breaks in the massing, and commercial buildings that are below the proposed adjusted building size cap of 50,000 sf that is expected to be implemented to the Land Development Regulations. In addition, Mogul Capital and its architectural team have ensured that each building has its own unique architectural features, roof planes and design to ensure additional diversity amongst the buildings. The architectural features include additional push and pull and provide substantial breaks in the massing of each building.
- Basement and Garage Depth:
 - Mogul Capital removed an entire basement level across the whole Project, which has dramatically reduced the total square footage of the Project. More notably, the updated plans require significantly less excavation and will be positioned much shallower in depth. This provides a substantial adjustment in response to the Town's feedback on the ground water and environmental concerns.
- Project Access:
 - Mogul Capital removed the right-in, right-out access that was previously located off North Cache, and instead has located it off Glenwood Street. While access off North Cache was previously approved through the Wyoming Department of Transportation, the new pre-application process raised WYDOT concerns surrounding the access. In addition, this provides a beneficial modification to the concerns about pedestrian experience while walking down North Cache.
- 2:1 Housing Bonus:
 - The Project now houses all the 2:1 bonus workforce housing density on-site, as compared to the previously contemplated off-site location at The Loop Apartments. While the Land Development Regulations do not currently prevent the 2:1 workforce housing bonus density to be placed off-site, the Town of Jackson is expected to soon amend the Land Development Regulations to require the density to be placed on-site. As such, the Project is programmed with 19 units of workforce housing and no off-site workforce housing is proposed.

While the adjustments listed above are just a few of the changes made to the proposed Project, the changes demonstrate Mogul Capital's willingness to continue working with the Town and Community on the Project. Mogul Capital seeks to make this project a gateway project that will benefit both residents and tourists alike by providing elegant residential housing and lodging. This Project is conveniently located at the north end of town, providing quick access into Yellowstone and the Grand Teton National Park, while also being a short walk to Town Square. By developing this project, we will improve a block with vacant parcels, aged buildings and infrastructure by building a well-designed lodging and housing project.

Attached is our Sketch Plan Application request form and package, the site plan and respective design set, and responses to the Town's comments to the Sketch Plan Pre-Application. We look

forward to moving the application forward and discussing with the Design Review Committee, Planning Commission, and Town Council.

Sincerely,
Brad Wagstaff
Chief Executive Officer



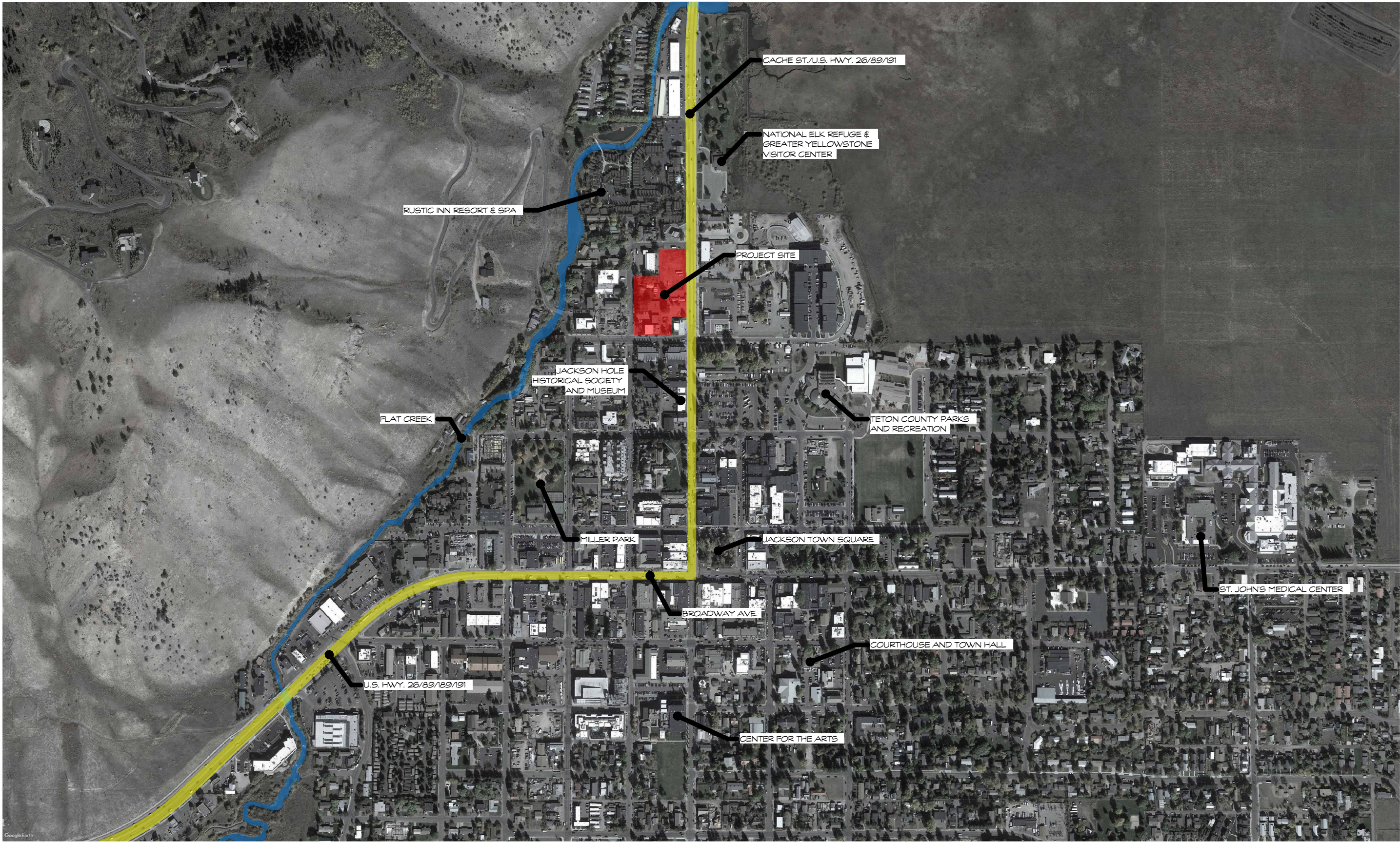
Sheet List

- 1 TITLE
- 2 SITE CONTEXT
- 3 TRAFFIC ANALYSIS
- 4 PROGRAM DATA
- 5 PARKING ANALYSIS
- 6 SITE PLAN
- 7 BASEMENT LEVEL FLOOR PLAN
- 8 FIRST LEVEL FLOOR PLAN
- 9 SECOND LEVEL FLOOR PLAN
- 10 THIRD LEVEL FLOOR PLAN
- 11 ROOF PLAN
- 12 HEIGHT CALCULATION PLAN
- 13 SITE SECTIONS
- 14 ELEVATION
- 15 ELEVATION
- 16 ELEVATION
- 17 ELEVATION
- 18 MATERIAL BOARD A
- 19 MATERIAL BOARD B
- 20 MATERIAL BOARD C
- 21 MATERIAL BOARD D
- 22 MATERIAL BOARD E
- 23 RENDERINGS
- 24 RENDERINGS
- 25 RENDERINGS
- 26 RENDERINGS
- 27 RENDERINGS
- 28 RENDERINGS
- 29 RENDERINGS
- 30 RENDERINGS

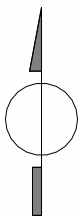


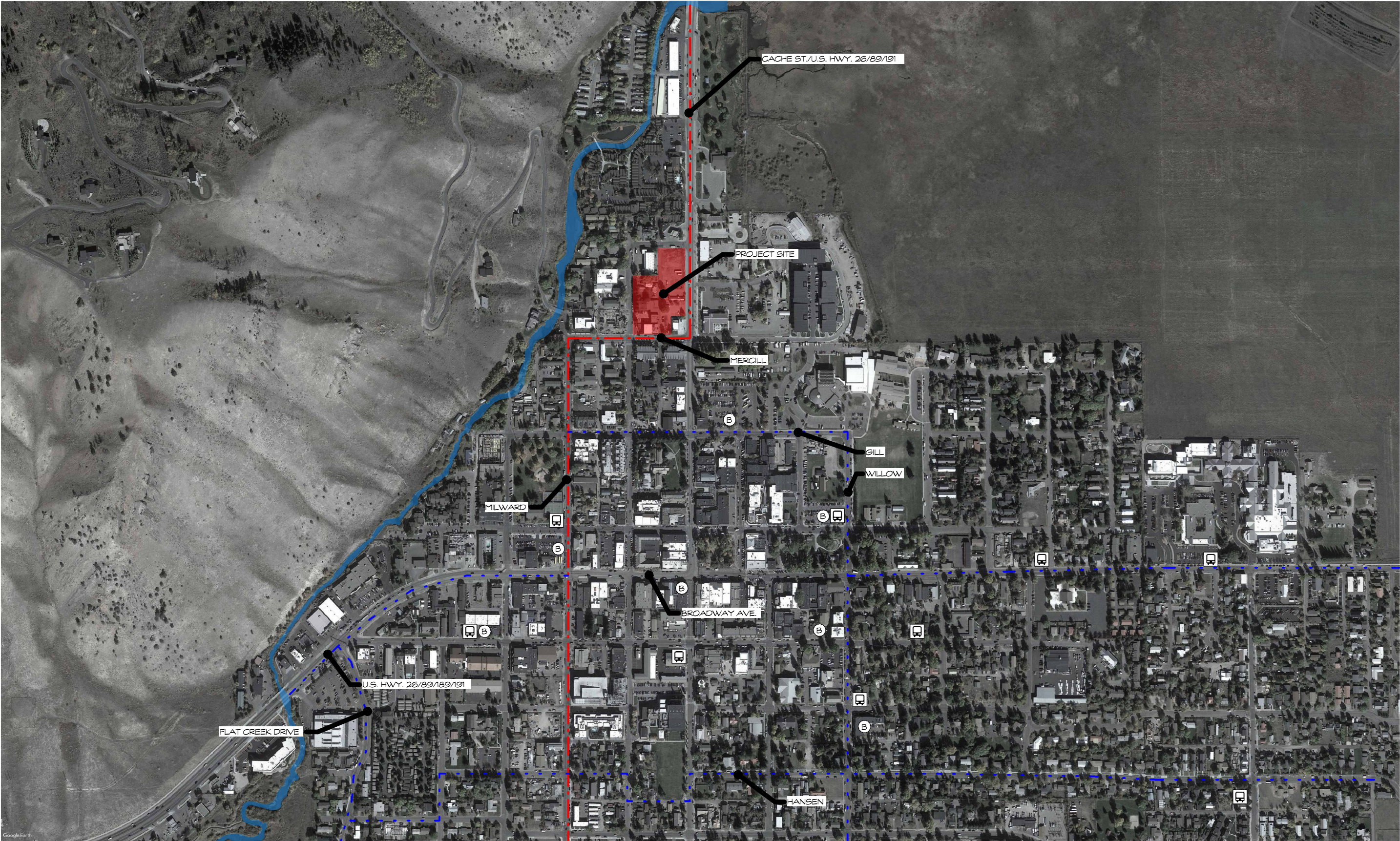
350 N CACHE

upwalldesignarchitects



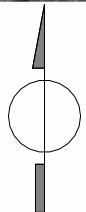
SITE CONTEXT
PLAN





- GRAND LOOP
- PATHWAY
-  START BUS
-  START BIKE

TRAFFIC ANALYSIS
PLAN



PROJECT DATA

SITE AREA: 107,121 SF

ALLOWABLE LODGING FAR:

TOTAL LODGING FAR:

85,696 SF

81,060 SF

SCALE OF DEVELOPMENT

ALLOWED LODGING FAR

TOTAL WORKFORCE HOUSING

ALLOWED MARKET RATE (2:1 BONUS)

TOTAL MARKET RATE HOUSING

BASEMENT EXEMPT AREA

HABITABLE AREA

PARKING AREA

TOTAL HABITABLE AREA (EXCLUDES PARKING)

TOTAL BUILDING AREA INCLUDING PARKING

TOTAL LODGING FAR

TOTAL RESIDENTIAL AREA

85,696 SF

25,325 SF

50,650 SF

48,405 SF

12,060 SF

66,041 SF

184,650 SF

250,691 SF

81,060 SF

88,571 SF

LODGING FAR

ABOVE GRADE

BOH

CIR.

ELEVATOR

HOTEL UNIT

LOBBY

RESTAURANT

VERT. CIR.

BELOW GRADE

BOH

ELEVATOR

EXERCISE

LOBBY

SPA

VERT. CIR.

4,085 SF

10,734 SF

1,467 SF

52,014 SF

2,858 SF

3,509 SF

6,393 SF

81,060 SF

558 SF

349 SF

5,595 SF

915 SF

2,889 SF

1,754 SF

12,060 SF

UNIT COUNT

HOTEL SUITE

HOTEL UNIT

TOTAL HOTEL KEYS

MARKET RATE HOUSING

WORKFORCE HOUSING

TOTAL HOUSING UNITS

13

101

114

20

19

39

NON-HABITABLE (PARKING) AREA		HABITABLE AREA BY LEVEL	
A	36,448 SF	A	
B	7,186 SF	BASEMENT LEVEL	2,959 SF
C	7,153 SF	LEVEL 1	29,578 SF
D	5,115 SF	LEVEL 2	24,525 SF
E	10,140 SF	LEVEL 3	21,725 SF
TOTAL AREA	66,041 SF	78,787 SF	
		B	
		BASEMENT LEVEL	470 SF
		LEVEL 1	6,898 SF
		LEVEL 2	7,829 SF
		LEVEL 3	6,177 SF
		21,374 SF	
		C	
		BASEMENT LEVEL	472 SF
		LEVEL 1	5,219 SF
		LEVEL 2	7,046 SF
		LEVEL 3	6,566 SF
		19,303 SF	
		D	
		BASEMENT LEVEL	10,751 SF
		LEVEL 1	7,820 SF
		LEVEL 2	6,425 SF
		LEVEL 3	6,565 SF
		31,561 SF	
		E	
		BASEMENT LEVEL	367 SF
		LEVEL 1	10,241 SF
		LEVEL 2	12,094 SF
		LEVEL 3	10,924 SF
		33,625 SF	
		TOTAL AREA	184,650 SF

PARKING ANALYSIS



PARKING REQUIRED

164 TOTAL STALLS

WORKFORCE	19 UNITS	29 STALLS	1.5 RATIO
MARKET RATE	20 UNITS	30 STALLS	1.5 RATIO
HOTEL	114 UNITS	86 STALLS	.75 RATIO
COMMERCIAL			
RESTAURANT	1 PER 73SF	46 STALLS	

TOTAL REQUIRED		191	
SHARED RESTAURANT (20%)	-9	182	
BIKE DECREASE (10%)	-18	164 TOTAL FINAL REQUIRED (WITH PLANNING DIRECTOR APPROVAL)	

PARKING PROVIDED

203 TOTAL STALLS

ACCESSIBLE VAN	2	6 FOR THE FIRST 200 STALLS
ACCESSIBLE VEHICLE	7	+1 PER ADDITIONAL 100 STALLS REQUIRED
COMPACT	2	
ON STREET	21	
STANDARD	171	
TOTAL NUMBER OF STALLS	203	
EV PARKING CAPABLE REQUIRED (30% OF STALLS)	61	
INSTALLED EV PARKING REQUIRED (5% OF STALLS)	11	
EV PARKING CAPABLE PROVIDED	49	
INSTALLED EV PARKING PROVIDED	12	

BICYCLE PARKING REQUIRED

91 TOTAL

HOUSING	66 TOTAL (.75 PER BEDROOM)	17 SHORT TERM (25%)	49 LONG TERM (75%)
HOTEL	11 TOTAL (1 PER 11 UNITS)	6 SHORT TERM (50%)	5 LONG TERM (50%)
RESTAURANT	10 TOTAL (1 PER 22 PEOPLE)	7 SHORT TERM (REMAINDER)	3 LONG TERM (3)
AMENITIES	4 TOTAL (1 PER 1650 SQFT)	2 SHORT TERM (50%)	2 LONG TERM (50%)
SHORT TERM BIKE REQUIRED	32		
LONG TERM BIKE REQUIRED	59		

BICYCLE PARKING PROVIDED

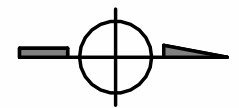
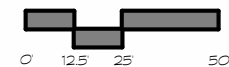
120 TOTAL

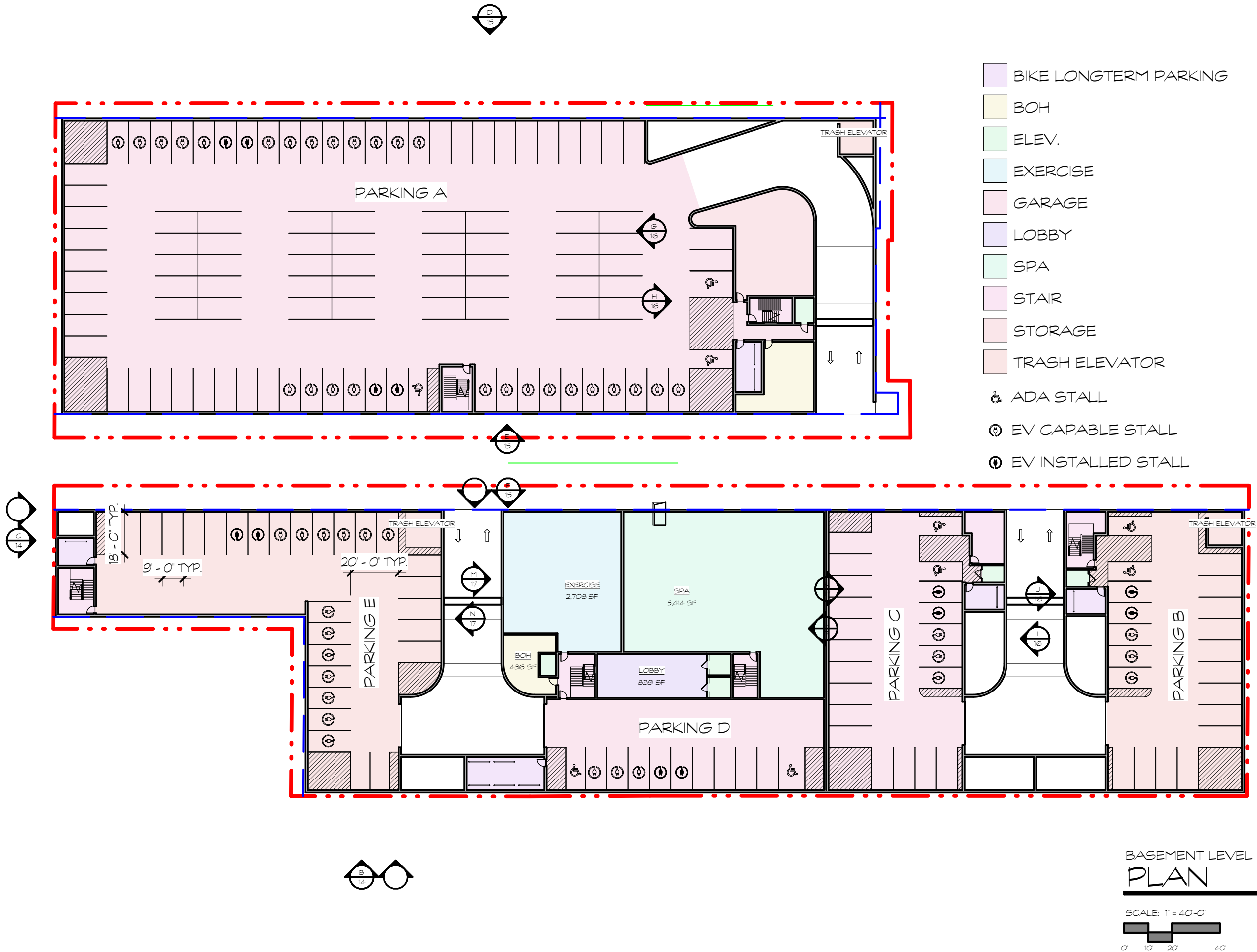
SHORT TERM BIKE PROVIDED	40
LONG TERM BIKE PROVIDED	80



SITE
PLAN

SCALE: 1" = 50'-0"

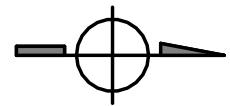
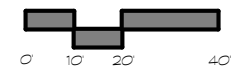






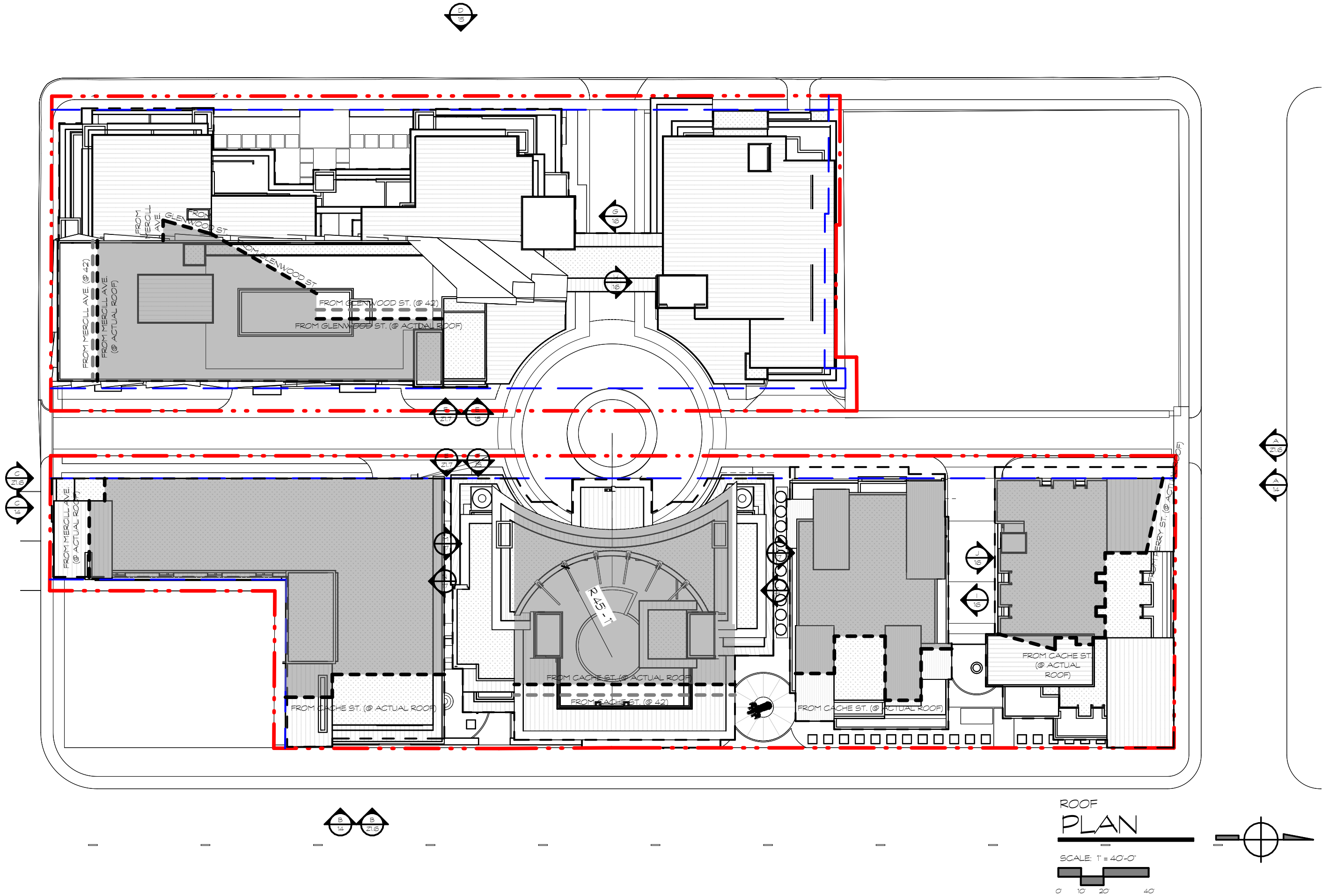
LEVEL 1 PLAN

SCALE: 1" = 40'-0"

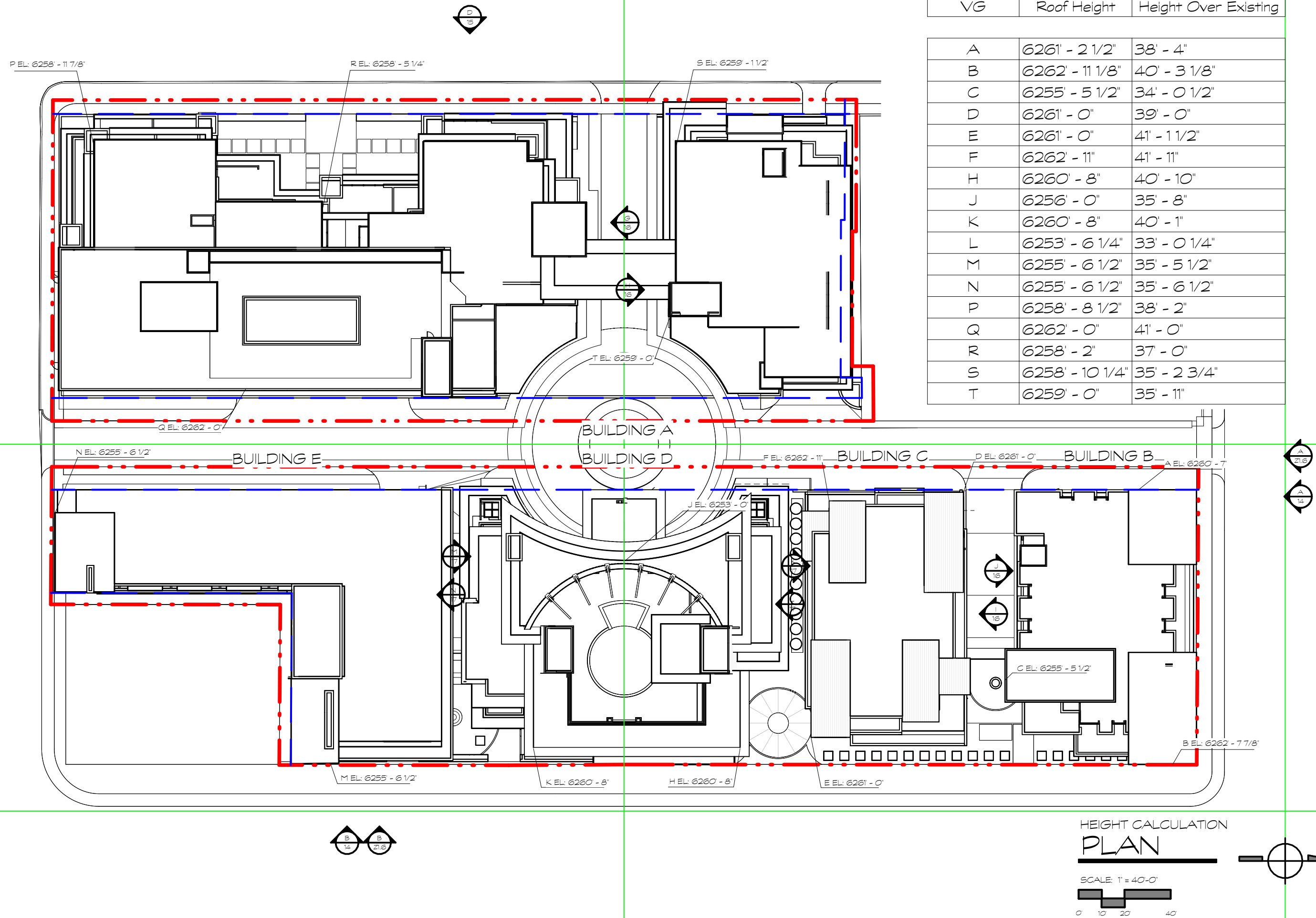


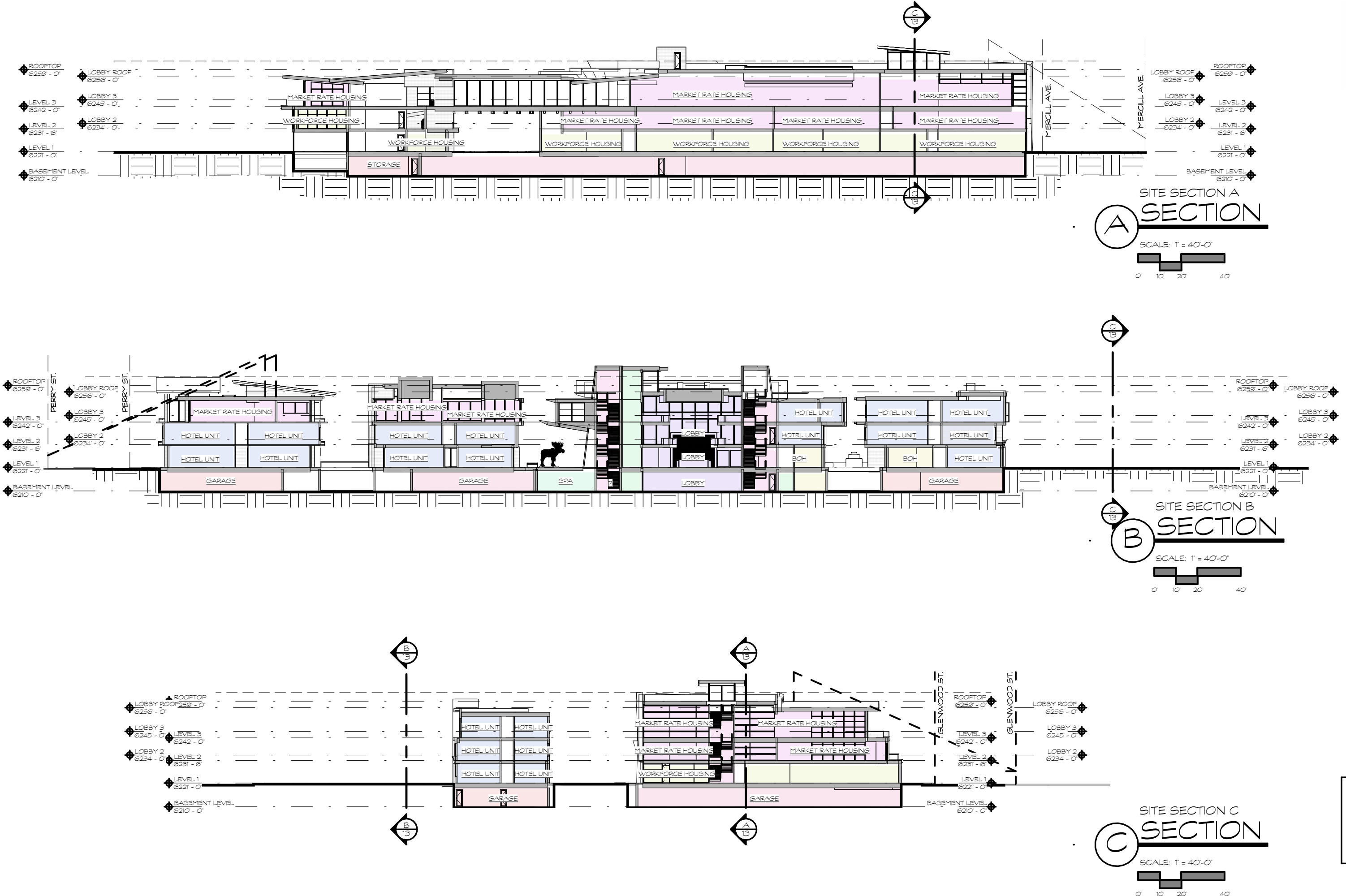






HEIGHT CALCULATION		
VG	Roof Height	Height Over Existing
A	6261' - 2 1/2"	38' - 4"
B	6262' - 11 1/8"	40' - 3 1/8"
C	6255' - 5 1/2"	34' - 0 1/2"
D	6261' - 0"	39' - 0"
E	6261' - 0"	41' - 11 1/2"
F	6262' - 11"	41' - 11"
H	6260' - 8"	40' - 10"
J	6256' - 0"	35' - 8"
K	6260' - 8"	40' - 1"
L	6253' - 6 1/4"	33' - 0 1/4"
M	6255' - 6 1/2"	35' - 5 1/2"
N	6255' - 6 1/2"	35' - 6 1/2"
P	6258' - 8 1/2"	38' - 2"
Q	6262' - 0"	41' - 0"
R	6258' - 2"	37' - 0"
S	6258' - 10 1/4"	35' - 2 3/4"
T	6259' - 0"	35' - 11"





NORTH
A ELEVATION



EAST
B ELEVATION



SOUTH
C ELEVATION





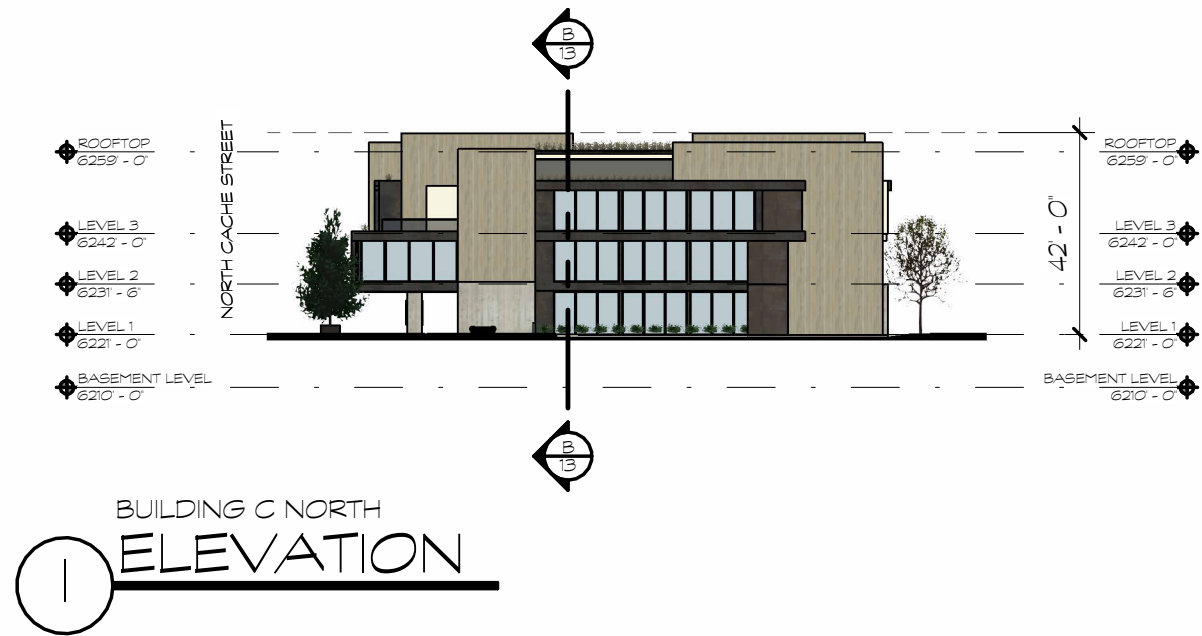
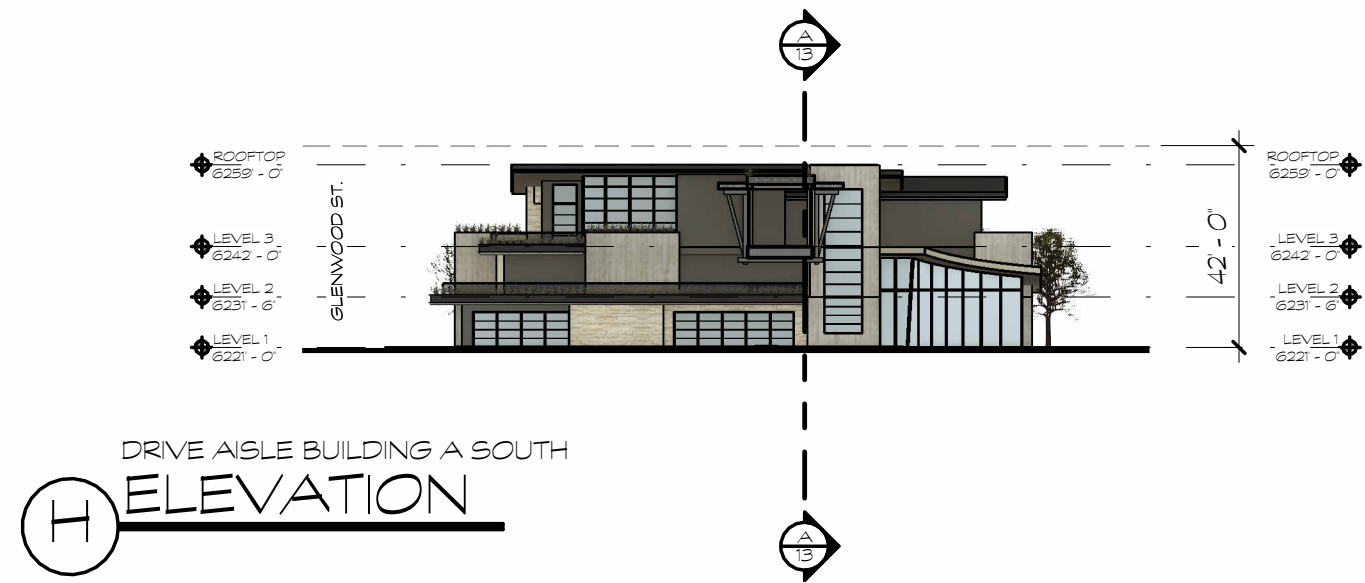
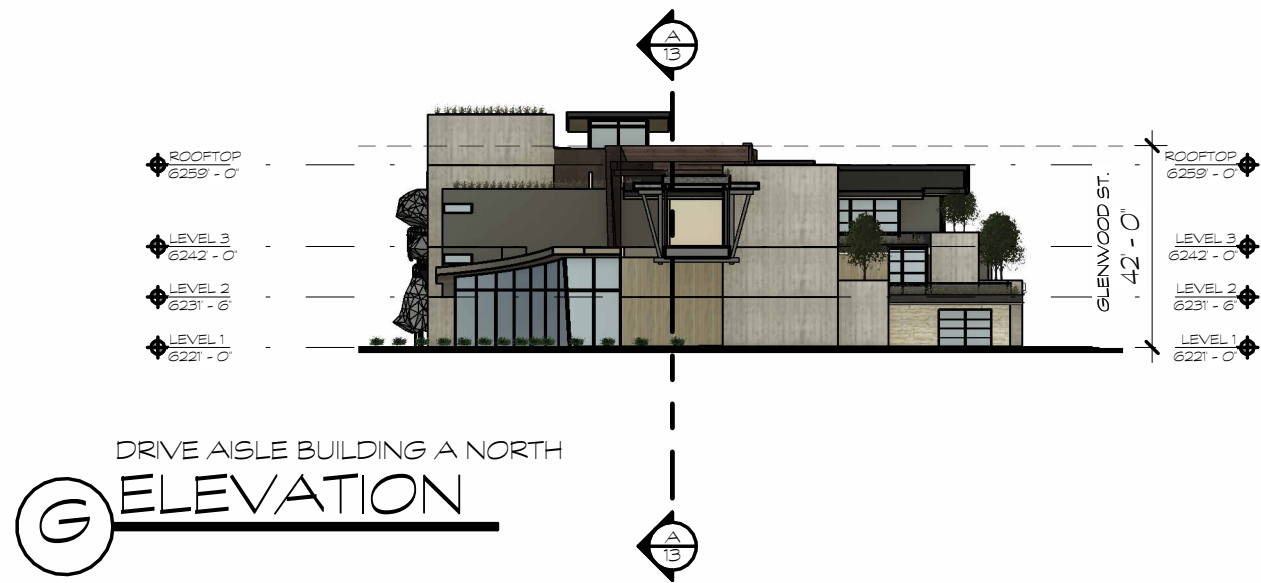
WEST
ELEVATION

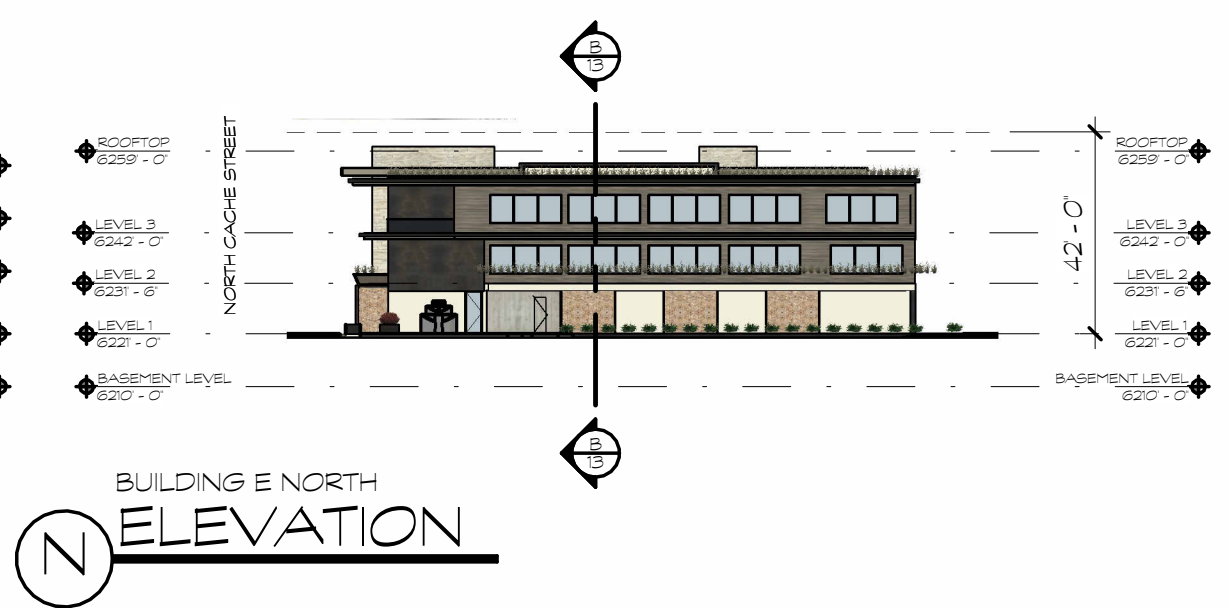
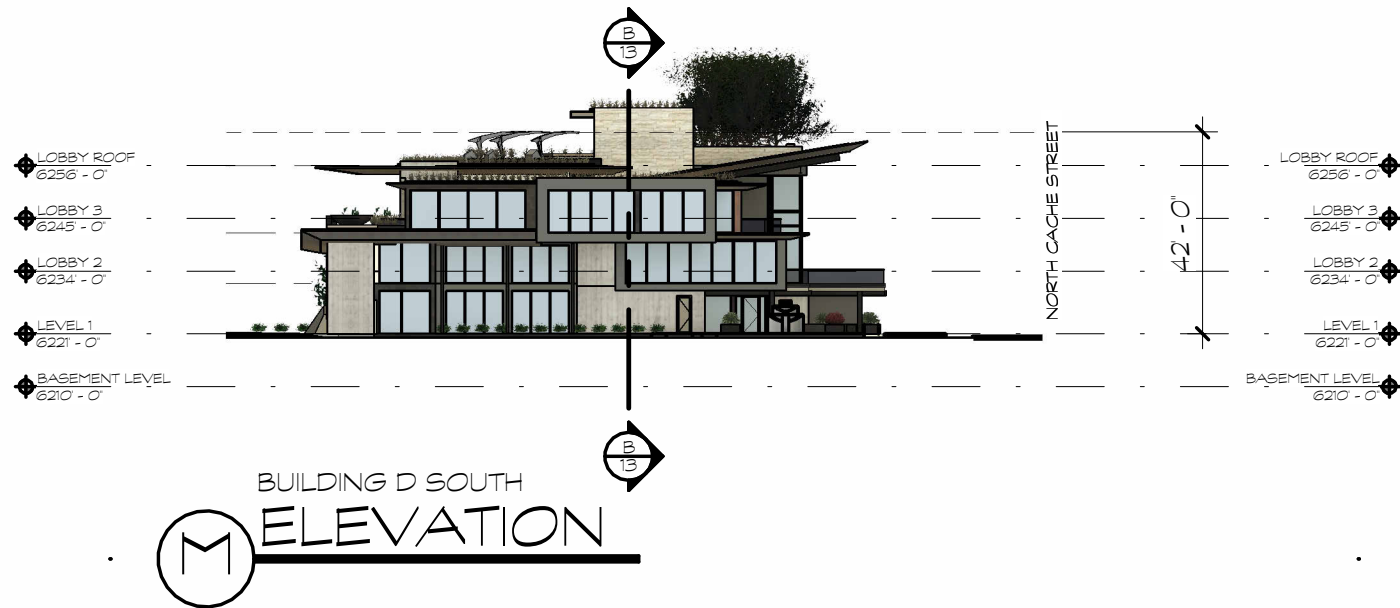
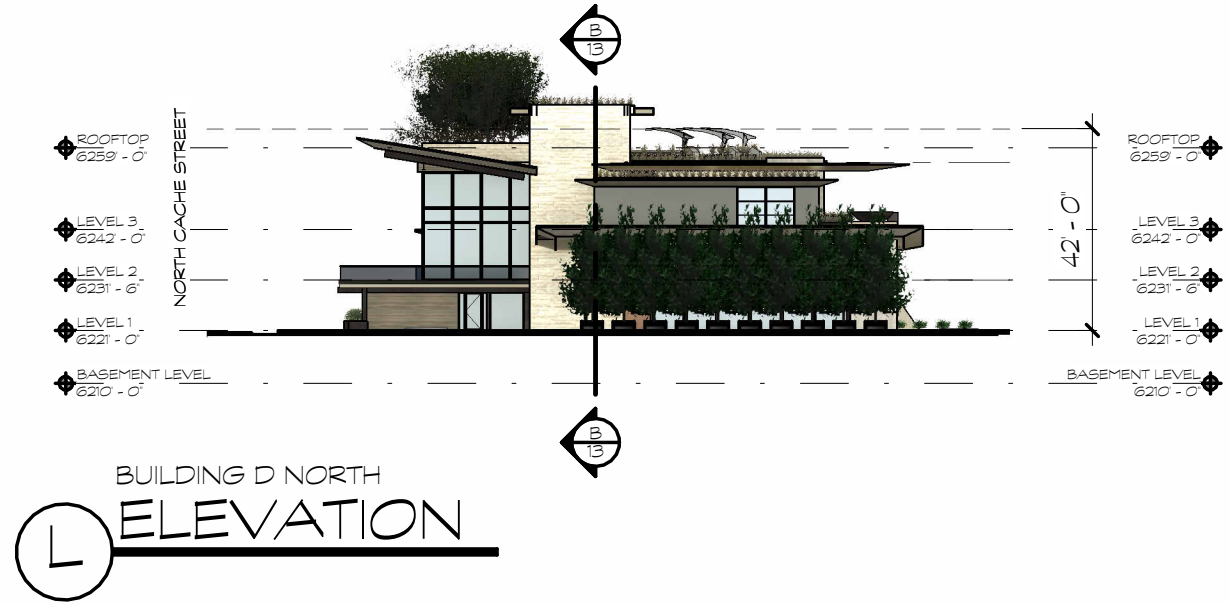
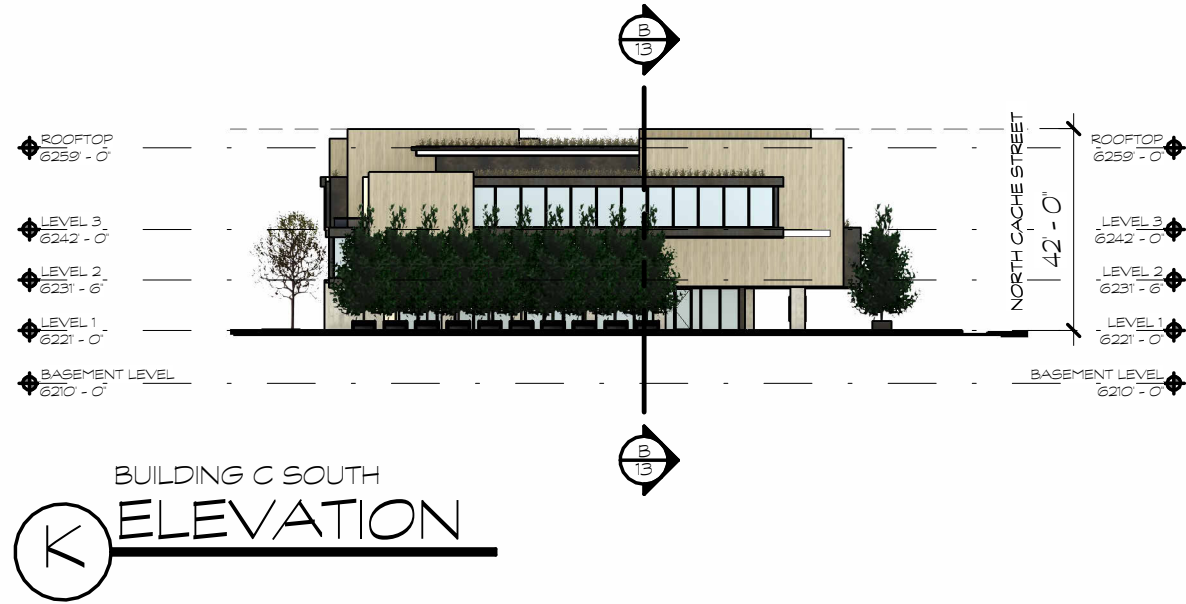


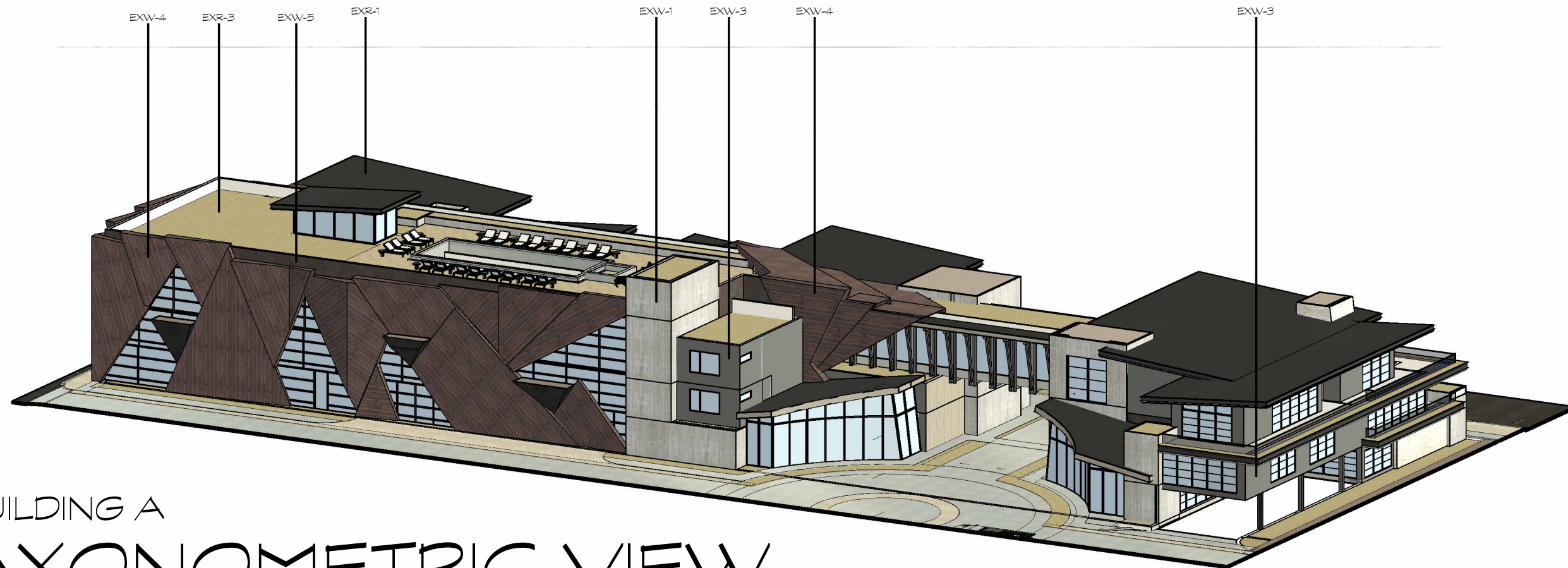
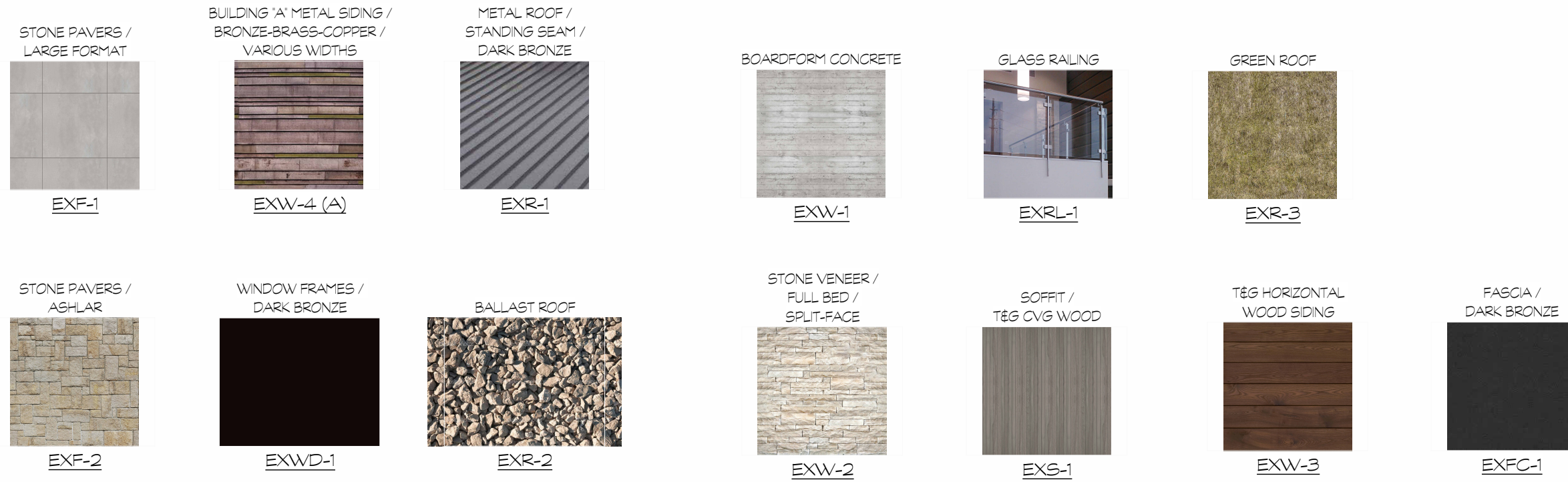
ALLEY EAST
ELEVATION



ALLEY WEST
ELEVATION

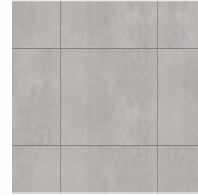






BUILDING A AXONOMETRIC VIEW

STONE PAVERS /
LARGE FORMAT



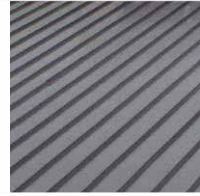
EXF-1

WINDOW FRAMES /
DARK BRONZE



EXWD-1

METAL ROOF /
STANDING SEAM /
DARK BRONZE



EXR-1

STONE PAVERS /
ASHLAR



EXF-2

GLASS RAILING



EXRL-1

BALLAST ROOF



EXR-2

BOARDFORM CONCRETE



EXW-1

SOFFIT / WOOD



EXS-2

GREEN ROOF



EXR-3

T&G HORIZONTAL
WOOD SIDING



EXW-3

FASCIA / WOOD



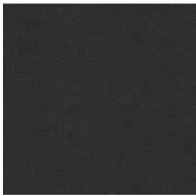
EXFC-2

METAL PANELS /
ACM / DARK BRONZE

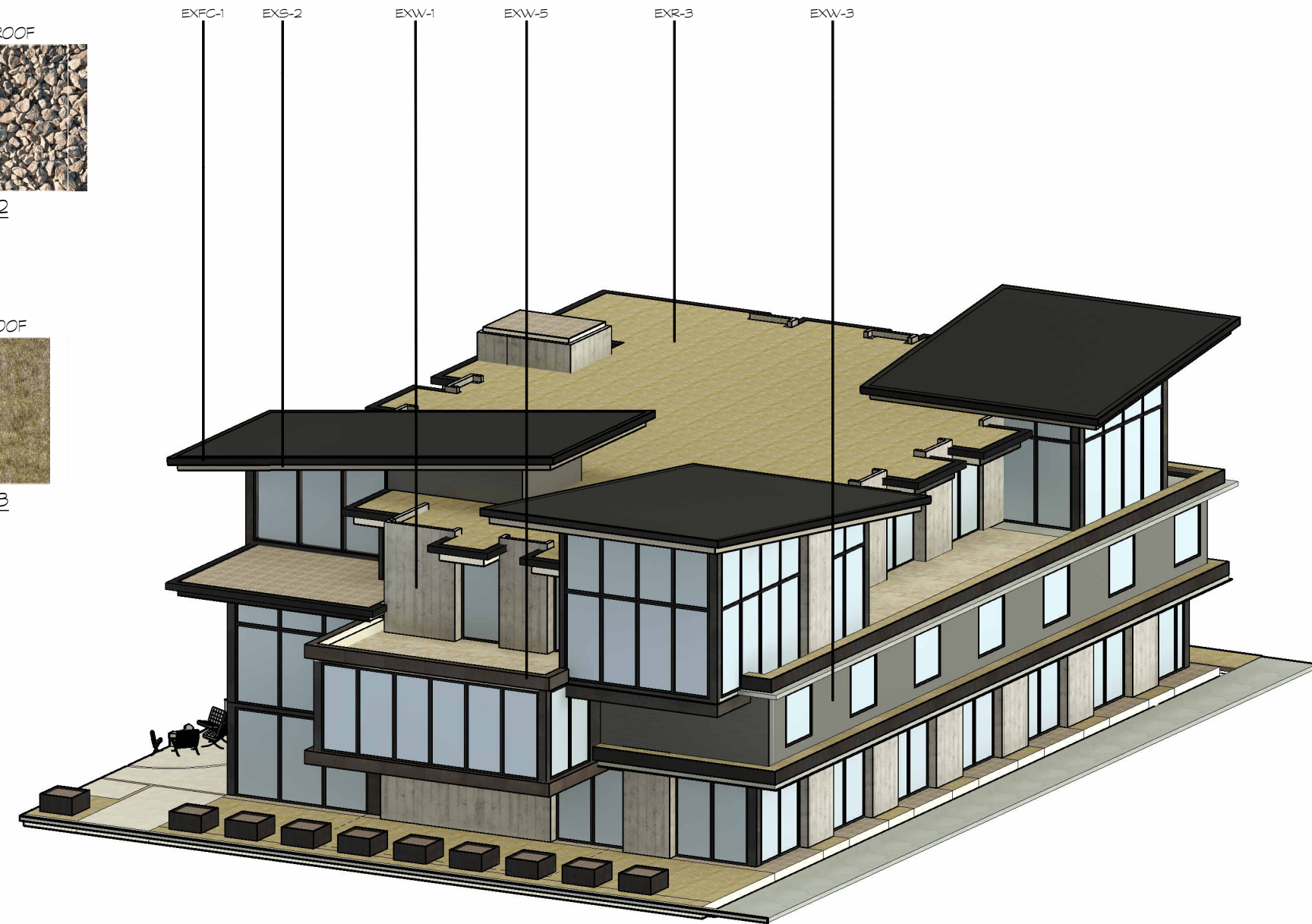


EXW-5

FASCIA /
DARK BRONZE



EXFC-1



BUILDING B

AXONOMETRIC VIEW

STONE PAVERS /
LARGE FORMAT



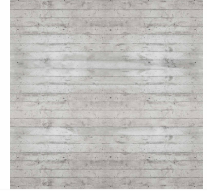
EXF-1

WINDOW FRAMES /
DARK BRONZE



EXWD-1

BOARDFORM CONCRETE



EXW-1

GLASS RAILING



EXRL-1

T&G HORIZONTAL
WOOD SIDING



EXW-3

BUILDING "C" SOFFIT
/ T&G WOOD



EXS-3 (C)

METAL PANELS /
ACM / DARK BRONZE



EXW-5

BALLAST ROOF



EXR-2

BUILDING "C" T&G
VERTICAL WOOD SIDING

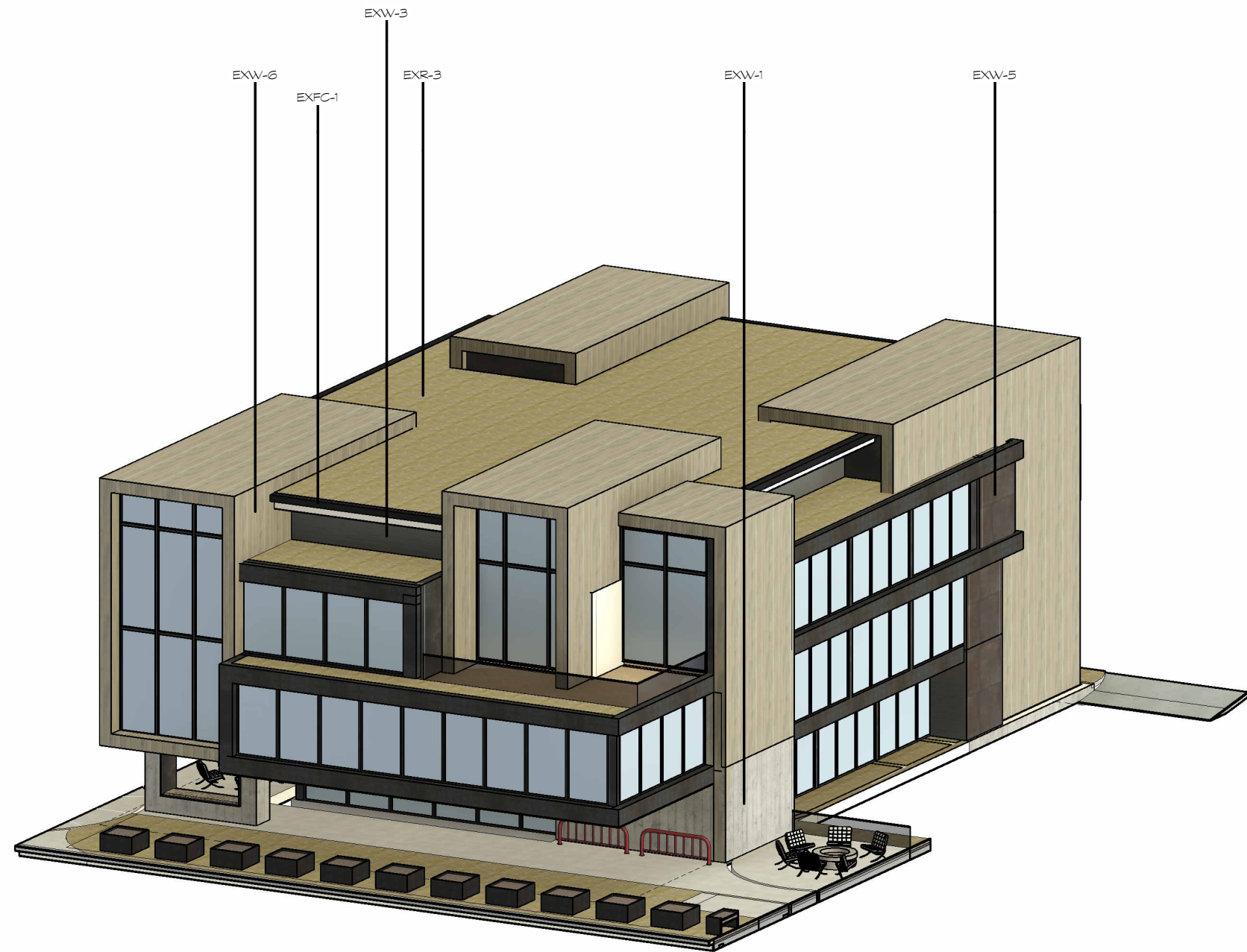


EXW-6 (C)


GREEN ROOF

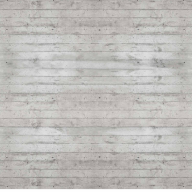



EXR-3





BUILDING C AXONOMETRIC VIEW

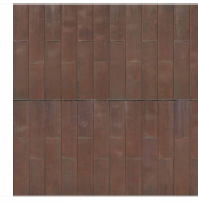
STONE PAVERS /
LARGE FORMAT

EXF-1


BOARDFORM CONCRETE

EXW-1


STONE VENEER /
FULL BED /
SPLIT-FACE

EXW-2


T&G HORIZONTAL
WOOD SIDING

EXW-3


BUILDING /
WOOD SIDING / RECLAIMED

EXW-7

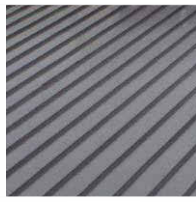
BUILDING "D"
METAL PANELS

EXW-8


WINDOW FRAMES /
DARK BRONZE

EXWD-1


GLASS RAILING

EXRL-1

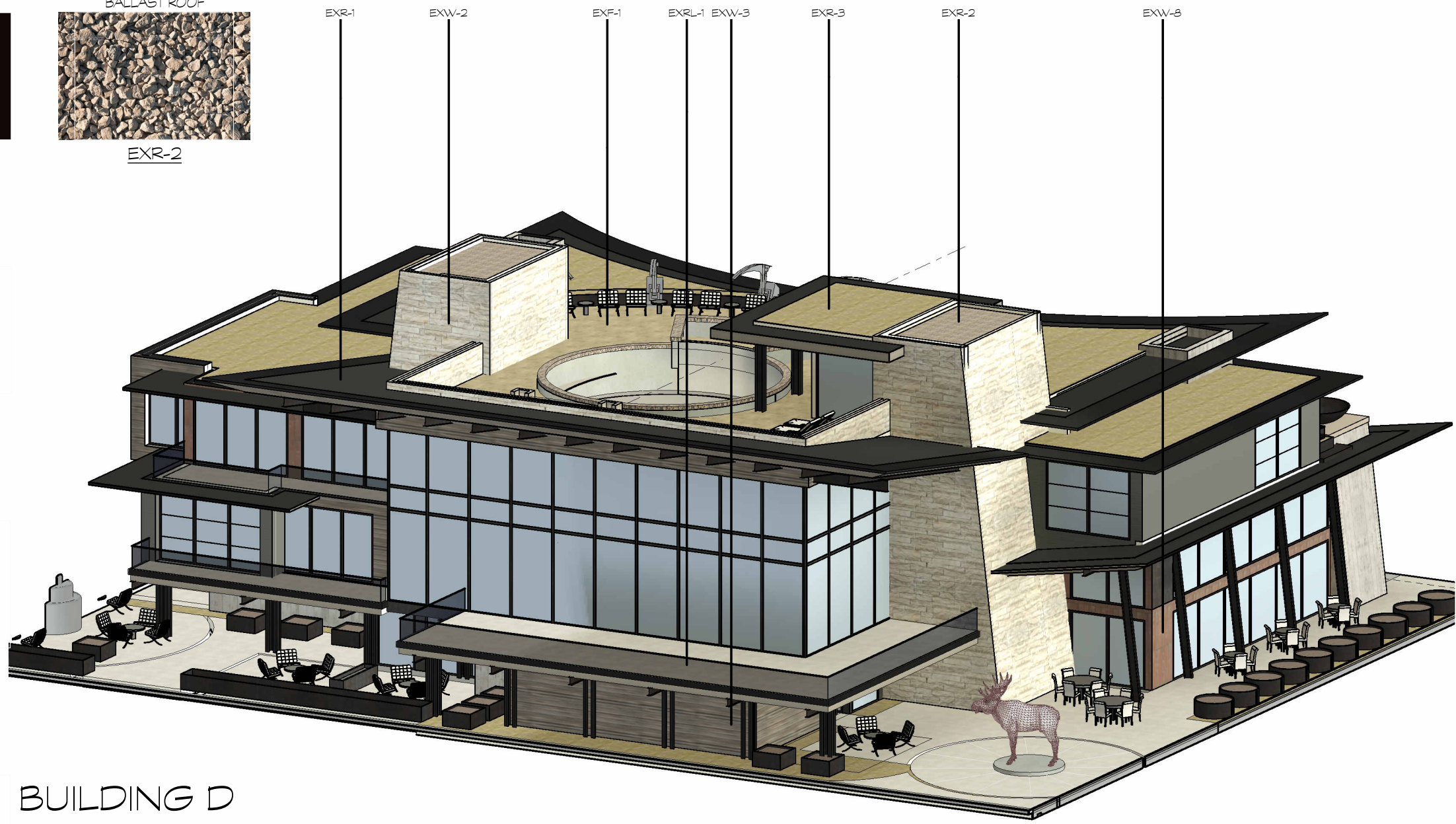
SOFFIT /
T&G CVG WOOD

EXS-1

FASCIA /
DARK BRONZE

EXFC-1

METAL ROOF /
STANDING SEAM /
DARK BRONZE

EXR-1

BALLAST ROOF

EXR-2

GREEN ROOF

EXR-3



EXR-1 EXW-2 EXF-1 EXRL-1 EXW-3 EXR-3 EXR-2 EXW-8

BUILDING D

AXONOMETRIC VIEW

STONE PAVERS / LARGE FORMAT

EXF-1

SCREEN / SPLIT LOGS / STACKED

EXW-9 (E)

FASCIA / DARK BRONZE

EXFC-1

GLASS RAILING

EXRL-1

GREEN ROOF

EXR-3

SOFFIT / WOOD

EXS-2

FASCIA / WOOD

EXFC-2

BOARDFORM CONCRETE

EXW-1

WINDOW FRAMES / DARK BRONZE

EXWD-1

BALLAST ROOF

EXR-2

STONE VENEER / FULL BED / SPLIT-FACE

EXW-2

METAL PANELS / ACM / DARK BRONZE

EXW-5

BUILDING / WOOD SIDING / RECLAIMED

EXW-7

EXW-1

EXW-5

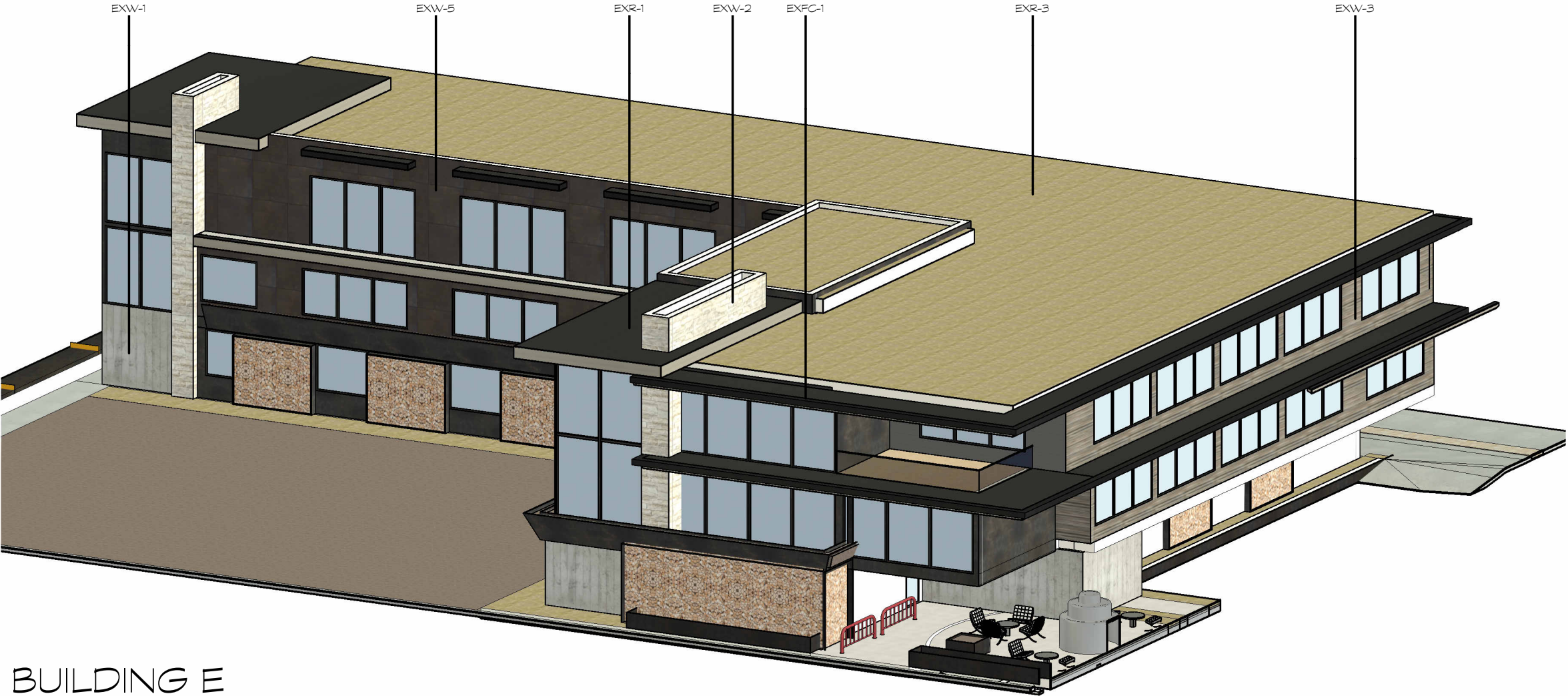
EXR-1

EXW-2

EXFC-1

EXR-3

EXW-3



BUILDING E

AXONOMETRIC VIEW



BUILDING D FROM N CACHE



BUILDING C FROM N CACHE



BUILDING B FROM N CACHE



BUILDING A



BUILDING A



BUILDING A FROM SOUTH



BUILDING A FROM COURTYARD



BUILDING D FROM COURTYARD

Land Development Regulation Compliance

Zoning: Section 2.2.12 CR-2 Commercial Residential - 2

2.2.12.8.1 Lot Standards

ITEM 1: Building Setbacks:

- 1.1 Primary Street Setback range
Required: (min.-max.) 0'-10'
Provided: 0'
- 1.2 Secondary Street Setback range
Required (min.-max.) 0'-10'
Provided: 0'
- 1.3 Side Interior
Required: (min.) 5'
Provided: 5'
- 1.4 Alley
Required: 10'
Provided: 10'
- 1.5 Abutting protection
Required: (min.) 10'
Provided: Not Applicable

ITEM 2: Landscaping Area:

- 2.1 Landscape Surface Ratio
Required: 10% = 11,300 SF
Provided: 13% = 14,000 SF

ITEM 3: Plant Units:

- 3.1 All uses
Required: 1/1000 SF Landscape Area
Provided: 14 Minimum

Required: 1/12 Parking spaces
Provided: Not Applicable

ITEM 4: Fencing:

4.1 Height in any primary or secondary street yard
Required: (max.) 4'
Provided: Not Applicable

4.2 Height in interior side or rear yard
Required: (max.) 6'
Provided: Not Applicable

4.3 Setback from pedestrian frontage
Required: (min.) 1'
Provided: Not Applicable

4.4 Setback from side or rear lot line
Required: (min.) 0'
Provided: Not Applicable

ITEM 5: Parking Setbacks:

5.1 Primary Street, above ground
Required: (min.) 30'
Provided: Not Applicable

5.2 Secondary Street, surface parking
Required: (min.) 30'
Provided: Not Applicable

5.3 Secondary Street, tuck-under, enclosed, or structured parking
Required: (min.) 0'
Provided: Not Applicable

ITEM 6: Access:

6.1 Curb Cut Width
Required: (max.) 24'
Provided: along Glenwood St. = 20'
along Cache St. = Not Applicable

2.2.12.8.2 Bulk Standards

ITEM 7: Street Facade:

7.1 Width of ground and second story in primary setback range
7.1.1 % of lot width

Required: (min.) 70%

Lot width

North: 130' (Required Min:91')

East: 400' (Required Min:280')

South: 220' (Required Min:154')

West: 350' (Required Min:245')

Provided:

North: L1: 108' L2: 108'

East: L1: 328' L2: 316'

South: L1: 167' L2: 160'

West: L1: 300' L2: 270'

7.1.2 Length from street corner

Required: (min.) 30'

Provided: L1 = 70', L2 = 70'

7.2 Width of ground and second story in secondary street setback range

7.2.1 % of lot width

Required: (min.) 35%

Lot width is N:130' (Required Min:45.5')

Provided: L1 = 107', L2 = 107'

7.2.2 Length from street corner

Required: (min.) 30'

Provided: L1 = 107', L2 = 107'

7.2.3 Forecourt

Required: 1/3 of facade width (max.) = 35' depth (min.); within 30" of grade, transparency or forecourt facing facade = 40%

Provided: 280' facade width/3; 93' provided, 20'

ITEM 8: Building Height:

8.1 Stories

Required: (max.) 3

Provided: 3 stories above grade + 1 exempt basement levels

8.2 Height

Required: (min.): 24' - (max.): 42' to top of parapet (T.O.P.)

Exception: 75% transparency for roof railings/paparets

Provided: (min.): 42' - (max.):

ITEM 9: Building Stepback:

9.1 Stepback for any 3rd story street facade or street facade over 30'

Required: (min.)10'
Provided: 10'

- 9.2 Encroachment in setback
Allowed: (max. % of overall facade width) 60%
Provided: 60%

ITEM 10: Scale of Development:

- 10.1 Total Site Area: 107,121 SF)
- 10.2 Allowed Lodging Floor Area Ratio (FAR) 85,696 SF
- 10.3 Allowed 2:1 Bonus Housing: 50,650 SF
- 10.4 Basement Exempt Area:
Habitable: 12,060 SF
Basement Parking: 66,041 SF
- 10.5 Total Habitable Area (Excludes Structured Parking): 184,650 SF
- 10.6 Total Building Area (Including Parking) = 250,691 SF
- 10.7 Total Lodging and Amenities SF (80,060 SF FAR + 12,060 SF Exempt) = 92,120 SF

2.2.12.8.3 Form Standards

ITEM 11: Pedestrian Frontage:

- 11.1 Trees in grates
- 11.1.1 Frontage dimensions
Required: (min.) 13.5' from back of curb
Provided: 13.5' min.

ITEM 12: Building Frontage Options: Residential

- 12.1 Story height
Required: Ground story height (min.) 9'
Upper story height (min.) 9'
Ground floor elevation (min.-max.) 0'-5'
Provided: Ground story height (min.) 12'

Upper story height (min.) 9'
Ground floor elevation (min.-max.) 0'-5'

12.2 Transparency

Required: Ground story primary street (min.): 20%
Ground story secondary street (min.): 20%
Upper story, primary/secondary (min.): 20%
Provided: Ground story primary street: greater than 40%
Ground story secondary street: greater than 40%
Upper story, primary/secondary: greater than 20%

ITEM 13: Parking Type Options: Underground parking

2.2.12.8.4 Environmental Standards Noted

Not Applicable

2.2.12.8.5 Scenic Standards

ITEM 14: Exterior Lighting:

- 14.1 Light Trespass is prohibited
Site Lighting design prohibits trespass
- 14.2 All light fixtures over 600 lumens shall be fully shielded
All light fixtures over 600 lumens are fully shielded
- 14.3 Max. lumens per SF of site development
Required: 3
Provided: Will Comply
- 14.4 Lumens per acre of site development (max.)
Required: All fixtures - 50,000
Unshielded fixtures - 2,000
Provided: Will Comply
- 14.5 Light Color
Required: $\leq 3,000$ Kelvin
Provided: 3000 Kelvin

2.2.12.8.6 Natural Hazard to Avoid

Noted, Not Applicable

2.2.12.8.7 Signs

Noted and will comply

2.2.12.8.8 Grading, Erosion Control, and Stormwater

Noted

2.2.12.8.9 Physical Development Permit Required

ITEM 15: ≥15,000 SF Required:
 - Sketch Plan
 - Development Plan
 - Building Permit
 - DRC Review

2.2.12.C.1 Allowed Uses

ITEM 16: Allowed Uses:

 16.1 Mixed Uses - Residential/Lodging

2.2.12.C.2 Use Requirements

ITEM 17: Use Requirements:

 17.1 Parking

 17.1.1 Residential
 Parking Factor: 1.5
 Residential Unit Count: 39
 Parking Stalls Required: **59 Stalls**

 17.1.2 Commercial
 Hotel Parking Factor: 0.75
 Hotel Unit Count: 114
 Parking Stalls Required (Hotel): **86 Stalls**
 Restaurant Parking Factor: 1/73 SF - 20%
 Restaurant Area: 3,509 SF
 Parking Stalls Required (Restaurant): **37 Stalls**
 Hotel Bar & Lounge Parking Factor: 1/40 SF/3
 Hotel Bar & Lounge Area: N/A
 Parking Stalls Required (Hotel Bar & Lounge): N/A
 Bicycle reduction: **-18 Stalls**

17.1.3 Disability Parking Spaces

Required: if ≥ 200 , 6 + 1 per 100 parking spaces provided

Provided: 9 stalls

17.1.4 Electrical Vehicle Supply Equipment (EVSE)

Required: (Other Residential/Lodging)

I. EVSE Capable: 30%

203 stalls x 0.30 = 61 stalls

II. EVSE Installed: 5%

203 stalls x 0.05 = 11 stalls

Provided:

I. EVSE Capable: 49 stalls

II. EVSE Installed: 12 stalls

17.1.5 Street Parking Credit - 21

Total Parking Required: 164 stalls

Total Parking Provided: 203 stalls

2.2.12.C.3 Operational Standards

Noted and will comply

2.2.12.D.1 Allowed Subdivision and Development Options Noted

Noted

2.2.12.D.2 Residential Subdivision Requirements

Noted

2.2.12.D.3 Required Subdivision and Development Option Permits

Noted

Findings for Sketch Plan Approval:

- **Consistent with desired future character:**
 - o This finding is not applicable according to the Wyoming Supreme Court. However, The project is located in the CR-2 zone which the Town has identified as a commercial area. Per the comprehensive plan, the future goal is to create a vibrant pedestrian-oriented mixed-use district with a variety of non-residential and residential uses and is intended for redevelopment with a mixture of two to three story commercial, lodging and residential buildings, a high level of pedestrian and transit connection, high quality public spaces and a high concentration of tourist related accommodations and services. Parking should be hidden underground. Buildings should contain good urban design principles including massing, articulation, and provision of public spaces. The project is in the Town's lodging overlay. The comprehensive plan further elaborates and notes that a key component of achieving this goal will be to have visitors and residents visit the area more often and stay longer by increasing the availability of lodging and residential units and creating a vibrant Retail Shopping District. The Proposed Project meets the desired future character for its location. The Proposed Project is a mixed use project that includes a combination of residential units, lodging, and a restaurant. The project is programmed to attract both residents and tourists alike. The project is anticipated to both enhance the pedestrian experience and create a pedestrian-oriented mixed use campus with multiple uses.
- **Achieves the standards and objectives of the NRO and SRO if applicable:**
 - o Neither of these items are applicable.
- **Does not have significant impact on public facilities and services including transportation potable water and wastewater facilities parks schools police, fire and EMS.**
 - o The Project will not have a negative impact on public facilities and services including transportation, potable water and wastewater facilities, parks, schools, police, fire, or EMS. Attached herein is an analysis and estimate on the water and waste water usage for the Project, which Applicant will be working with the Town Engineering department to refine as the project is finalized. In addition, should the Project require additional utility capacity, the Applicant will pay its share to upgrade those utilities. With respect to stormwater, the Project will not discharge more than the historical amount. The alley is not being vacated thus will not hinder alley access/utilities and access for Fire/EMS. The project will require DEQ permits and approval for addressing the existing site contamination. As a reminder, the proposed site has had substantial historical uses, some of which have been demolished. Those uses included a 25 key motel, two office buildings totaling approximately 20k sf, multiple residential homes, and multiple retail buildings.
- **Complies with all relevant standards of the LDRS and other town ordinances as can be determined by the level of detail of a sketch plan**

- The Project complies with all relevant standards of the LDRs, design guidelines and other Town Ordinances as can be determined by the level of detail supplied for Sketch Plan review.
- **Is in substantial conformance with all standards or conditions of any prior applicable permits or approvals**
 - No applicable prior permits or approvals.



October 31, 2024

RE: Sketch Plan Application: N Cache Lodging and Residential Development

Dear Town Staff,

Pursuant to the Pre-Application comments provided by the Town Staff and respective departments on October 30th, 2024, please find responses to the individual items below. To guide the reading below, the comments received from the Town are marked with a number, whereas Mogul's response begins with a letter.

With respect to several departmental review comments, Applicant recognizes that the purpose of Sketch Plan is to publicly review large physical development for general consistency with the LDRs at a preliminary conceptual level of detail before the development project is fully designed. We look forward to finalizing the site designs through the public process to finalize our responses to the comments below. The Project will be LDR compliant.

Engineering:

1. All improvements shall be designed by a professional engineer licensed to do such work in the State of Wyoming.
 - a. All improvements will be designed by a professional engineer licensed to do work in Wyoming.
2. The Town Council may require installation and construction of utilities, pavement and other land improvements in excess of subdivision design needs, to assure adequate service to future development areas.
 - a. Applicant looks forward to discussing with Town Council once the site plan is finalized through the Sketch Plan process and Town Engineering can provide feedback on future development requirements.
3. The water main along Cache Street requires upsizing.
 - a. Applicant looks forward to further analyzing and working with Town Engineering during Development Plan, and Engineering has provided further feedback to determine whether upsizing is necessary, based on attached estimated calculations as well as determining any cost share structure applicable to such upsizing.
4. The water mains in Perry and Mercill may require upsizing.
 - a. Applicant looks forward to further analyzing and working with Town Engineering during Development Plan, and Engineering has provided further feedback to determine whether upsizing is necessary, based on attached estimated

calculations as well as determining any cost share structure applicable to such upsizing.

5. The sewer main in the alley may need replaced or rerouted directly to Glenwood.
 - a. Applicant looks forward to further analyzing and working with Town Engineering during Development Plan, and Engineering has provided further feedback to determine whether upsizing is necessary, based on attached estimated calculations as well as determining any cost share structure applicable to such upsizing.
6. The Cache Creek Tube needs to be preserved or re-aligned, including providing/vacating easements as appropriate.
 - a. Applicant's Project will result in continuous flows through the Cache Creek Tube at its current alignment at the east and west property lines. Any re-routing or upsizing over applicant's property will be coordinated with the Engineering department and appropriate easements will be granted. Stormwater runoff from this project will impact the decision to potentially upsize the tube if required. The attached utility plan includes a proposed re-routing of the Cache Creek tube.
7. Provide plans and details that address the environmental issues with this area. Provide documentation or approval from the Wyoming Department of Environmental Quality for the proposed work.
 - a. Applicant recognizes that a portion of the site is identified as contaminated. Wyoming Department of Environmental Quality has previously produced a Remedial Action Plan. Applicant will be providing updated development plans to the DEQ so that an updated 2024 remedial action plan can reflect the current plans and impacts on the plume. The Remedial Action Plan will present potential remedial action levels for the site, potential remedial alternatives, budgetary estimates of costs to implement select remedial options, and a recommendation on the remedial option best suited for the site. As site plans are finalized through Sketch Plan approval and impacts can be specifically addressed in the updated Remedial Action Plan, Applicant shall at its expense hire a qualified environmental remediation consultant to analyze impacts and remediation alternatives and shall provide a copy of the report to council for its input. Such consultant's report will address the means and methods of addressing contaminated soil such as through dig and haul, insitu soil stabilization, insitu chemical oxidation, soil shredding, on-site soil mixing and chemical oxidation. The consultant will remain engaged through the remediation process, working directly with the DEQ and Town Engineering team. Ultimately, the Applicant will implement DEQ's recommended remedial option best suited for the site as set forth in the Remedial Action Plan which will also address any need to manage and dispose of petroleum impacted water generated by any dewatering. Lastly, if recommended, the Applicant will work with an environmental consulting firm to install subgrade vapor mitigation systems.
8. Provide information on groundwater depths, impacts to groundwater, groundwater quality, dewatering treatment and disposal (during construction and any permanent facilities), and address possible issues with surrounding property due to dewatering.

- a. Applicant has conducted geotechnical analyses on the site and will restudy the Sketch Plan approved project. During construction, any groundwater encountered will be pumped from the area containing the subterranean parking area, treated for sedimentation, and injected back into the groundwater. Petroleum contaminated groundwater will be treated as required by Wyoming DEQ. Construction dewatering is a common practice and will be done in compliance with the Wyoming DEQ and qualified and experienced consultants. Final site plan and geotechnical analysis will inform the amount of dewatering and the methods by which such subsurface water shall be treated and disposed of. In response to Town and Community feedback, Applicant removed an entire basement level across the whole Project, which has dramatically reduced the depth of the structures and impact to the groundwater/volume of dewatering necessary.
9. Show the proposed kitchen/cafe/restaurants grease mitigation. Appropriately sized grease trap/interceptor(s) are required. Sizing and grease retention calculations will need to be submitted with the building permit for approval.
 - a. Applicant recognizes grease mitigation will be an important component of kitchen/restaurant activities and will be finalized and shown during Development Plan.
10. Encroachment Agreements are required for any encroachments into the public way.
 - a. Applicant anticipates encroachments into the Town ROW which will principally be soil nails and as the site plan is finalized, we will work with Engineering to obtain appropriate permits.
11. Provide plans, details, and dimensions, etc. to pave the alley to ToJ standards, including any curb and gutter required for drainage. Provide a plan for improvements, access, and use of the alley, including finished surfaces. Depending on final utility plans, loading, access, services, maintenance arrangements, etc. alternative designs for the alley, it may be possible to install a non-standard alley that is less utilitarian and provides a more functional space.
 - a. As opposed to traditional alleyway, applicant would like to explore enhancing this alleyway for both vehicular, pedestrian, and cyclist traffic. Applicant proposes to pay for the approved enhancements and to assume maintenance responsibility for the entirety of the alleyway, potentially transforming this from a traditional alleyway to an enhanced, activated, multimodal passageway. We look forward to exploring requirements and desires from engineering and the Town council.
12. Provide a plan for proper snow storage including a plan for snow removal for the proposed cul-de-sac/roundabout area.
 - a. The plan set attached identifies snow storage locations. With regard to the cul-de-sac drop off area on private property, outside of the alleyway, Applicant plans to use a snow melt system to manage the snow fall, and to explore whether or not the Town would accept a snow melt system underneath the alleyway in the portion made up of the cul-de-sac.
13. Provide justification and reasoning for the access to building E from Glenwood.
 - a. The plan set attached has been adjusted based off of WYDOT feedback through the pre-application process. WYDOT has indicated they will no longer grant access from North Cache and as such, Applicant has responded with the attached plan

set with access from Glenwood and the alley. Applicant will pursue right of way permits for the construction a new curb cut and any enhancements to the Glenwood curb extending the length of the property.

14. Provide a plan which shows the location of trash receptacles and large truck deliveries.
 - a. Please see this noted in the plan set attached for preliminary locations of trash receptacles and large truck access deliveries.
15. Provide plans and details for the right-of-way improvements, e.g. alley pavement, sidewalks, and pathways including streetlights, street signage, pavement/curb markings, etc. Include right of way section analysis to determine where the center of existing road is relative to the property boundary and curb location relative to the community streets plan. Initial review shows that Glenwood and Perry are wider than proposed community street sections and the curbs could be moved out to facilitate buffers in the frontage. Mercill is more complicated and design should be coordinate with Town staff, including Pathways to determine if a protected bike lane is warranted.
 - a. In discussions during the pre-application, Staff has identified some potential availability for sidewalk and curb enhancement in conjunction with the community streets plan and existing wider streets. Applicant looks forward to discussions with Council and Staff to determine the appropriate locations for enhancements as well as any cost share. All improvements shall be LDR compliant and the applicant's hope is that such improvements enhance not only this development but also the streets for our neighbors and the surrounding neighborhood. Such plans will be finalized as the site plan is finalized and feedback is obtained.
16. Provide a plan that shows the location and route of all dry utilities, communications, and power services that meet ToJ setbacks. Facilities serving private properties should be located on private property. Refer LDRs Article 7, Division 7.7 below.
 - a. Please see attached draft utility plans which will be refined through Sketch Plan.
17. Provide a plan for the Cache Creek Tube to remain in place or be realigned. CenturyLink/Lumen facilities are still blocking the crossing at Mercill and Cache.
 - a. Please reference item 6 and the attached utility plan.
18. Provide a plan that shows the accesses to the project relative to the access across the streets.
 - a. Please see attached plan set.
19. Provide confirmation that WYDOT still approves the Cache Street access.
 - a. NAP for the attached plan set.
20. Provide more information on how the development will be owned, managed, and if any of the buildings will be subdivided. Ownership will help determine how the utilities will be designed.
 - a. Residential buildings are anticipated to be condominiumized. Hotel will be owned separately. Condominiums in the mixed-use buildings that include the hotel product will be separately owned. Ownership and platting will be further refined at a development plan level. The attached utility plan details the proposed relocation of the utilities.
21. Clarify how a below-grade parking facility that serves and is below multiple buildings will be owned and managed.

- a. Pursuant to Staff feedback at the Pre-application conference, below grade parking may be connected to multiple buildings and provide connectivity. All parking will be managed by either the hotel or a master homeowner's association.
- 22. Provide a plan for how will you serve 10 separate buildings with sanitary sewer, water distribution, storm, power, communications, gas, etc. With regards to ToJ Code the standard is one (1) service per lot. Show existing lot lines for reference.
 - a. Please find the draft utility plan attached. Applicant looks forward to discussing the utility plan with the Engineering department. In the event applicant needs to submeter any utilities, it shall do so on its property at its expense.
- 23. Provide elevated sidewalks and squared off accesses at all entries and alleys.
 - a. Applicant plans to comply with the LDRs and code for sidewalks and squared off accesses at all entries and alleys.
- 24. Provide a plan to minimize impacts to infrastructure by providing...
 - a. Comment included in the departmental reviews was incomplete and reads per the above.

Town of Jackson Code

1. A construction-staging narrative shall be submitted for review and approval with the Development Plan application.
 - a. Understood, if a staging plan is required for the Project a narrative will be submitted with the Development Plan application. If required, Applicant will work to identify a sufficient staging area to be utilized for the Project.
2. Provide information on how the structure will be shored and constructed. Clarify if any shoring will encroach into the public way.
 - a. Depending on the final site plan, Applicant anticipates shoring and soil nails will occur on some if not all of the site. In such event, applicant will apply for encroachment permits for such soil nails. The shoring plan will be refined and defined with feedback from Staff and Town Council during sketch plan; the necessity for shoring will depend on the ultimate location of buildings in relation to the property lines.
3. Development plans provided shall show all work to be completed within the Towns rights-of way. Show all encroachments into the Towns rights-of-way or easements. Encroachment agreements are required for encroachments of buildings, retaining walls, foundations, canopies, balconies, roofs, shoring, etc.
 - a. Applicant will provide details and pursue any required encroachment agreements for the Sketch Plan approved project at Development Plan. Such encroachments will be informed by DRCs input with push and pull of structures adjacent to the property lines.
4. A demolition permit is required for each existing structure to be removed from the site. Water and sewer services to be abandoned for the project shall be abandoned at the main during the demolition phase of the project.
 - a. Applicant will pursue the required demolition permits at the appropriate time.

LAND DEVELOPMENT REGULATIONS

Development shall comply with the physical development standards for its zoning.

5.7 GRADING, EROSION CONTROL, AND STORMWATER

5.7.2 Grading Standards:

1. Provide a grading plan that shows compliance with this section. Plan shall include existing and proposed site contours with elevation labels, spot elevations, high and low points, grade breaks.
 - a. Pursuant to Town Engineering feedback during the pre-application conference, grading plans will be provided for the Sketch Plan approved project during Development Plan as the site plan is refined with Town Council, public, and Staff feedback.

5.7.3 Erosion Control Standards:

1. Provide an erosion control plan that shows compliance with this section.
 - a. Pursuant to Town Engineering feedback during the pre-application conference, grading plans will be provided for the Sketch Plan approved project during Development Plan as the site plan is refined with Town Council, public, and Staff feedback.

5.7.4 Stormwater Management Standards:

1. Provide an erosion control plan that shows compliance with this section.
 - a. Pursuant to Town Engineering feedback during the pre-application conference, grading plans will be provided for the Sketch Plan approved project during Development Plan as the site plan is refined with Town Council, public, and Staff feedback.

6.2 PARKING AND LOADING STANDARDS

6.2.2 Required Parking and Loading:

1. Provide a plan that shows the proposed parking in compliance with this section. Include location and dimension of parking spaces, accessible spaces, including van accessible spaces, and bicycle parking. For uses that require deliveries, show the proposed loading facilities.
 - a. The attached plan complies with the parking requirements and is noted.

6.2.5 Off-Street Parking and Loading Design Standards:

1. Provide a parking plan in compliance with this section. Include surface materials and drainage plans, access and circulation, and snow storage.
 - a. The site plan shows access, circulation, and snow storage. Drainage and surface materials will be informed as the site plan is refined.

6.4 OPERATIONAL STANDARDS

6.4.1 Outside Storage:

1. Provide a Construction Management Plan / Narrative, for any offsite storage address compliance with this section.
 - a. The draft construction management plan has been attached herein, and will be further refined as construction means and methods are determined by the contractor, such as crane placement and timing of improvements which impact pedestrian, vehicular, and cyclist circulation. Any required offsite storage or

staging will be addressed in a construction management plan that will be finalized through the Development Plan.

7.2 SUBDIVISION STANDARDS

7.2.2 Standards Applicable to all Development

7.2.2.A Improvements:

1. Provide plans and specifications for roads, streets, alleys, sidewalks, and pathways including streetlight, signage, and markings.
 - a. The plans and specifications will be developed through sketch plan, taking into account the community streets plan and opportunities for enhancements in the right of way. Specific engineering plans will be provided for the Sketch Plan approved project during Development Plan.
2. Provide a sanitary sewer wastewater plan.
 - a. Please see attached proposed draft utility plan. This will be further refined through Sketch Plan.
3. Provide a water supply plan.
 - a. Please see attached proposed draft utility plan. This will be further refined through Sketch Plan. Please see attached draft domestic water demand.
4. Provide a storm drainage plan, basins, calculations, treatment method. Stormwater discharged to a public storm sewer or right of way requires treatment. Wyoming DEQ 5D2 permits are required for subsurface discharges.
 - a. Cache Creek tube capacity is unknown at this time and whether it can handle additional storm drainage from this development. In the event it cannot, the applicant will run its own discharge line either parallel to the Cache Creek Tube or North in the alley to Perry Street and will discharge it's stormwater adjacent to the Cache Creek tube into Flat Creek after pre-treatment (sand and oil separator) and detention at historic rates. Calculations and the storm drainage plan will be informed as building, roof, and landscaping design is finalized with feedback at DRC, PC, and Town Council. Calculations for the stormwater will depend on total site landscaping and design, which will be finalized based on feedback from DRC, PC, and Town Council. Preliminary storm drain calculations are attached.
5. Provide an irrigation plan or statement.
 - a. Once DRC, PC, and Town Council determine the location of the buildings, a detailed irrigation plan will be provided.
6. Provide a plan for all other utilities, such as telephone, cable TV, electric, fiber, gas.
 - a. See attached utility plan.
7. Provide a plan for permanent reference monuments, property corners, etc.
 - a. Applicant plans on vacating internal lot lines through boundary adjustments and a potential plat. Reference monuments located in improvements such as sidewalks will be monumented by brass caps cast in concrete sidewalks and verified by surveyors. Building setbacks will largely render conflicts between construction and surveying monuments moot.
8. Permits are required prior to construction of development improvements.
 - a. Applicant will apply for building permits once its Development Plan is approved..

9. All improvements shall be designed by a professional engineer licensed to do such work in the State of Wyoming.
 - a. All improvements will be designed by a professional engineer licensed to do work in Wyoming.
10. The Town Council may require installation and construction of utilities, pavement and other land improvements in excess of subdivision design needs, to assure adequate service to future development areas.
 - a. Applicant looks forward to discussing with the Town Council at the appropriate time and exploring cost shares as necessary.

7.6 TRANSPORTATION FACILITY STANDARDS

7.6.2 Access to Roads, Streets and Highways:

1. Provide a plan showing the point of access. Show compliance with the standards for access and justification for access to collector and arterial roads.
 - a. Please see the preliminary Sketch Plan application attached. Further information will be provided for the Sketch Plan approved project during Development Plan.

7.6.3 Streets Alleys and Easements:

1. Provide a plan showing all proposed streets, alleys, and easements. Show compliance with the general standards, extension, engineering access, right-of-way, pavement widths, urban and rural classification, alignment, profile, grades, and intersections.
 - a. Requested detailed engineering plans will be provided for the Sketch Plan approved project during Development Plan and will be dependent on adjustment of community street widths in discussion with Town Engineering and Council. All alignment, profiles, and grades will be LDR compliant.

7.6.4 Street and Road Standards (section not used, for reference):

1. Provide a plan that generally complies with the Community Streets plan and Bicycle improvements plan. Include property lines, dimensions, radii, elevations, slopes, grade changes, etc.
 - a. Engineering and Pathways have both expressed an interest in narrowing adjacent streets and both widening and or providing a possible bike lane (protected or not).
2. For all pedestrian areas provide a plan that includes dimensions, radii, elevations, slopes (running and cross slopes, not oblique slopes), ramp slopes, grade breaks, stair dimensions, handrails, guards, etc. showing compliance with ADA, IBC, and Town standards.
 - a. Project plan set will abide by ADA, IBC, and Town Standards. Additional engineering plans will be provided for the Sketch Plan approved project during Development Plan.
3. Design street frontages using existing road grades for control of sidewalk and buffer grades. Treat any buffer and sidewalk all as one plane, without grade breaks. Accommodate accessibility within the private property and not in the right-of-way.
 - a. Understood and Applicant looks forward to working with the Town in finalizing the Sketch Plan approval for the Project.

7.7 REQUIRED UTILITIES

7.7.2 Potable Water Supply:

1. Provide a water system analysis indicating the required domestic and fire flow demands. Identify impacts to or upgrade requirements for the existing distribution, supply, or treatment system.
 - a. Please see attached draft water demand calculations. Required water supply studies will be provided at Development Plan.
2. Show compliance with state regulations, construction standards, connections for lots of record, provisions for system expansion, and fire protection. Provide information on planned metering and backflow prevention locations.
 - a. As referenced above, applicant will provide private submetering as necessitated by ownership, will provide adequate piping or apparatus for system expansion, and will install all required backflow prevention devices at appropriate locations informed by final site plan. The proposed Project will be in compliance with state and local regulations.
3. Water services over 2-inch in diameter require engineering design.
 - a. Understood, and Applicant will work with Town Engineering on the final design of water services.

7.7.3 Sanitary Sewer System:

1. Provide a sanitary sewer analysis. Identify downstream impacts on existing sewers, lift stations, and treatment facilities.
 - a. Preliminary sanitary sewer demand analysis is attached. Applicant will require feedback from Town Engineering on downstream impacts and looks forward to discussing any necessary.
2. Show compliance with state regulations, construction standards, maximum allowable infiltration, connections for lots of record, and provisions for system expansion.
 - a. This detailed analysis will be provided during Development Plan.

7.7.4 Irrigation System:

1. Provide an irrigation plan for the entire development. For all systems provide the irrigation water demand, type of backflow preventer, location, and whether irrigation will be metered separately from domestic.
 - a. See reference above.

7.7.5 Other Utilities:

1. Provide a plan for all other utilities, such as telephone, cable TV, electric, fiber, gas. All utilities shall be installed underground. Provide right-of-way or easements as required.
 - a. See attached draft utility plan. Right of way easements will be provided as required.
2. Show that private utilities can be located on private properties, e.g. transformers.
 - a. See attached draft utility plan.

8.3.2.C FINDINGS FOR APPROVAL

1. Provide a plan for mail delivery that does not have a significant impact on public facilities and services, e.g. mail and parcel room, USPS Caller service, private mail delivery, etc.
 - a. With finalization of site plan, Applicant intends on providing mail room and package delivery storage/system.

IBC CHAPTER 11 / ADA ACCESSIBILITY

1. Show compliance with accessibility regulations including but not limited to: 1104.1: At least one accessible route within the site shall be provided from public transportation stops, accessible parking, accessible passenger loading zones, and public streets or sidewalks to the accessible building entrance served.
 - a. Project will abide by IBC requirements and ADA accessibility requirements.
2. At least one accessible route shall connect accessible buildings, accessible facilities, accessible elements and accessible spaces that are on the same site.
 - a. Project will abide by IBC requirements and ADA accessibility requirements.
3. Accessible routes shall coincide with or be located in the same area as a general circulation path. Where the circulation path is interior, the accessible route shall be interior. Where only one accessible route is provided, the accessible route shall not pass through kitchens, storage rooms, restrooms, closets or similar spaces.
 - a. Project will abide by IBC requirements and ADA accessibility requirements.

Additional Comments:

1. FLOOR AREA BREAKDOWN - In the Sketch Plan and Development Plan submittal, the applicant needs to clearly identify how/where the base FAR and 2:1 bonus are being allocated both in the floor plans and narrative. When calculating FAR, hallways, mechanical spaces, stairs, and elevators count. Stairs are counted every two floors and elevators are counted only once. The previous Loop project did a good job of breaking down the floor area via color-coded plans. Use that as an example for this project.
 - a. See attached plan set which includes color coded plans. Floor area is fungible between lots of record on a noncontiguous site.

Floor area calculations are looked at based on the “site”. The maximum floor area allowed on a site is the maximum habitable floor area not including basement floor area, as defined in Division 9.5. LDRs, Section 9.4.6.C.1. The maximum above-grade floor area is calculated by multiplying the allowed FAR by the applicable site area. LDRs, Section 9.4.6.C.3. “Site” means the entire area of the land on which a use or development is existing or proposed. A site may be a portion of a lot of record or may include multiple lots of record. LDRs, Section 9.5. Here, the “site” includes multiple lots of record separated by an alley.

In the CR-2 district, gross site area is used to calculate maximum floor area. LDRs, Section 9.4.6.C.2. Gross site area is the area of the lot of record or site as determined by a certified boundary survey. LDRs, 9.4.4.A. Gross site area may include noncontiguous sites under certain conditions. Specifically, gross site area may equal the total area of 2 or more noncontiguous sites provided the following standards are met (see LDRs, Section 9.4.4.A.1):

- a. The location of development that results from the transfer of density/intensity from one noncontiguous site to another shall be consistent with areas designated for development in the Comprehensive Plan;
- b. Infrastructure shall be available, or can be constructed, to sufficiently serve the proposed development; and
- c. Open space that results from the transfer of density/intensity from one noncontiguous site to another shall accomplish the objectives of open space preservation described in these LDRs and the Comprehensive Plan.

With regard to (a), this is an in-Town site, all within District 2 (Town Commercial Core), Subarea 2.3: Downtown under the Comprehensive Plan. This area is a transitional area intended for redevelopment with a mixture of two to three-story commercial, lodging, and residential buildings and a high concentration of tourist-related accommodations and services. p. IV-24 in Comprehensive Plan. Both portions of the site, the east and west sides of the alley, are in the same Comprehensive Plan District and same zone – we are not dealing with a transfer between two different Comprehensive Plan Areas.

With regard to (b), infrastructure will be sufficient here to serve the proposed development.

With regard to (c), open space and landscape surface ratios will be complied with on the site. See the enclosed LDR compliance checklist.

The Town has allowed the transfer and utilization of FAR from one noncontiguous site/lot to another. In 2022, the Town approved Hotel Jackson's transfer of 4,463 square feet of FAR from 145 North Glenwood Avenue to 135 N. Cache pursuant to P22-156 (Amendment to DEV), noting that the Town allows the shifting of floor area from noncontiguous parcels, and treating noncontiguous parcels as one site, if there is common ownership and the sites would be contiguous but for a right of way between them. Here, we have both common ownership and contiguity but for a right of way between the parcels (i.e. the alley).

Moreover, the entire site is included in one Sketch Plan application so that the two sides of the alley would be tied together in one planning application. In the Hotel Jackson application, the Town required the owner to record a deed restriction on the Glenwood parcel for record-keeping purposes so that future owners had notice that the Glenwood parcel had 4,463 square feet less FAR than what was allowed. Here, instruments could be recorded, if necessary, to document any FAR transfer between parcels on the east and west sides of the alley so that the public and future owners are put on notice of available FAR on either side of the site. Moreover, irrespective of ownership, the Mogul parcels east of the alley and those west of the alley will be tied together for entitlement purposes since all lots have been included in one application.

To the extent that Town Staff has a different interpretation of the definition of the site, the programming can be adjusted to be located on the respective sides of the alleyway.

2. **FUNCTION OF THE ALLEY/VALET/LOADING** – Staff will be requesting additional information on the function and improvements within the alley, especially related to the proposed circular turnaround. Are there any underground or above-ground encroachments in the alley? Need to discuss maintenance such as snow plowing...is the applicant planning on maintaining the alley being that this is their primary vehicular entrance? Also, when a guest parks in the hotel valet area and is staying in buildings A or K, is the plan that they simply walk outside during the winter to these buildings or is there a strategic pedestrian pathway connection plan proposed?
 - a. As planned, applicant is not requesting encroachments for the cul-de-sac drop off space. A snow melt system is planned under the private property section of the cul-de-sac drop off and loading area. Applicant is interested in discussing these improvements, including snow melt in the middle of the cul-de-sac, which discussion will be informed by Engineering's feedback on how this alley should function and be improved. Applicant is open to discussing improvements and maintenance of the alleyway with the Town public works. With respect to the parking structures, the attached plan set would include parking beneath the structures. A further detailed parking summary is attached herein.
3. **SITE FUNCTIONALITY** – Staff will be requesting additional information and explanation on how the site functions, pedestrian movement/access, parking, etc.
 - a. Applicant is happy to further discuss operations and plans with the Town staff during the Sketch Plan process. Applicant has preliminary ideas and is looking forward to incorporating comments from the Town Staff, DRC, PC, and Town Council.
4. **2:1 BONUS** – Does the applicant still plan to put the workforce portion of the 2:1 bonus off-site? Staff does not advise proposing the workforce portion off-site because one of the Council's recent big building changes is to require all 2:1 to be on the same site.
 - a. After hearing Town's concerns regarding 2:1 bonus being located onsite. The Applicant has placed all 2:1 bonus workforce housing on-site. Applicant has interest in further exploring the Town's interest in allowing the required workforce housing to be located off-site in exchange for affordable deed restrictions.
5. **STREET DESIGNATION** - Cache and Glenwood will be primary streets and the other streets secondary. Primary streets require a 70% building frontage within the 0'-10' setback range. The 0'-10' measurement is not from the property line but rather starts 13.5' back from the existing back of the curb. Secondary streets require a 30% building frontage use the same setback range as the primary (i.e., 0'-10' and measured 13.5' back from the back of curb).
 - a. Preliminary site design complies with these requirements.
6. **PEDESTRIAN FRONTAGE TYPE** - The pedestrian frontage type requires Trees in Grates on Cache, Mercill, and Perry. Trees in Lawn may be desired on Glenwood.

- a. Understood, trees are planned to be placed in grates on Cache, Mercill, and Perry. Applicant is looking forward to feedback on the vegetation along Glenwood and the additional landscaping plans.
- 7. BASEMENT DEFINITION – Similar to the previous Sketch Plan submittal, the applicant will need to demonstrate that the basement complies with the Town’s basement definition which states "a basement is any story for which the finish floor of the story above is less than four feet above finished grade for at least 50 percent of the perimeter of the story and at no point greater than ten feet above finished grade." Since the last submittal, the Planning Director has interpreted that areas dedicated as basement window wells/walkout basements count against the basement definition’s 50% perimeter rule.
 - a. All buildings with basements will demonstrate compliance with the Town’s basement definition. Building D is the only building with basement habitable space which includes the spa. That basement space is fully subgrade, and the building no longer include partially below grade space that is qualified as basement.
- 8. HOUSING MITIGATION - Regarding existing housing credits, please provide a breakdown of the uses and their associated square footage for each property. Housing credits shall mean a building or use existing on December 18, 1995, or the building or use legally established since that date with the highest affordable workforce housing requirement. Staff believes this information already exists but it was not included in this particular submittal.
 - a. Please find this in the attached submittal.
- 9. EV PARKING – The new EV parking standards in LDR Section 6.2.2.F will apply to this development.
 - a. Building will abide by the EV parking standards and is shown in the plan set.
- 10. BIKE PARKING - The new bike parking standards in LDR Section 6.2.2.D will apply to this development.
 - a. Bike parking meets the LDR requirements and is shown in the plan set.
- 11. SIDE INTERIOR SETBACK - The CR-2 zone has a 10’ rear setback off the alley.
 - a. Town Staff mentioned during the pre-application conference that the corner lots may benefit from a 5 foot setback from the alleyway. Applicant remains interested in pursuing such setback on limited corner lots in order to help break up massing along the alley.
- 12. LANDSCAPE SURFACE RATIO AND PLANT UNITS - Demonstrate compliance with the 10% LSR requirement and plant unit requirement. All required plant units need to be provided on-site. The required street trees are a separate requirement.
 - a. Project meets 10% LSR ratio as shown in the plan set.
- 13. PARKING REQUIRED - Need clarification on the amount of parking required which includes factoring in the commercial spaces. In addition, staff did not find any historical parking credits tied to any of the subject properties. Also, is the hotel and residential uses served by valet parking? Need details on the automated parking system that was mentioned in the narrative.
 - a. Please find a parking summary attached which details the requirements and parking provided. Per discussion with Staff during the pre-application conference, Applicant was informed that the Town would permit connection and connectivity

below the buildings via the parking structures. This is anticipated to be much more efficient than the current programming. Applicant looks forward to discussing this with Staff and if permitted making these revisions during the Sketch Plan process, as Staff communicated that the change would not be deemed a substantial modification.

14. PHASING – Staff will be looking for more details about the phasing plan with the Sketch Plan submittal. Since all uses need parking, staff assumes that the buildings with the automated parking will received CO first.
 - a. As of Sketch Plan submittal, Applicant plans to build the Project in one phase, which will include the delivery of the parking.
15. SUBDIVISION - Are there any plans to subdivide the buildings and/or units into ownership condominiums?
 - a. Residential buildings are anticipated to be condominiumized. Hotel will be owned separately. Condominiums in the mixed-use buildings that include the hotel product will be separately owned. Ownership and platting will be further refined at a development plan level.
16. LDR COMPLIANCE - Applicant needs to provide a full LDR compliance checklist and housing mitigation plan.
 - a. LDR compliance checklist is attached herein, as is the housing mitigation plan.

Sincerely,
Brad Wagstaff
Chief Executive Officer



Neighborhood Meeting Summary

Date: 10/30/2024

Location: Jackson SouthTown Clubhouse

Attendees:

- Mogul Capital Team (Brad Wagstaff, Brent Layton, Cole Larson)
- Stefan Fodor (Fodor Law Office)
- Joe Paulson
- Michael Pruett
- Dawn Pruett
- Cortney Spraybemy
- Mark Sullivan
- Rob Neill
- Phoebe Kudar
- Jacob Harasek
- Keith Hier
- Stephan Haerns
- Michael Kudar
- Joe Kudar
- Two attendees elected to not sign in

The Applicant gave a general overview of the history of the project and the updated plans being submitted to the Town of Jackson.

The Applicant opened the floor to questions. Those questions are summarized below.

Q: What is the total square footage of the project?

- A: Total above grade habitable square footage of the project is approximately ~175,000 SF.
Note: final submittal to the Town deviates slightly from this number.

Q: Would it be better to do all the parking underground in non-separate structures?

- A: Yes, it would be more efficient to not have to separate the parking structures. The current LDRs do not explicitly provide this, but it would provide efficiencies. The current submittal is showing separate parking structures as that is what the Town currently explicitly allows.

Q: Can Mogul speak to what is going to happen to the alley with the circular drop off and reception and how others expect to use it?

- A: The alleyway will remain open to public use at all time and the Project's use will consist of cross traffic with queuing on our Property to get to the circular drop off. We do hope that the Town will allow us to improve the alleyway. The plan is to renovate the alleyway and make it more pedestrian and user friendly.

Q: How does loading work with the alley way?

- A: We are working to refine that but each building will have its own drop off/loading programming.

Q: An unidentified attendee requested information on how the public was notified about this meeting?

- A: Mogul Capital sent letters directly to the neighbors per the Town requirements and posted a sign on the project site notifying the public of the meeting in accordance with Town regulations.

Q: An unidentified attendee asked how we can obtain the copy of the site plan and renderings, and track changes in the project?

- A: After the project is submitted to the Town, all plans are made public. Mogul is also happy to send plans to individuals as requested.

Q: How many workforce units will be built compared to market rate units?

- A: Current design plans include roughly 50,000 SF of habitable market units, and 25,000 SF of habitable workforce units. *Note: final submittal to the Town deviates slightly from this number.*



Parking Requirements and Details

Residential Unit Parking

The proposed property has 20 market rate residential units and 19 workforce housing units. In compliance with the LDRs, these units are allocated 1.5 parking spaces per unit for a total of 59 parking spaces. These parking spaces are located within the subterranean parking garage. Residential units are programmed to include designated parking spaces within the respective parking structures.

Hotel Parking

The proposed property has 114 hotel rooms. Each room is allocated 0.75 parking spaces per the LDRs, for a total of 86 parking spaces allocated for hotel guest use. These 86 spaces are located within the parking garage structures. The hotel parking is currently intended to be valet managed. While parking for the hotel is provided on the East Side of the alleyway, additional parking is available on the West Side of the alleyway and would be serviced by the valet.

Restaurant Parking

The property will include a restaurant that will be open to both guests and the public. The restaurant is currently programmed to be 3,509 sf. The LDR requirements for the restaurant parking is 1 per 73 sf. While the restaurant is open to the public and outside guests for lunch and dinner, it will serve only hotel guests for breakfast and room service. Applicant estimates that based on this operating plan, the location of the restaurant inside the hotel with no direct external restaurant street access that approximately 33% of the patrons in the restaurant will be either guests of the hotel or owners of the residential units. As such, this would indicate a parking requirement of 32 spaces. There are 32 parking spaces within the parking structure allocated for restaurant parking, which can be valeted or self-parked. There are an additional 21 street parking credits associated with the property, per the LDRs. This totals to 53 parking spaces for restaurant and property use. Of note, the Property is programmed to include a total of 45 excess parking spaces which can be available for additional restaurant parking if necessary. Due to the excess of parking, the restaurant parking allocation can be LDR compliant should Staff disagree with the operating assumption of 33% of patrons being from the Property.

Bike Parking

The property will include ample bike storage for the use of hotel guests, residents, and day patrons alike. This will contain a mix of both long-term and short-term bike parking. There will be 40 short-term bike parking spots and 80 long-term bike parking spots available on the property, which is in compliance with the LDRs.

North Cache Parking Requirements				
Use	Size	LDR Parking Ratio	Parking Sharing Allowed in LDRs	Parking Required
Market Rate Residential	20 units	1.5x	-	30
Workforce Housing	19 units	1.5x	-	29
Hotel	114 keys	0.75x	-	86
Restaurant	3,509 sf	1 space per 73 sf	20%	38
LDR Required Parking				182
Proposed Parking Requirement Reductions				
Bike Parking Reduction (10%) *(Planning Director Approval)				-18
Restaurant Usage Adjustment (33% total reduction of requirement)*				-6
Total				158

** Value shown represents delta between the LDR allowed parking sharing of 20% and 33% proposed*

Parking Summary	
Parking Required	158
Parking Provided	
Parking Garage Standard	171
Accessible Van	2
Accessible Vehicle	7
Compact	2
Street Parking Credits	21
Total Provided	203
Excess Parking Provided	45

Summary

Per the LDRs and Mogul Capital's operating assumptions, the required parking for the project equates to 158 spaces. Of note, this includes the assumption that Staff and the Planning Director agree with the 10% bike parking reduction and restaurant operating assumption. Should the bike parking reduction not be included, and the restaurant shared parking remain at the allowed 20% in the LDRs, the required parking for the Property is 182 spaces. The property adequately provides that requirement with a total of 207 parking spaces, indicating substantial excess parking spaces. Of the 203 spaces, 182 spaces are located in the parking garage, with 21 additional spaces contributed from the street parking credits for the Project.



Housing Mitigation Plan – North Cache

Housing Credits:

In review of prior uses of the site, the housing credit calculator provides housing credits for the project. These prior uses include the following:

- 375 N Cache: Teton Texaco
 - o Prior use included 3,000 sf of heavy retail and service, as the space was utilized as a gas station.
 - o This use equates to 0.369 housing credits, per the housing calculator
- 355 N Cache: The Old Orvill's Thrift Shop
 - o Prior use included a thrift shop (retail) which occupied 2,500 sf
 - o This use equates to 0.539 housing credits, per the housing calculator
- 335 N Cache: Lewis & Clark
 - o Prior use included an expedition company that occupied 1,500 sf
 - o This use equates to 1.440 housing credits, per the housing calculator
- 325 N Cache & 45 Mercill: Pioneer Motel
 - o Prior use included the Pioneer Motel, classified as conventional lodging. The Pioneer Motel held 25 keys.
 - o This use equates to 2.554 housing credits, per the housing calculator
- 330 N Glenwood & 65 Mercill: Office
 - o 330 N Glenwood: Use of the site includes an office building totaling 16,459 sf
 - o 65 Mercill: Use of site includes an office building totaling 5,366 sf
 - o This use equates to 5.204 housing credits, per the housing calculator
- 360 N Glenwood & 370 N Glenwood: Residential House
 - o Use of site included a detached single family unit (unrestricted), totaling 1,216 sf
 - o Use of site includes a detached single family housing, totaling 1,840 sf
 - o This use equates to 0.072 housing credits, per the housing calculator

In total, the prior uses of the site equate to 10.177 per the TOJ housing calculator (attached herein)

Pursuant to the Jackson/Teton County Affordable Housing Checklist – Housing Mitigation Plan submittal, please see Mogul Capital’s responses below.

As the Town is aware, Mogul Capital intends to utilize The Loop, the 194-unit approved multifamily development located off of South Park Loop Road, to mitigate the housing affordable requirements generated by the proposed Project. The housing mitigation calculator indicates that 3.568 units are required to mitigate the project. If the project is approved, Mogul Capital would agree to deed restrict these 4 rental units at The Loop. The mitigation calculator and LDRs indicate that those units will be spread as follows, and the supporting housing mitigation calculator is attached:

- AMI Range: 0-50%
 - 1 Two-bedroom
- AMI Range: 50-80%
 - 1 Studio
- AMI Range 80-120%
 - 1 Studio
 - 1 Two-bedroom

The Loop will consist of two buildings. Both buildings will include a diversified mix of affordable, workforce, and market rate housing. All units in each building will be available once the respective building is delivered and a certificate of occupancy is received. Thus, the deed restricted units would be available at the same time as the market rate units.

In addition, Mogul is utilizing the 2:1 bonus housing tool for the proposed Project. After hearing Town’s concerns regarding 2:1 bonus being located onsite, the Applicant has placed all 2:1 bonus workforce housing on-site. Applicant has interest in further exploring the Town’s interest in allowing the required workforce housing to be located off-site in exchange for affordable deed restrictions. To satisfy the 2:1 requirement, the proposed Project includes 25,325 sf of workforce housing, which represents 1/3 of the total housing of the Project.



Jackson/Teton County Affordable Housing Checklist - Housing Mitigation Plan (LDR Division 6.3.6)

Every development application must include a Housing Mitigation Plan for sufficiency. Every Housing Mitigation Plan is required to contain the following information:

Applicability (LDR 6.3.6.A.3.a)

1. Does your development qualify for an exemption from a housing mitigation requirement? Yes ☐ No ☒
If yes, explain and refer to the proper LDR _____

2. Are there credits associated with your development? Yes ☒ No ☐ If yes, explain where the existing credits came from, provide the calculation, and refer to the proper LDR. _____
Yes, the credits are associated with the prior uses of the site. These uses are detailed in the attached housing mitigation calculator.

Calculation of Requirement (LDR 6.3.6.A.3.b)

3. Does your development require or are you otherwise requesting approval of an Independent Calculation (LDR section 6.3.3.B)? Yes ☐ No ☒
If yes, provide the calculation according to 6.3.3.B.2 along with impact analysis, verifiable local information, industry specific rather than business specific data, etc. The Planning Department is available to help with this calculation prior to submittal of your application. Attach as a separate sheet.

☒ I have attached a separate sheet with the calculation and supporting data for my Independent Calculation

Housing Mitigation Requirements Calculator. If no to 1 and 3 above, calculate your development's requirement, using the Housing Mitigation Requirements Calculator. The calculator can be found at this link: www.jacksonwy.gov/200/Planning Attach a copy of the first page of the calculator showing the calculations and requirements with your Housing Mitigation Plan. The Planning Department is available to help with this calculation prior to submittal of your application.

☒ I have attached a copy of the first page of the Housing Mitigation Requirements Calculator which includes my requirement and unit types.

Type of Affordable Housing Provided – Standard Restrictions. (LDR 6.3.4)

4. How many ownership or rental units are you proposing in which income ranges with how many bedrooms?
Please complete the matrix below:

Bedrooms	0 – 50%	50% - 80%	80% - 120%	Workforce	Ownership or Rental
1-bedroom		1	1		Rental
2-bedroom	1		1		Rental
3-bedroom					

Special Restriction. The Special Restriction is a contract between The Board of County Commissioners or the Town Council and the owner of real property developed or designated to satisfy the development requirements. The Special Restrictions will be recorded on the property. Appropriate restriction templates can be obtained from the Jackson/Teton County Affordable Housing Department or at this link:
www.tetonwyo.org/1856/Deed-Restriction-Templates .

☐ I have attached a draft of the Standard Restrictions for each unit.

Livability Standards (LDR 6.3.4.D and E) (Rules and Regs Section 2-3). Restricted housing must comply with the Livability Standards in the Jackson/Teton County Housing Department Rules and Regulations. The Livability Standards include amount of cabinetry/counter space, storage, closets, room sizes, appliances, laundry facilities, bathrooms, etc. The Rules and Regulations can be found at this link:
www.tetonwyo.org/1332/Housing-Rules-and-Regulations .

☒ I have reviewed the Livability Standards and understand the requirements. I have/will contact the Housing Department for approval of my units early in the design process to get their approval prior to submitting for building permit.

Method for Providing Required Housing (LDR 6.3.5):

One or a combination of the below methods may be proposed to satisfy your housing requirement. They are listed in the order of preference/priority. Highest priority must be used unless it is demonstrated that a higher priority is impracticable (LDR 6.3.5.C)

1. Construction of new units either on-site or off-site (LDR 6.3.5.D.1).
2. Conveyance of land for affordable/workforce housing (LDR 6.3.5.D.2).
3. Utilization of a banked affordable or workforce housing unit (LDR 6.3.5.D.3).
4. Restriction of an existing residential units as an affordable/workforce housing unit (LDR 6.3.5.D.4).
5. Payment of an in-lieu fee (LDR 6.3.5.D.5).

☒ I have attached a detailed explanation of my proposed method of providing Required Housing.

Phasing Plan (LDR 6.3.5.A.3). Restricted Housing Units shall be ready for occupancy no later than the free market portion of the development is occupied. If the free market portion is to be developed in phases, then the Restricted Housing Units shall be developed prior to or in proportion to the free market portion.

☒ I have attached a detailed description of my phasing plan as it pertains to Affordable/Workforce units.

Development of a new house, hotel, or commercial space generates the need for employees. The construction workforce builds the space, the commercial workforce or residential service workforce works in the space, and first responders are needed to protect the space. Only about 27% of the employees generated by development can afford housing in the community, but the community's "community first" character goal is that 65% of employees live locally. To bridge this affordability gap, each development is required to include affordable workforce housing proportional to the employees it generates. These housing mitigation requirements are established in Division 6.3 of the Land Development Regulations. This worksheet is intended to assist in meeting the requirements for a project. However, an error in the worksheet does not amend the actual standard; if you find an error please notify the Planning Department. Fill in the highlighted cells, all the other cells will autopopulate.

Calculating the Requirement (Sec. 6.3.2 & 6.3.3)

Step 1: Location

Town of Jackson

The applicable regulations vary by jurisdiction please identify the location of your project using the above dropdown options.

The required housing is based on the existing and proposed use of the site. Step 2 is to enter the existing use and Step 3 is to enter the proposed use. Section 6.3.2 of the LDRs establishes the applicability of the affordable workforce housing standards and Section 6.3.3 establishes the specifics on calculation of the requirement. Enter each use in its own row, add rows if needed. If a building has multiple units with the same use, describe each unit in its own row. (For example: if a duplex is composed of a 2,300 sf attached unit and a 1,700 sf attached unit, put each unit in its own row do not put in 4,000 sf of attached single-family.) If a unit type (e.g. apartment floor plan, or commercial tennant space) is replicated exactly multiple times, you may use the "Use Quantity" column to avoid adding multiple rows.

Step 2: Existing Development

Housing is only required for new development. Please describe the existing use of the site so that it can be credited from the housing requirement. The definition of existing use is Section 6.3.2.A.1 of the LDRs. Generally, the existing use to enter is the use with the highest housing requirement that either existed in 1995, or has been permitted since 1995. Please attach proof of existence.

Existing Use (Sec. 6.3.2.A)	Housing Requirement (Sec. 6.3.3.A)	Use Size: bedrooms	Use Size: habitable sf	Use Quantity	Housing Credit
Conventional Lodging	0.102*bedrooms	1	0	25	2.554
Office	0.000247*sf		21094	1	5.204
Retail	0.000216*sf		2500	1	0.539
Outfitter/Tour Operator			1500	1	1.440
Heavy Retail/Service	0.000123*sf		3000	1	0.369
Detached Single-Family Unit (Unre	0.000017*sf+(Exp(-15.49+1.59*Ln(sf)))/2.176		1528	2	0.072
Existing Workforce Housing Credit					10.177

Step 3: Proposed Development

Please describe the proposed use of the site to determine if affordable workforce housing is required as part of the development. Describe the end result of the proposed development. (For example: in the case of an addition do not enter the square footage of the addition, enter the size of the unit upon completion of the addition.)

Proposed Use	Housing Requirement (Sec. 6.3.3.A)	Use Size: bedrooms	Use Size: habitable sf	Use Quantity	Housing Required
Conventional Lodging	0.102*bedrooms	1	0	114	11.645
Restaurant/Bar	0.000599*sf		3509	1	2.100

Affordable Workforce Housing Required:3.568 units

Fee-in-Lieu Amount:n/a

If the amount of required affordable workforce housing is less than one unit, you may pay the above fee in-lieu of providing the required housing. If you elect to pay the fee, your Housing Mitgation Plan is complete. If the requirement is greater than one unit, or you would like to provide a unit to meet the requirement, please proceed to the [Unit Type Sheet](#).

Type of Units Required (Sec. 6.3.4)

Step 4: Type of Units Required

The LDRs include a schedule for how many units have to be affordable to each income range and how many units have to have 1, 2, or 3 bedrooms. The LDRs also allow for adjustment, subject to approval by the Housing Director. To propose an adjusted allocation, change the values in the gold cells; the customized allocation must meet the customization rules provided. If a rule is violated, the cell will turn red. See also Section 6.3.4 of the LDRs.

Affordability	LDR Schedule	Proposed	Custom %	Customization Rules
Units Affordable to Households making 50% Median Income	1.000	1.000	28%	Custom % shall be ≥ 25%
Units Affordable to Households making 80% Median Income	0.568	0.568	16%	Custom % shall be ≥ 19%
Units Affordable to Households making 120% Median Income	2.000	2.000	56%	Custom % shall be < 43%
Workforce Units	0.000	0.000	0%	Custom % shall be < 13%
Total	3.568	3.568	Proposed total shall be ≥ Required	

Bedroom Mix	1 Bed/Studio	2 Bedroom	3 Bedroom	Total Units	Total Employees Housed	Customization Rule
LDR Schedule	1.568	2.000	0.000	3.568	6.422	The Proposed total shall be greater than or equal to the Schedule total
Proposed Allocation	1.568	2.000	0.000	3.568	6.273	
Employees per Unit per LDRs	1.45	2.00	2.10	1.80		

Step 5: Distribution of Affordability by Unit Size

The bedroom mix must be proportionally distributed among the affordability mix. If you modify the distribution (gold cells) the totals must match your allocation totals (they will turn red if they do not). A customized distribution should be within 1 (rounded up or down) of the even distribution displayed (it will turn red if it is not). See also Section 6.3.4 of the LDRs.

Requirement Distribution					Even Distribution		
	1 Bed/Studio	2 Bedroom	3 Bedroom	Total	1 Bed/Studio	2 Bedroom	3 Bedroom
Affordable to 50% Median Income	0.000	1.000	0.000	1.000	0.429	0.429	0.143
Affordable to 80% Median Income	0.568	0.000	0.000	0.568	0.243	0.243	0.081
Affordable to 120% Median Income	1.000	1.000	0.000	2.000	0.857	0.857	0.286
Workforce Restriction	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Total	1.568	2.000	0.000				

Once you have determined the proposed distribution please fill out the [Method Sheet](#) regarding how the required housing will be provided.

CONSTRUCTION MANAGEMENT PLAN

Summary

NORTH CACHE HOTEL & CONDOMINIUMS

1. Construction of a hotel & condominium building. The property is also intended to include a restaurant and spa for the hotel and condominium owners.
2. The proposed schedule will be finalized once the Project has proceeded through the necessary entitlement approvals with the Town of Jackson.
3. Any right-of-way easement work is scheduled to be 30-day duration and the anticipated start date will be determined further in the design and approval process.
4. Public Impact Notice, Construction Parking, Site logistics, temporary use of streets, alleys and public property will be addressed in the permit application.
5. Stormwater Prevention Plan will likely include sandbags around all storm drain and detention areas. Rough grading will also be completed to drain storm water to the controlled detention areas. Mogul Capital plans to work with the TOJ to construct a detailed Stormwater Prevention Plan.
6. Site Plan will be provided when plans are submitted to the city. Construction Management Plan will be negotiated with the TOJ and will show the proposed work, temporary facilities, barriers, pedestrian routes, haul routes, and adjacent streets with sidewalks.
7. As of now, the intent is to setup a crane as needed on site. Current plans would include the need for two cranes, but that may change through the process. A Crane Agreement will be provided if cranes are to be erected on Town's property. A Crane Swing Agreement is for cranes that encroach into the airspace above Town right-of-way or easements. The agreement will be provided prior to crane erection. We understand there will be additional insurance required for this scope.
8. We understand the criteria's needed for protection of pedestrians: barriers to be no less than 6' in height, barriers will be placed on the side of the walkway nearest the construction, barriers will extend full length of the construction site, openings in such barriers will be protected by gates/doors that are normally kept closed and it will not interfere with Town Snow Removal Operations. We will be in compliant with Barrier and Covered Walkway Criteria.
9. All Stabilized construction access will be maintained.
10. Traffic Control plans will be submitted with permit applications if any traffic control is necessary.
11. Shoring Plans will be provided with grading permit application.

DETAILED LITERATURE

Construction Management Plan: Lots 1-12, 15-18 Block 3 J.R. Jones Addition Plat No 113

Project Scope:

The North Cache project consists of a hotel and condominium structure with underground parking structures. Once a permit is in place we intend to clear and grub the lot, relocate any required utilities and begin with shoring and the underground parking garages. The project will be built onsite and a more detailed scope will be provided through the process. Once we break ground we anticipate the project duration to be less than 36 months.

Public Impact:

Owner/Contractor will coordinate with Town of Jackson (TOJ) to develop a distribution list of neighbors that may be affected by construction activities, including the TOJ Engineering Division. Contractor will work to minimize any effect on adjacent properties by maintaining proactive communication including giving a minimum 48-hour notice of any closures and after-hours work. Owner/Contractor acknowledges that the right-of-way (R.O.W.) is a shared public use area and will coordinate with TOJ and neighbors for special event planning and construction activities that may encroach on the R.O.W.. Along US-191, pedestrian traffic will be controlled with temporary signage at street, intersections and property boundaries.

Site Logistics & General Staging Plan:

Site logistics will be included in the submission package. The staging area will house contractor's job office trailer, material laydown, dumpsters, temporary toilets, and construction worker parking. Construction work hours, unless by special exception, will be Monday-Friday 7AM-7PM and 8AM-5PM during weekends and holidays.

Site Fencing & Access:

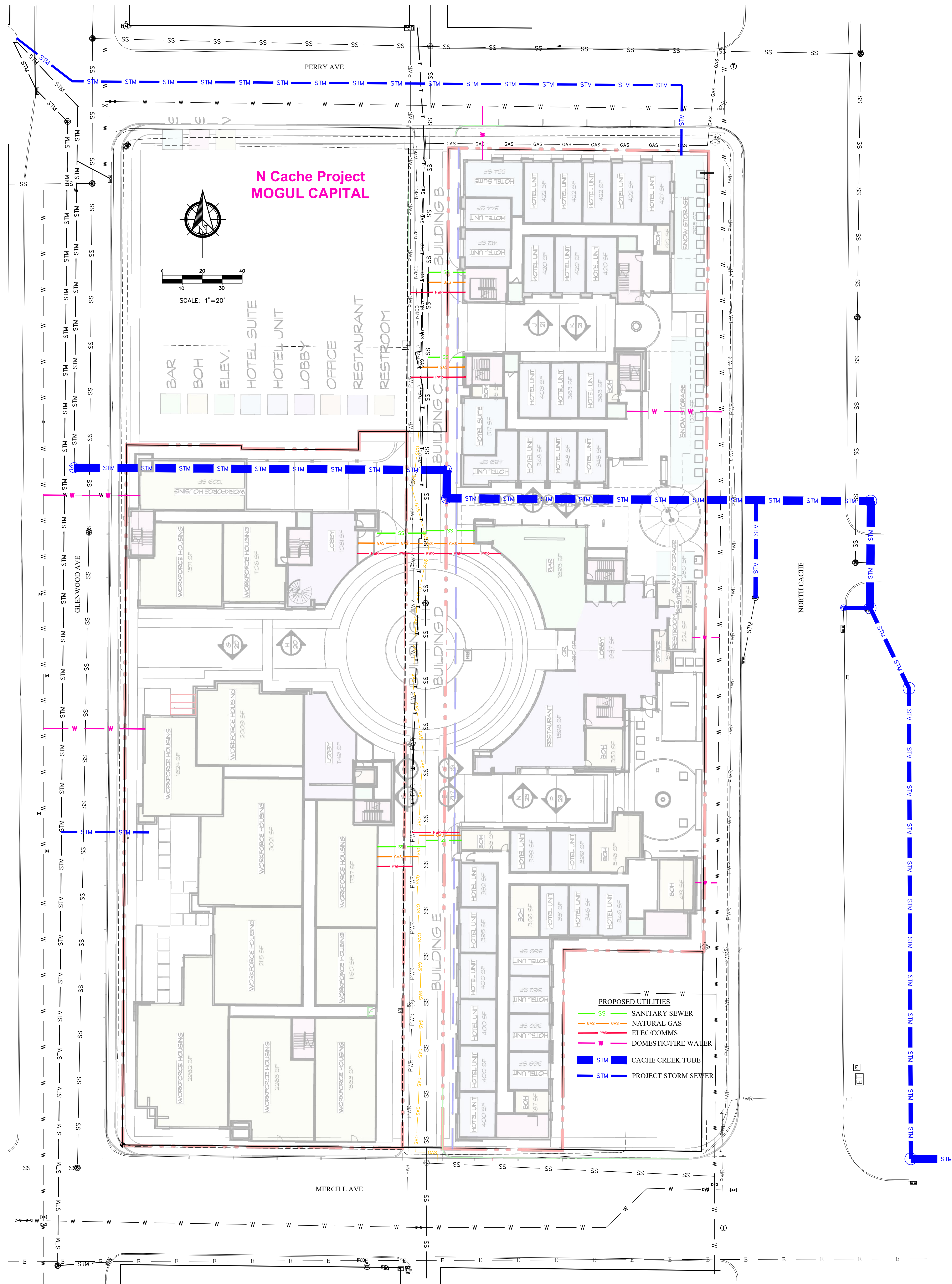
The entire site will be surrounded by temporary metal construction fence with fabric screen. The fence will be placed at the back of sidewalk on the plan north boundaries of the property, near the property line on all four sides. Vehicle gate in the fence will be placed at main entrance into the property on the Glenwood Ave side and be utilize for delivery trucks to pull-through to laydown area. Vehicle gate will include vehicle tracking control pads and a skid steer with sweeper attachment will remain on site to maintain clean roads.

Parking, Street, & Alley Encroachments:

It is anticipated that site logistics will require minimal portions of the adjacent street lanes and sidewalk MAY be occupied by construction equipment or staging at different times during the project. Contractor will follow the temporary use criteria for the use of public property.

Stormwater Management:

All erosion control measures and BMPs will be implemented per plan as developed by the civil engineer, including inlet protection and perimeter controls. All stormwater will be contained to the site with no runoff or outfalls. Vehicle tracking control mats will be installed and maintained at all vehicle access points. Temporary toilets will be properly bedded and staked. Gas, diesel, and oils will be stored in proper containment vessels. Shoring: Shoring will not be required during Phase 1, but will be required for Phase 2 excavation. Details of shoring for Phase 2 will be included on a Phase 2 construction management and staging plan. Method of shoring will be determined following finalization of new foundation's structural design. If soil nails or any other method requiring encroachment into Town's right of way is proposed, then an encroachment agreement will be applied for well in advance of excavation.





Utility Estimate

The Project is being submitted for a conceptual Sketch Plan review. As such, final design including fixture counts and types has not been completed. Applicant looks forward to receiving feedback from the Design Review Committee, Planning Commission, and Town Council, who's feedback will inform the final design and final anticipated utility usage for the Project. As requested by the Town Engineering department during the pre-application process, please find estimates below pertaining to the Project. These estimates are based on this conceptual level of design and are anticipated to change as the design is finalized. As a reminder, the proposed site has had substantial historical uses, some of which have been demolished. Those uses included a 25 key motel, two office buildings totaling approximately 20k sf, multiple residential homes, and multiple retail buildings.

Hotel Water Demand Estimate:

Project Name: NCache Hotel				
Building Type: Hotel				
Number of Units in Building: 114				
Number of Units in Calculation: 114				
Total Fixture Count in Calculation: N=622				
99th Percentile Demand Estimate: Q=23.8 GPM				
Peak Period Hunter Number: H(n,p)=6.0				
Peak Period Stagnation Probability: Pr[Zero Demand]=0%				
Method of Computation: Wistort's Method				
FIXTURE	COUNT	Probability of Use	Fixture Flow Rate	Maximum Recommended Fixture Flow Rate
Bathtub (no Shower)	114	0.48	5.5	5.5
Bidet	0	0.58	2	2
Combination Bath/Shower	0	1.81	5.5	5.5
Faucet, Lavatory	228	1.27	1.5	1.5
Shower, per head (no Bathtub)	114	1.23	2	2
Water Closet, 1.28 GPF Gravity Tank	21	0.58	3	3
Dishwasher	31	0.35	1.3	1.3
Faucet, Kitchen Sink	114	1.27	2.2	2.2
Clothes Washer	0	1.74	3.5	3.5
Faucet, Laundry	0	1.27	2	2
Faucet, Bar Sink	0	1.27	1.5	1.5

Residential Water Demand Estimate:

Project Name: N Cache Residential				
Building Type: Condominium				
Number of Units in Building: 39				
Number of Units in Calculation: 39				
Total Fixture Count in Calculation: N=493				
99th Percentile Demand Estimate: Q=36.7 GPM				
Peak Period Hunter Number: H(n,p)=6.0				
Peak Period Stagnation Probability: Pr[Zero Demand]=0%				
Method of Computation: Wistort's Method				
FIXTURE	COUNT	Probability of Use	Fixture Flow Rate	Maximum Recommended Fixture Flow
Bathtub (no Shower)	20	0.48	5.5	5.5
Bidet		0.58	2	2
Combination Bath/Shower	78	1.81	5.5	5.5
Faucet, Lavatory	117	1.27	1.5	1.5
Shower, per head (no Bathtub)	86	1.23	2	2
Water Closet, 1.28 GPF Gravity Tank	51	0.58	3	3
Dishwasher	39	0.35	1.3	1.3
Faucet, Kitchen Sink	39	1.27	2.2	2.2
Clothes Washer	39	1.74	3.5	3.5
Faucet, Laundry	12	1.27	2	2
Faucet, Bar Sink	12	1.27	1.5	1.5

Sewerage Disposal System:

As mentioned above, the final design will dictate the unit types, fixture counts, and ultimately indicate the final estimated sewerage disposal. Based off today's conceptual plan set, the table below details the projected GPD for the sewerage disposal for the property.

Sewerage Disposal Estimate			
	GPD	Units/Size	Indicated Total GPD
Residential	601	39	23,439
Hotel Rooms	200	114	22,800
Restaurant (per seat)	80	140	11,200
Total			57,439

Fire Flow Demand Estimate:

The table below details the estimated fire flow by building. Of note, the calculations include a reduction for flow as result of the plans for the building to include an approved automatic sprinkler system. (NFPA 5.3 2)

Fire Flow Demand						
Building	Habitable Area	Involvement	Sprinkler Reduction	Flow (100% Involvement)	Flow (50% Involvement)	Flow (25% Involvement)
Building A	78,787	100.00%	75.00%	6,566	3,283	1,641
Building B	21,374	100.00%	75.00%	1,781	891	445
Building C	19,303	100.00%	75.00%	1,609	804	402
Building D	31,561	100.00%	75.00%	2,630	1,315	658
Building E	33,625	100.00%	75.00%	2,802	1,401	701